

## Gate Way Specific Plan Amendments Added

Specific Plan Regulations	Ordinance 869	April 21 <sup>st</sup> 1987
Amendment #1	Resolution #88-136	August 16 <sup>th</sup> 1988
Amendment #2	Ordinance 921	September 6 <sup>th</sup> 1988
Amendment #3	Resolution #91-26	Jan 29 <sup>th</sup> 1991
Amendment #4	Resolution #91-171	August 6 <sup>th</sup> 1991
Amendment #5	Resolution # 95-13	July 28 <sup>th</sup> 1995
Amendment #6	Ordinance 1166	October 3 <sup>rd</sup> 1995
Amendment #7	Ordinance 1215	May 6 <sup>th</sup> 1997

ORDINANCE NO. 869

4/21/87

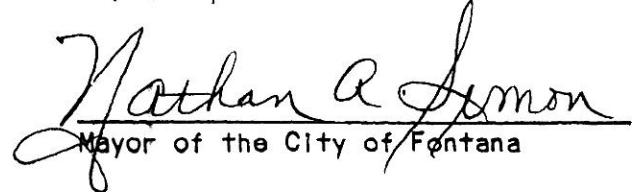
AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF FONTANA  
ADOPTING THE FONTANA GATEWAY SPECIFIC PLAN (SPECIFIC  
PLAN NO. 13)

THE CITY COUNCIL OF THE CITY OF FONTANA DOES ORDAIN AS  
FOLLOWS:

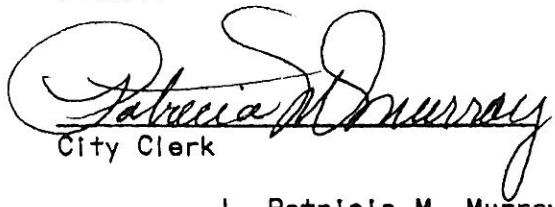
SECTION 1: The Fontana Gateway Specific Plan (Specific Plan No. 13) in its entirety is hereby adopted by reference, a copy of which is on file in the City Clerk's office.

SECTION 2: This ordinance shall take effect thirty (30) days after the date of its adoption and prior to the expiration of fifteen (15) days from the passage thereof shall be published at least once in the Herald News, a newspaper of general circulation, published and circulated in the City of Fontana and thenceforth and thereafter the same shall be in full force and effect.

APPROVED AND ADOPTED this 21st day of April, 1987

  
Nathan A. Simon  
Mayor of the City of Fontana

ATTEST:

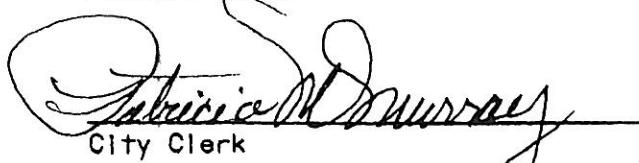
  
Patricia M. Murray  
City Clerk

I, Patricia M. Murray, City Clerk of the City of Fontana and Ex-Officio Clerk of the City Council, do hereby certify that the foregoing ordinance, which was introduced at a regular meeting of said City Council on the 7th day of April, 1987 was finally passed and adopted not less than five days thereafter on the 21st day of April, 1987, by the following vote, to-wit:

AYES: Mayor Simon, Councilmen Boyles, Day, Koehler, Kragness

NOES: None

ABSENT: None

  
Patricia M. Murray  
City Clerk

I further testify that said ordinance was thereupon signed by the Mayor of the City of Fontana.



#1 8/14/88

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF FONTANA,  
CALIFORNIA, ADOPTING A CERTAIN MITIGATED NEGATIVE  
DECLARATION CONCERNING AMENDMENT NO. 1 TO THE FONTANA  
GATEWAY SPECIFIC PLAN.**

**WHEREAS**, the City Council of the City of Fontana, California (the "City"), has by its duly adopted Ordinance No. 809 approved and adopted the Fontana Gateway Specific Plan (the "Specific Plan"); and

**WHEREAS**, prior to the approval and adoption of the Specific Plan, the City Council considered and certified as complete a Final Environmental Impact Report ("EIR") concerning the Specific plan; and

**WHEREAS**, the City Council proposes to consider the adoption of an Ordinance approving and adopting Amendment No. 1 to the Specific Plan; and

**WHEREAS**, for purposes of considering the adoption of said Ordinance, City Staff have prepared a proposed Mitigated Negative Declaration (the "Negative Declaration"), attached hereto as Exhibit "A", concerning the proposed Amendment No. 1 to the Specific Plan, in accordance with the requirements of the California Environmental Quality Act, Public Resources Code Section 21000, et seq. ("CEQA"); and

**WHEREAS**, a notice of public hearing has been duly published and mailed and a public hearing has been duly conducted by the City Council concerning the approval and adoption of the Negative Declaration, and the City Council has considered all testimony and written submissions relevant thereto.

**NOW, THEREFORE, BE IT RESOLVED**, by the City Council of the City of Fontana, California, as follows:

**Section 1.** Based upon any and all written submissions and oral testimony provided to the City Council, the City Council finds and determines that the adoption of Amendment No. 1 to the Specific Plan will not have a significant impact on the environment.

**Section 2.** The City Council hereby certifies the Mitigated Negative Declaration, attached hereto as Exhibit "A" and incorporated herein by reference.

**Section 3.** The City Council hereby authorizes and directs the City Staff to prepare and the City Clerk to file with the County Clerk of the County of San Bernardino a Notice of Determination concerning the adoption of the Negative Declaration.

**APPENDIX M**  
**NOTICE OF NEGATIVE DECLARATION**

**City of Fontana  
Planning Department  
8353 Sierra Avenue  
Fontana, CA 92335**

**Date of Declaration: May 23, 1988      Effective Date: July 25, 1988**

**Applicant:      City of Fontana**

**Address:      8353 Sierra Avenue**

**Project Title:      Amendment #1 Fontana Gateway Specific Plan**

**Env. Log EIR**

**Description of Property:**      The area generally located within the Fontana Gateway Specific Plan area and generally located south of I-10, north of Jurupa Avenue, between Etiwanda and Mulberry Avenues.

**(Plans and specifications are available for public inspection at the public counter of the Planning Department)**

**Pursuant to the provisions of the California Environmental Quality Act of 1970 (Public Resources Code, Sections 15000. et seq), the City of Fontana has determined that the above-referenced project will not have a significant effect upon the environment. An environmental impact report will not be required.**

**Appeal Period**

**This decision may be appealed to the City Council by any person aggrieved, or by an officer, board, department, or commission of the City. Any appeal shall be filed within fourteen (14) days after the publication of this decision or it shall be dismissed by the City Council. The notice of Appeal shall be in writing and shall be filed in duplicate in the office of the City Clerk upon forms provided by the Planning Department.**

10

**CONDITIONAL APPROVAL GRANTED:** YES(  ) NO(  )

**CONDITIONS:** \_\_\_\_\_  
\_\_\_\_\_

**COMMENTS:** \_\_\_\_\_  
\_\_\_\_\_

## MITIGATION MEASURES

The Environmental Impact Report specifies that "Concurrent with the development of individual phases of the proposed project (and prior to the issuance of the Certificate of Occupancy) a storm drain facility sufficient to serve the private improvements under development shall be installed or assured.

Installation of the Master Storm Drain Facilities in Etiwanda Avenue to provide the ultimate storm drain are contingent upon the installation of regional facilities outside of the Specific Plan Area. Such facilities have not been either planned or funded. The proposed use of storm drain detention facilities is an interim measure only and does not violate the Mitigation Measure as stated in the Environmental Impact Report in that it takes measure to assure that these facilities will be funded and installed at the earliest possible date.

While the use of detention basins will have some interim environmental impact, this impact is considered marginal due to limited number of properties that will require use of detention basins will multiply that impact.

X2  
a/4/88

**ORDINANCE NO. 921**

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF FONTANA  
APPROVING AMENDMENT NO. 1 TO THE FONTANA GATEWAY SPECIFIC  
PLAN AMENDING THE DESIGN GUIDELINES RELATING TO INTERIOR  
SIDE AND REAR YARD SETBACKS, LANDSCAPE REQUIREMENTS AND THE  
CONSTRUCTION AND USE OF STORM DRAIN DETENTION BASINS.

THE CITY COUNCIL OF THE CITY OF FONTANA DOES ORDAIN AS FOLLOWS:

**SECTION 1.** Page 41, Chapter 4.5.2 Setbacks. Paragraph is amended to read:

- A. Building setback is defined as the distance from the property line to the building face. Average on-site setbacks will provide diversity along the streetscape. In no case will the street side setback be less than the minimum setback allowed in the Specific Plan except where a modification by the Planning Commission is granted.
- B. Interior side and rear yard setback requirements for properties west of the San Sevaine Channel will be evaluated on a case by case basis. Buildings may be placed on the interior side or rear yard property, when approved by the Planning Commission.

**SECTION 2.** Page 42, Setback Schedule. This table is amended as follows:

**Setback Schedule**

(Measured from ultimate property/ROW line)

	<b>BUILDING</b>	<b>PARKING AND LANDSCAPING</b>
Etiwanda	35 ft. (av) (25 ft. min)	30 ft. min
Jurupa	35 ft. (av) (25 ft. min)	30 ft. min
Mulberry	35 ft. (av) (25 ft. min)	30 ft. min
Slover	35 ft. (av) (25 ft. min)	30 ft. min

*X3* 11/29/91

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF FONTANA RECOMMENDING APPROVAL OF **FONTANA GATEWAY** SPECIFIC PLAN AMENDMENT #1 TO AMEND THE TEXT REQUIRING DEVELOPMENT TO COMPLY WITH THE COUNTY HAZARDOUS WASTE MANAGEMENT PLAN TO THE CITY COUNCIL.**

**WHEREAS**, the Planning Commission has found this amendment to be consistent with the adopted Fontana Area General Plan; and

**WHEREAS**, the Fontana Gateway Specific Plan shall be amended to add subsection 4.2.14 to the Table of Contents to read:

**"4.2.14 Hazardous Waste Management Plan"**

**WHEREAS**, the Fontana Gateway Specific Plan shall be amended to add subsection 4.2.14 to Chapter 4.0 Development and Design Regulations, section 4.2 General Provisions to read:

**"4.2.14 Hazardous Waste Management Plan"**

**"All uses and/or operation shall comply with the HAZARDOUS WASTE MANAGEMENT PLAN per Section 33-202 of the Code of the City of Fontana."**

**NOW, THEREFORE**, in consideration of the evidence received at the hearing, and for the reasons discussed by the Commissioners at said hearing, the Planning Commission now finds as follows:

1. The proposed Amendment is consistent with the General Plan which designates this property as Specific Plan #13 Fontana Gateway. Reference is made to the County Hazardous Waste Management Plan, page 11 of the Safety Element of the General Plan. This amendment will assist in implementation of the County Hazardous Waste Management Plan.

8/6/91

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF  
FONTANA APPROVING FONTANA GATEWAY SPECIFIC PLAN  
AMENDMENT #1 TO AMEND THE TEXT REQUIRING  
DEVELOPMENT TO COMPLY WITH THE COUNTY HAZARDOUS  
WASTE MANAGEMENT PLAN.**

**WHEREAS**, the City Council has found this amendment to be consistent with the adopted Fontana Area General Plan; and

**WHEREAS**, in consideration of the evidence received at the hearing, and for the reasons discussed by the City Council, the City Council now finds as follows:

1. The proposed Amendment is consistent with the General Plan which designates this property as Specific Plan #13 Fontana Gateway. Reference is made to the County Hazardous Waste Management Plan, page 11 of the Safety Element of the General Plan. This amendment will assist in implementation of the County Hazardous Waste Management Plan.
2. Maximum flexibility will still be maintained at the point of Design review.
3. Increasing awareness of developments that use hazard waste or material by increasing the noticing requirement and complying with the CHWMP will help to assure proper services are available and emergency response plans are instituted.
4. Each individual project proposed in any of the three Specific Plans will require its own circulation review at time of submittal. Circulation patterns are not affected by this amendment.
5. Compliance with the CHWMP assists in proper location of facilities. Chapter 5 of the CHWMP contains recommended siting criteria.
6. Adequate aesthetic and quality control will be assured at the time of design review application.
7. Both compliance with CHWMP, the increased noticing requirement and any environmental work to be

#5  
1/28/95

RESOLUTION NO. PC 95-13

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF FONTANA RECOMMENDING THAT THE CITY COUNCIL ADOPT AN ORDINANCE APPROVING THE NEGATIVE DECLARATION, AND ~~FONTANA GATEWAY~~ SPECIFIC PLAN AMENDMENT #2 (SPA #95-03) AMENDING THE SPECIFIC PLAN TO CHANGE THE SOUTHERN CALIFORNIA EDISON (SCE) EASEMENT AREA FROM ITS PRESENT DESIGNATION OF SCE EASEMENT TO GENERAL INDUSTRIAL (M-2-G) AND CONDITIONALLY PERMIT WATER RECLAMATION FACILITIES

WHEREAS, on April 21, 1987, the City Council adopted the ~~Fontana Gateway~~ Specific Plan; and

WHEREAS, the adoption of ~~Fontana Gateway~~ Specific Plan Amendment #2 (SPA #95-03) will amend the Specific Plan to change the Southern California Edison (SCE) area easement from its present designation of SCE easement to General Industrial (M-2-G) and conditionally permit Water Reclamation Facilities; and

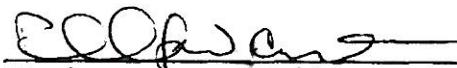
WHEREAS, on August 3, 1995, ~~Fontana Gateway~~ Specific Plan Amendment #2 (SPA #95-03) was duly noticed in the Herald News, a newspaper of general circulation in the City of Fontana; and

WHEREAS, on August 28, 1995, the Planning Commission of the City of Fontana conducted a public hearing on ~~Fontana Gateway~~ Specific Plan Amendment #2 (SPA #95-03) and received testimony from the public; and

WHEREAS, the pages of the ~~Fontana Gateway~~ Specific Plan shall be amended to read as indicated on the attachment.

NOW, THEREFORE, in consideration of the evidence received at the public hearing, and for the reasons discussed by the Commissioners at said hearing, the Planning Commission hereby recommends the City Council adopt an ordinance approving the Negative Declaration, and the ~~Fontana Gateway~~ Specific Plan Amendment #2 (SPA #95-03).

APPROVED AND ADOPTED this 28th day of August 1995.

  
\_\_\_\_\_  
Chairperson, Philip Cothran

**AN ORDINANCE OF THE CITY OF FONTANA AMENDING  
THE FONTANA GATEWAY SPECIFIC PLAN**

THE CITY COUNCIL OF THE CITY OF FONTANA DOES ORDAIN AS FOLLOWS:

**WHEREAS**, the Planning Commission of the City of Fontana, at a meeting duly noticed and conducted on August 28, 1995, considered Fontana Gateway Specific Plan Amendment #2 (SPA #95-03) to change the Southern California Edison easement area from its present designation of SCE easement to General Industrial and conditionally permit Water Reclamation Facilities; and

**WHEREAS**, notice of the public hearing concerning the subject project was given pursuant to the Government Code by publication in the Herald News, a newspaper of general circulation within the City on August 17, 1995; and

**WHEREAS**, all of the notices required by statute or the City Municipal Code have been given as required;

**WHEREAS**, the City Council has determined a Negative Declaration is appropriate for this project.

**NOW, THEREFORE**, be it resolved, determined, and ordered by the City Council of the City of Fontana, approves the Negative Declaration and amends the Fontana Gateway Specific Plan as on the attachment.

**SECTION 2.** This Ordinance shall take effect thirty (30) days after the date of the adoption and prior to the expiration of fifteen (15) days from passage thereof, shall be published by the City Clerk at least once in Herald News, a newspaper of general circulation, published and circulated in the City of Fontana, and thenceforth and thereafter the same shall be in full force and effect.

**APPROVED AND ADOPTED** this 3rd day of October, 1995.

**APPROVED AS TO LEGAL FORM:**

  
City Attorney

X  
A  
5/6/97

**ORDINANCE NO. 1215**

**AN ORDINANCE OF THE CITY OF FONTANA  
APPROVING SPECIFIC PLAN AMENDMENT NO.  
97-02 (AMENDMENT NO. 3 TO THE FONTANA  
GATEWAY SPECIFIC PLAN), AMENDING  
VARIOUS SECTIONS REGARDING  
LANDSCAPING, SCREENING, AND OUTDOOR  
STORAGE**

**WHEREAS**, the Planning Commission of the City of Fontana, at a meeting duly noticed and conducted on February 24, 1997, recommended that the City Council approve Specific Plan Amendment No. 97-02 (Amendment No. 3 to the Fontana Gateway Specific Plan) as described in Resolution No. PC 97-03; and

**WHEREAS**, on April 15, 1997 the City Council held a noticed public hearing concerning Specific Plan Amendment No. 97-02 (Amendment No. 3 to the Fontana Gateway Specific Plan); and

**WHEREAS**, the Fontana Gateway Specific Plan is hereby amended pursuant to the attached exhibits in which items identified with strike overs will be removed and items in bold, underlined and italicized will be added.

**NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF FONTANA AS FOLLOWS:**

**Section 1.** The adoption of the Negative Declaration of Environmental Impact and the State of California Department of Fish and Game de Minimis Impact Finding adequately addresses the environmental impact of Specific Plan Amendment No. 97-02.

**Section 2.** Section 4.2.14 of the Fontana Gateway Specific Plan is hereby amended and attached as **Exhibit "A"** and incorporated herein by reference.

**Section 3.** Section 4.2.3 of the Fontana Gateway Specific Plan is hereby amended and attached as **Exhibit "B"** and incorporated herein by reference.

**Section 4.** Section 4.6.1. of the Fontana Gateway Specific Plan is hereby amended and attached as **Exhibit "C"** and incorporated herein by reference.

**Section 5.** Section 4.8.2. of the Fontana Gateway Specific Plan is hereby amended and attached as **Exhibit "D"** and incorporated herein by reference

4.2.14

Hazardous Waste Management Plan

All uses and/or operations shall comply with the Hazardous Waste Management Plan per Section ~~33-02~~ 9-1 of the Code of the City of Fontana.

4.2.3 Outdoor Storage (See Figures 13-15, 21-15)

- All materials, supplies, equipment, and operating trucks shall be stored within an enclosed building or a screened area. *Areas used for the outdoor storage of vehicles, equipment and/or building materials (raw or finished) are permitted to use compacted slag, gravel or such other material deemed suitable by the Community Development Director.*

4.8.2 Loading Facilities (See Figure 13-15, 21-25)

Loading facilities shall be screened from the public **view right-of-way**.

This space is for the  
County Clerk's stamp

## PROOF OF PUBLICATION (2015.5 C.C.P.)

State of California  
County of San Bernardino

I am a citizen of the United States and a resident of the County aforesaid; I am over the age of eighteen years, and not a party to or interested in the above entitled matter. I am the principal clerk of the printer of the Fontana Herald News, a newspaper of general circulation, printed and published every Thursday in the City of Fontana, County of San Bernardino, and which newspaper has been adjudged a newspaper of general circulation by the Superior Court of the State of California, in and for the County of San Bernardino, under the date of March 15, 1955, Case Number 73171, and that the notice, of which the annexed is a printed copy (set in type not small than nonpareil), has been published in each regular and entire issue of said newspaper and not in any supplement thereof on the following dates, To-wit: April 17, 1997.

I certify (or declare) under penalty of perjury that the foregoing is true and correct.

Dated at Fontana, California, this 17th day of April 1997.

  
Signature

### City of Fontana Summary of Proposed Ord. No. 1215 PO# 96-25428

CITY OF FONTANA  
FONTANA, CALIFORNIA

SUMMARY OF PROPOSED  
ORDINANCE  
NO. 1215

NOTICE IS HEREBY GIVEN that the City Council of the City of Fontana, at a Regular Meeting scheduled on Tuesday May 6, 1997, in the City Hall Council Chambers, 8353 Sierra Avenue, said Council will consider the adoption of Ordinance No. 1215, approving Specific Plan Amendment No. 97-02 (Amendment No. 3 to the Fontana Gateway Specific Plan) amending various sections regarding landscaping, screening and outdoor storage. A certified copy of the full text of the proposed ordinance is available in the office of the City Clerk of the City of Fontana, 8353 Sierra Avenue, Fontana, California 92335.

CITY COUNCIL OF THE CITY  
OF FONTANA  
David R. Eshleman, Mayor  
Kathy Montoya  
City Clerk

Publish: April 17, 1997

**FONTANA HERALD NEWS**  
16920 Spring Street  
P.O. Box 549  
Fontana, California 92334  
Phone (909) 822-2231      Fax (909) 355-9358

This space is for the  
County Clerk's stamp

## PROOF OF PUBLICATION (2015.5 C.C.P.)

State of California  
County of San Bernardino

I am a citizen of the United States and a resident of the County aforesaid; I am over the age of eighteen years, and not a party to or interested in the above entitled matter. I am the principal clerk of the printer of the Fontana Herald News, a newspaper of general circulation, printed and published every Thursday in the City of Fontana, County of San Bernardino, and which newspaper has been adjudged a newspaper of general circulation by the Superior Court of the State of California, in and for the County of San Bernardino, under the date of March 15, 1955, Case Number 73171, and that the notice, of which the annexed is a printed copy (set in type not small than nonpareil), has been published in each regular and entire issue of said newspaper and not in any supplement thereof on the following dates, To-wit: May 8, 1997.

I certify (or declare) under penalty of perjury that the foregoing is true and correct.

Dated at Fontana, California, this 8th day of May 1997.



Signature

### City of Fontana Summary of Proposed Ordinance #1215 PO# 96-25428

CITY OF FONTANA  
FONTANA, CALIFORNIA

#### SUMMARY OF PROPOSED ORDINANCE NO. 1215

NOTICE IS HEREBY GIVEN that the City Council of the City of Fontana at a Regular Meeting scheduled on Tuesday, May 6, 1997, in the City Hall Council Chambers, 8353 Sierra Avenue, said Council adopted of Ordinance 1215 approving Specific Plan Amendment No. 97-02 (Amendment No. 3 to the Fontana Gateway Specific Plan) amending various sections regarding landscaping, screening and outdoor storage.

A certified copy of the full text of the ordinance is available in the office of the City Clerk of the City of Fontana, 8353 Sierra Avenue, Fontana, California 92335.

**AYES:** Mayor Eshleman,  
Council Members Hooper,  
Mancha, Nuami, Roberts

**NOES:** None

**ABSENT:** None

CITY COUNCIL OF THE CITY  
OF FONTANA  
David R. Eshleman, Mayor

Kathy Montoya  
City Clerk

Publish: May 8, 1997

**FONTANA HERALD NEWS**

16920 Spring Street  
P.O. Box 549  
Fontana, California 92334  
Phone (909) 822-2231      Fax (909) 355-9358

## **1.0 INTRODUCTION**

### **1.1 BACKGROUND**

The Fontana Gateway Specific Plan is for a partially developed 755 gross acre industrial area situated in the unincorporated area of San Bernardino County.

The site is located one mile east of I-15 freeway at the Etiwanda on/off ramp of the I-10 freeway. It is comprised of separately owned large parcels with an approximately 185 acre parcel held by FG Associates.

The City initiated the Specific Plan Study in order to formulate a public-private industrial development program for the area which will be mutually beneficial to the City of Fontana and the area landowners. The specific objectives of the development program are to:

- o provide economic development opportunities,
- o ensure long-term fiscal and financial benefits to the City,
- o increase the taxable base of the City,
- o provide orderly and feasible industrial development, and
- o upgrade the gateway image of the City.

It is the intention of the City of Fontana to annex the property contained within the Fontana Gateway Specific Plan Area. This action is necessary before the Fontana Redevelopment Agency (FRA) can initiate any redevelopment activity within the subject area.

An existing agreement between the owners (FG Associates) of Sierra Industrial Park (which consists of approximately 185 acres of land bounded by Slover Avenue, Mulberry Avenue, the San Sevaine Flood Control Channel and the flood retention basin) and the Fontana Redevelopment Agency (FRA) illustrates the relationship between these two parties regarding the development of Sierra Industrial Park. The FRA has pledged to provide redevelopment financial assistance to property owners in exchange for assistance in the annexation of their property and a share of the profits resulting from its eventual development.

### **1.2 PURPOSE**

The City of Fontana has requested the preparation of a specific plan for the Fontana Gateway Planning Area prior to annexation. The purpose of the Specific plan is to ensure responsible development of this area through development and design regulations which are based upon a comprehensive land use planning approach.

The Specific Plan contains the necessary detailed development and design regulations, and implementation programs which will provide for an orderly development of the site. The Specific Plan, when adopted by city legislative action, will serve as a regulatory document through implementation of the development criteria set forth. Future development plans, tentative parcel maps and/or tract maps submitted within the planning area, must be consistent with the Specific Plan's Development and Design Regulations in order to obtain approval.

This Specific Plan is intended to encompass all discretionary approvals as may

be required by the City, and other regulatory agencies, including the approval of both "Master Design Plan(s)" and "Master Tentative Tract(s)".

#### 1.3 AUTHORITY

The Fontana Gateway Specific Plan is established through the authority granted to the City of Fontana through the California Government Code Section 65507, which establishes the authority for a legislative body to adopt, by ordinance, a specific plan when it is in the public interest to do so. The Planning Commission must hold workshops and public hearings before they can recommend the adoption of a specific plan. The City Council may then adopt a specific plan by ordinance. After it is adopted, a specific plan has an effect similar to the local Zoning Ordinance for the project site. The Fontana Gateway Specific Plan applies only to that property which will subsequently be annexed to the City of Fontana, known as the Fontana Gateway Planning Area. The Land Use Master Plan delineates the property which is subject to this Specific Plan. In addition, the California Government Code provides that a specific plan may include the following:

**Land Use:** An illustration of the location of housing, business, industry, open space, recreation facilities, educational facilities, churches and related religious facilities, public buildings and grounds, solid and liquid waste disposal facilities, together with regulations establishing height, bulk and setback lines.

**Circulation:** The plan may include the location of streets, road standards, maintenance provisions, and other transportation needs. This plan may include standards for both private and public facilities.

**Design:** The City, through the specific plan process, may provide basic design criteria to enhance local image.

#### 1.4 CALIFORNIA ENVIRONMENTAL QUALITY ACT COMPLIANCE

The Fontana Gateway Specific Plan Environmental Impact Report (EIR) assesses the environmental impacts of the proposed development (i.e., site plans, subdivision maps, etc.) in all areas of the specific plan, including: land use, circulation, infrastructure, implementation mechanisms, design guidelines, and policies of the specific plan. The Appendix of this report contains the EIR which includes: an introduction, description of existing conditions, assessment of environmental impacts, identification of mitigation measures and evaluation of project alternatives.

## 2.0

# SPECIFIC PLAN OVERVIEW

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

---

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## **2.0 SPECIFIC PLAN OVERVIEW**

### **2.1 INTRODUCTION**

The methodology for the preparation of the Fontana Gateway Specific Plan was divided into three basic phases, each phase building upon the findings of the previous phase. The City staff worked in coordination with the consultant team to insure that tangible decisions would be reached at the conclusion of each work phase. Workshops have been conducted to insure involvement of both the Planning Commission and the City Council.

The first phase of this project included an analysis of: local issues, problems and opportunities; physical and environmental characteristics of the site; economic development and market potential; and, the limitation of existing, basic, infrastructure facilities. This assessment, and coordination with City staff, provided a firm basis for formulating development concepts for the site.

The second phase of work involved the development of alternative land uses for the study area. These alternatives explored various ways that both the City and developer objectives could be transplanted into more refined land uses, design concepts and development opportunities for the property.

The final phase of work involved the refinement of the preferred alternative concept through more detailed planning and design of development and design controls.

The refined plan contains an implementation program to determine how the basic services necessary to support development can be provided. The purpose of this refined plan is to ensure that the Fontana Gateway Specific Plan is realistic, that it can be built and operated within the capabilities of responsible public agencies to serve it and that it furthers the goals and policies of the community.

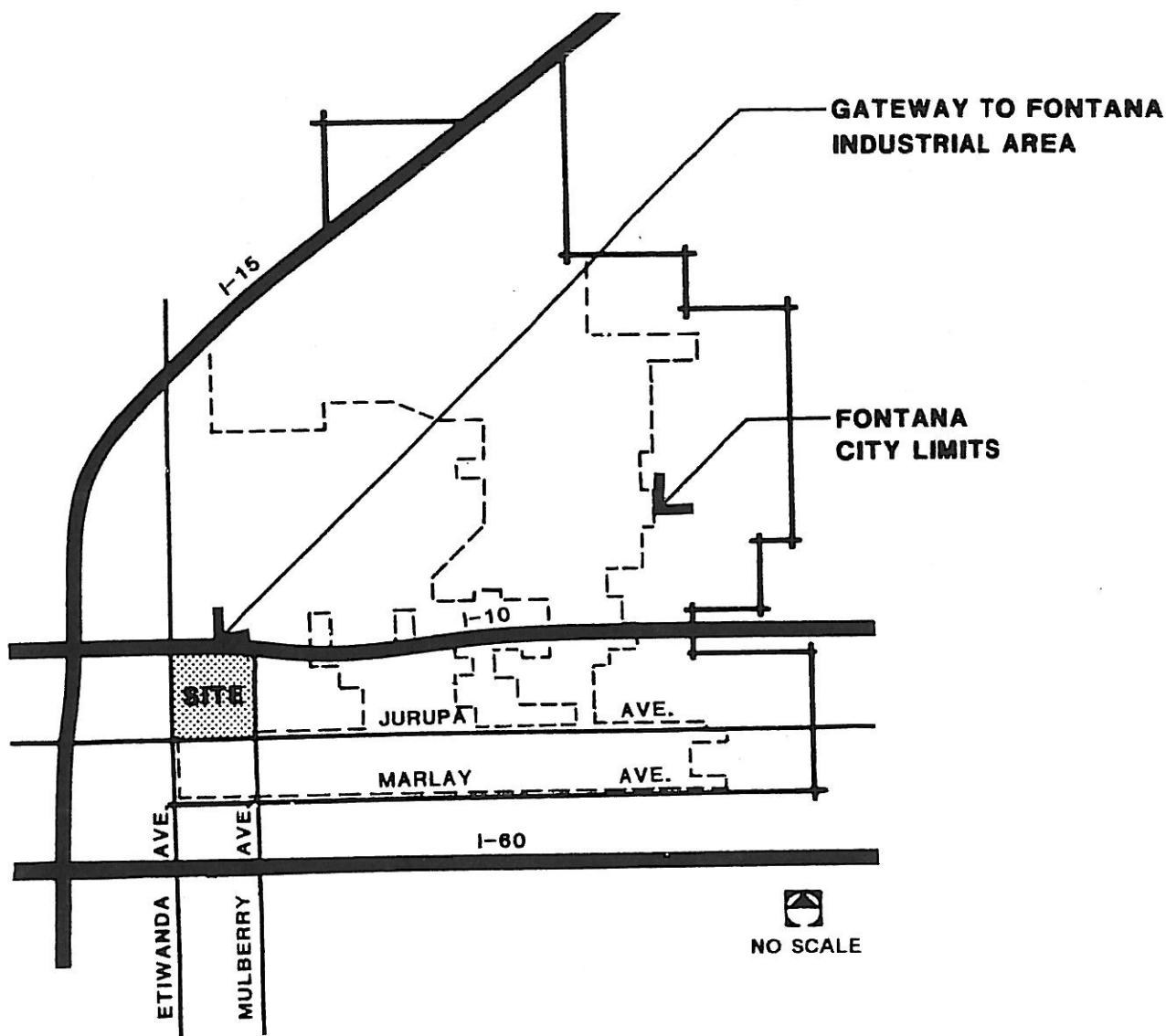
### **2.2 LOCATION**

The Fontana Gateway Specific Plan Area is located in the unincorporated area of San Bernardino County, adjacent to Fontana's southwest gateway corridor. The site is bounded on the north by the I-10 freeway, the east by Mulberry Avenue, the south by the planned extension of Jurupa Avenue, and the west by Etiwanda Avenue.

The site is situated regionally between the heavily populated and developed Los Angeles basin and the rapidly growing suburban San Bernardino/Riverside basin, referred to as the 'Inland Empire'. Urban development seeking lower land cost and density is relocating rapidly from the central Los Angeles basin. This wave of development is clearly evident from area statistics of population, housing, employment, and industrial growth. The entire 'Inland Empire' and particularly the eastern edge where the site is situated, is in a strong, long term process of urbanization.

### **2.3 OWNERSHIP (See Figure 4)**

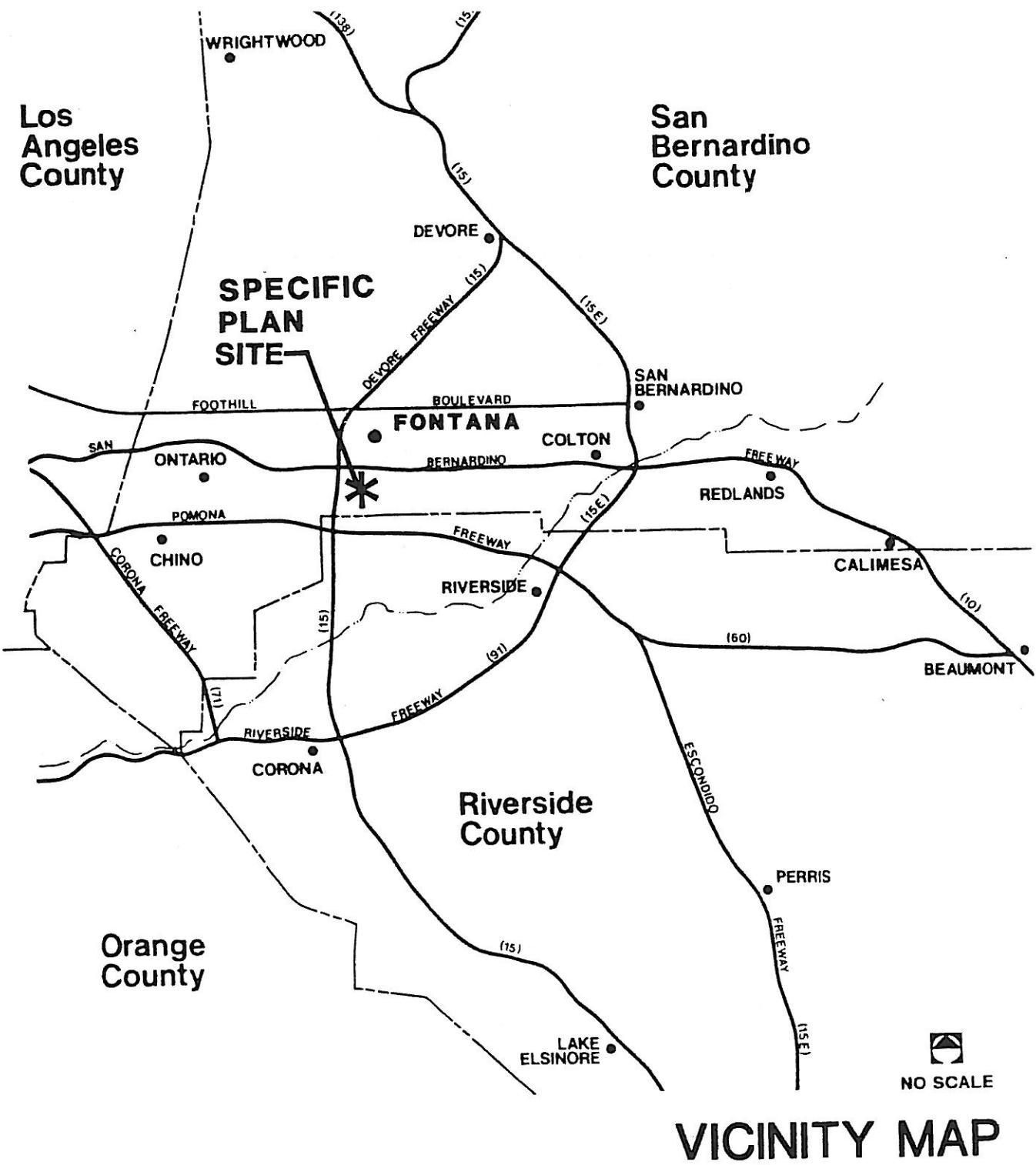
This site is composed of separately owned parcels of various large sizes with an approximately 185 acre parcel held by FG Associates.



## LOCATION MAP

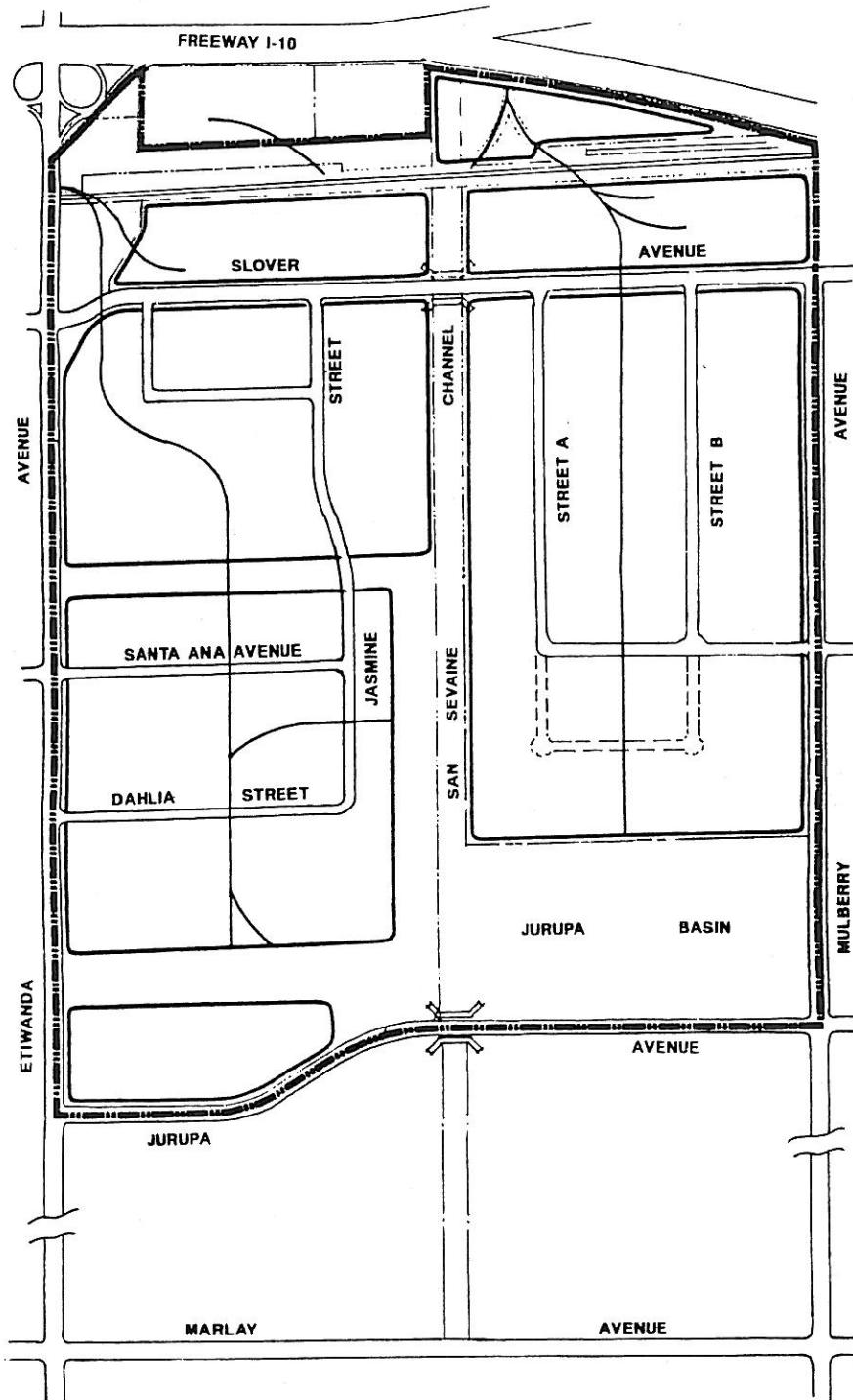
# CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS

## STUDY AREA MAP

NO SCALE

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## 2.4 PROJECT DESCRIPTION

The Fontana Gateway Specific Plan is primarily a planned industrial land use encompassing approximately 755 acres in the urbanizing area of southwest Fontana. The project would create a major new employment center, providing jobs for existing city residents and new residents of nearby planned residential communities.

The proposed industrial land use is comprised of mixed industrial land uses including support service commercial. Included within the mixed industrial land use category is the provision for incubator uses. These incubator uses are small, start-up industrial businesses, contained within an enclave of multi-tenant facilities ranging in individual size from 2,000 to 10,000 square feet.

The support service commercial and office land use component of the project consists of commercial activities which will support the industrial users within the Specific Plan area and provide an incentive for potential industrial users to locate in the Specific Plan area.

## 2.5 EXISTING SETTING

The study area is presently designated by the San Bernardino County and Fontana General Plan as Industrial. The County General Plan designates lands east of the study area across Mulberry as Industrial with the exception of lands directly east of the Jurupa Basin, which are designated Residential at a density maximum of one dwelling unit per acre. The area east of the study area is currently zoned by the County primarily as M-1. This has resulted in an uncontrolled mixture of residential and industrial uses caused by County M-1 zoning which has allowed incompatible uses to be constructed on adjacent parcels.

The Fontana General Plan designates the area to the east of the Fontana Gateway Specific Plan site as Residential (0-2 du/ac).

The area south of the project site is within the City of Fontana, and is referred to in the Southwest Industrial Park Specific Plan. Land to the west of the Fontana Gateway Specific Plan Area is within the City of Ontario. The Ontario General Plan designates lands west of Etiwanda Avenue as General Industrial.

## 2.6 ISSUES AND CONSTRAINTS (See Figure 5)

The Fontana Gateway Specific Plan was developed through a comprehensive process of data collection, issues analysis, definition of alternative land use solutions, and refinement of the preferred design concept.

In the early stages of planning, a number of issues, constraints, and opportunities for development were identified. The resolution of these concerns has been the main priority in developing the plan. If the plan is to be successful in meeting the City's objectives, it must address the local issues and constraints, and it should capitalize on the area's opportunities. The major issues addressed in the plan are listed below.

#### 2.6.1 Land Use Issues

- o Encouragement of a healthy business environment.
- o Project compatibility with existing and planned uses.
- o Numerous easements, i.e., Edison, flood control, railroad.
- o Mixing heavy industrial uses with other, lower intensity activities.
- o The opportunity for better zoning regulation through the specific plan mechanism.
- o Development Phasing
  - Land availability
  - Regional growth (development pressure)
  - Accessibility
  - Adjacent activity

#### 2.6.2 Visual Character Issues

- o The aesthetic quality of proposed development activities when viewed from:
  - Slover, Mulberry, Jurupa, Etiwanda
  - the I-10 corridor
- o The development and implementation of landscaping, streetscape and architectural design regulations.
- o Overhead power lines.
- o Gateway entrance into Fontana.
- o Existing buildings and improvements.
- o San Sevaine Creek.
- o Freeway landscaping/buffers.

#### 2.6.3 Traffic Issues

- o Access to and from I-10 for trucks and related industrial traffic.
- o Provision of all-weather crossings at Slover and Jurupa Avenue.
- o Proposed Jurupa Avenue realignment.
- o Improvement phasing, including traffic control and access provisions, for new developments.

#### 2.6.4 Infrastructure Issues

- o The provision of sewer, drainage, water facilities, streets and fire protection.
- o The coordination of onsite and off-site improvements with the City and special district master plans.
- o The phasing of infrastructure improvements.
- o The impact of proposed improvement costs.
- o Overall improvement financing.

#### 2.6.5 Environmental Issues

- o Conversion of agricultural land to industrial land uses.
- o Development of performance standards to control noise, glare, odors, dust and other potential nuisances to adjacent areas.
- o Necessary public facility improvements to mitigate utility and service impacts.
- o Air quality.

#### 2.6.6 Market, Economic and Fiscal Issues

- o Inherent market opportunities involving industrial uses.
- o Overall job creation.
- o Service commercial opportunities.
- o Potential for incubator uses.

#### 2.7 DEVELOPMENT OPPORTUNITIES

The formulation of the Fontana Gateway Specific Plan has required an interactive process between the City, Redevelopment Agency, (FRA), and property owners. The results of the market analysis have assisted in specifying area land uses. A Fiscal Analysis has provided feedback to shape the scope and timing of the proposed infrastructure improvements. This specific plan has outlined the following opportunities:

- o Project a reasonable and realistic development period and land use plan for the study area.
- o Formulate an infrastructure improvement plan which will adequately support the proposed development.
- o Provide positive fiscal programs without creating financial liabilities to the City's Redevelopment Agency (FRA).

- o Create an environment for the profitable development of the Specific Plan Area to the area's landowners.

## 3.0

# SPECIFIC PLAN COMPONENTS

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

---

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

### **3.0 SPECIFIC PLAN COMPONENTS**

#### **3.1 INTRODUCTION**

**This section of the Specific Plan includes various component plans which define the overall framework for development of the Fontana Gateway Specific Plan. Major components of the plan include:**

- o **Land Use Master Plan (3.3.1)**
- o **Circulation Master Plan (3.3.2)**
- o **Landscape and Signage Plan (3.3.3)**
- o **Implementation Strategy (3.3.4)**
- o **Development and Design Regulations (4.0)**

**These component plans establish specific standards and requirements to which individual development projects within the specific plan area must conform.**

**Please note that additional components related to Infrastructure Master Plans and development phasing can be found in Chapter 5.0 and Chapter 6.0 of this document.**

#### **3.2 DESIGN OBJECTIVES**

**Specifically, the Fontana Gateway Specific Plan is designed to achieve the following objectives:**

- o **Create a setting within which industrial and service commercial facilities can locate.**
- o **Establish development and design regulations that will insure a quality, integrated development that will endure over time.**
- o **Provide a land use and phasing program that will allow development to occur in an orderly, integrated manner and yet will have the flexibility to respond to changes in market demand.**
- o **Respond to the growing pressures of industrial expansion in the Fontana area.**
- o **Provide an expanding industrial economic base for the City of Fontana.**
- o **Provide employment opportunities.**
- o **Create a landscape and streetscape that will enhance the aesthetic and visual quality of the area**
- o **Provide a planned infrastructure, utility and service program that can meet the expanding needs of the project as they evolve, in an efficient and cost effective manner.**
- o **Utilize landscape buffers to minimize disturbance from the adjacent land uses.**

ORDINANCE NO. 1166

AN ORDINANCE OF THE CITY OF FONTANA AMENDING  
THE FONTANA GATEWAY SPECIFIC PLAN

THE CITY COUNCIL OF THE CITY OF FONTANA DOES ORDAIN AS FOLLOWS:

WHEREAS, the Planning Commission of the City of Fontana, at a meeting duly noticed and conducted on August 28, 1995, considered Fontana Gateway Specific Plan Amendment #2 (SPA #95-03) to change the Southern California Edison easement area from its present designation of SCE easement to General Industrial and conditionally permit Water Reclamation Facilities; and

WHEREAS, notice of the public hearing concerning the subject project was given pursuant to the Government Code by publication in the Herald News, a newspaper of general circulation within the City on August 17, 1995; and

WHEREAS, all of the notices required by statute or the City Municipal Code have been given as required;

WHEREAS, the City Council has determined a Negative Declaration is appropriate for this project.

NOW, THEREFORE, be it resolved, determined, and ordered by the City Council of the City of Fontana, approves the Negative Declaration and amends the Fontana Gateway Specific Plan as on the attachment.

**SECTION 2.** This Ordinance shall take effect thirty (30) days after the date of the adoption and prior to the expiration of fifteen (15) days from passage thereof, shall be published by the City Clerk at least once in Herald News, a newspaper of general circulation, published and circulated in the City of Fontana, and thenceforth and thereafter the same shall be in full force and effect.

APPROVED AND ADOPTED this 3rd day of October, 1995.

APPROVED AS TO LEGAL FORM:

/s/ Clark Alsop  
City Attorney

Ordinance No. 1166

I, Kathy Montoya, City Clerk of the City of Fontana and Ex-Officio Clerk of the City Council, do hereby certify that the foregoing Ordinance is the actual Ordinance introduced at a regular meeting of said City Council on the 19th day of September, 1995, and was finally passed and adopted not less than five (5) days thereafter on the 3rd day of October, 1995, by the following vote:

YES: Mayor Eshleman, Council Memebers Coleman, Hooper, Roberts, Watson

NOES: None

ABSENT: None

/s/ Kathy Montoya  
City Clerk of the City of Fontana

/s/ David R. Eshleman  
Mayor of the City of Fontana

ATTEST:

/s/ Kathy Montoya  
City Clerk of the City of Fontana

### 3.3 SPECIFIC PLAN COMPONENTS

#### 3.3.1 Land Use Master Plan (See Figure 6)

The Land Use Master Plan illustrates the proposed land use plan for the Fontana Gateway Planning area.

The Land Use Master Plan designates the entire site for the development of general industrial uses, intended to accommodate a broad range of industrial users; including support service commercial which is intended to act as support for the industrial users. Industrial users will provide a strong economic base for the City as well as employment opportunities for its residents.

The plan is to provide the potential for a full mix for uses including incubator industrial facilities. The following table presents a statistical summary of the various land uses.

#### FONTANA GATEWAY INDUSTRIAL SPECIFIC PLAN

#### STATISTICAL SUMMARY

<u>LAND USE</u>	<u>ACREAGE<sup>(1)</sup></u>
	Total
Industrial	641
Edison ROW	0
Railroad	35
Flood Control	97
Streets	46
Total Acres	<u>796*</u>

(1) All acreage references are approximate.

\*The text of the specific plan states the total specific plan acreage is 755 acres. The listed individual land uses add up to 796 acres.

#### 3.3.2 Circulation Master Plan (See Figure 7)

The circulation component of the Fontana Gateway Specific Plan is intended to establish design standards for roadways within the planning area. The circulation master plan includes the following elements:

- o Existing and proposed alignments for arterial highways and

### 3.3 SPECIFIC PLAN COMPONENTS

#### 3.3.1 Land Use Master Plan (See Figure 6)

The Land Use Master Plan illustrates the proposed land use plan for the Fontana Gateway Planning area.

The Land Use Master Plan designates the entire site for the development of general industrial uses, intended to accommodate a broad range of industrial users; including support service commercial which is intended to act as support for the industrial users. Industrial users will provide a strong economic base for the City as well as employment opportunities for its residents.

The plan is to provide the potential for a full mix for uses including incubator industrial facilities. The following table presents a statistical summary of the various land uses.

#### FONTANA GATEWAY INDUSTRIAL SPECIFIC PLAN

#### STATISTICAL SUMMARY

<u>LAND USE</u>	<u>ACREAGE<sup>(1)</sup></u>
	Total
Industrial	572 641
Edison ROW	69 0
Railroad	35
Flood Control	97
Streets	46
Total Acres	755 (796) <sup>*</sup>

(1) All acreage references are approximate.

#### 3.3.2 Circulation Master Plan (See Figure 7)

The circulation component of the Fontana Gateway Specific Plan is intended to establish design standards for roadways within the planning area. The circulation master plan includes the following elements:

- Existing and proposed alignments for arterial highways and

<sup>\*</sup> The text of the specific plan states the total specific plan acreage is 755 acres. The listed individual land uses add up to 796 acres.

collector streets within the Fontana Gateway Specific Plan Area.

- o Rights-of-way and cross-sections for arterial highways, collector streets and local streets.

The circulation element of the City's General Plan establishes the basis for highway planning throughout the City. Considering the recommendations of the City's circulation plan as it is being amended by General Plan Amendment No. 86-4, a detailed traffic circulation study was prepared for the Fontana Gateway Specific Plan. The traffic study addressed the following:

- o Estimated trip generation for each designated land use on the Land Use Plan and for surrounding areas, with the distribution of these trips on the street system.
- o Estimated future traffic volumes for all roads shown in the specific plan, based on the distribution and intensity of uses in the Land Use Plan.
- o Recommended street classifications, rights-of-way, and cross-sections.
- o Estimated future volume to capacity ratios and intersection capacity utilization rates for streets and intersections.

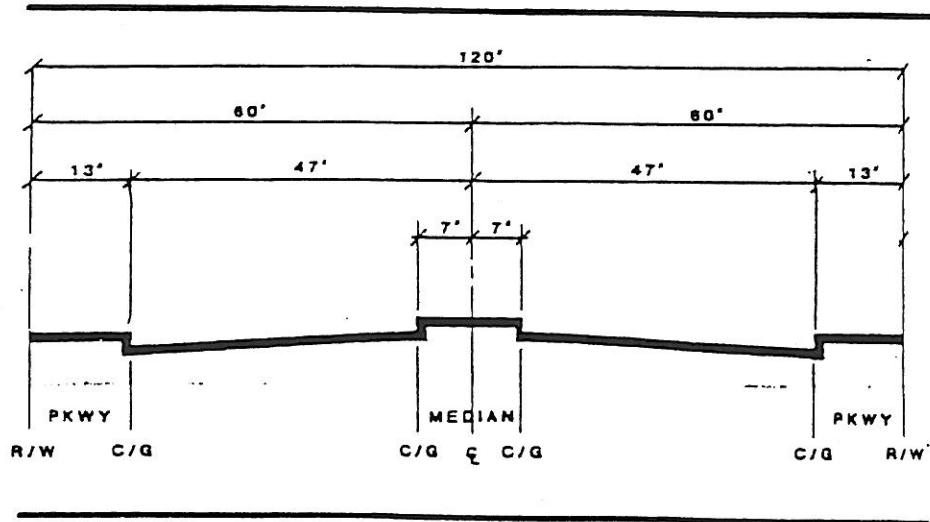
#### **3.3.2.1 Street Improvements**

The Circulation Plan for the Fontana Gateway Specific Plan identifies the following roadways. Please note that the cross sections which follow illustrate proposed, ultimate ROW dimensions.

### Etiwanda Avenue

Within the Specific Plan area, Etiwanda Avenue is master planned to be a 6 lane roadway. However, due to intermittent development, various roadway cross sections exist between Interstate 10 and Jurupa Avenue. The following sections illustrate ultimate pavement and right-of-way dimensions.

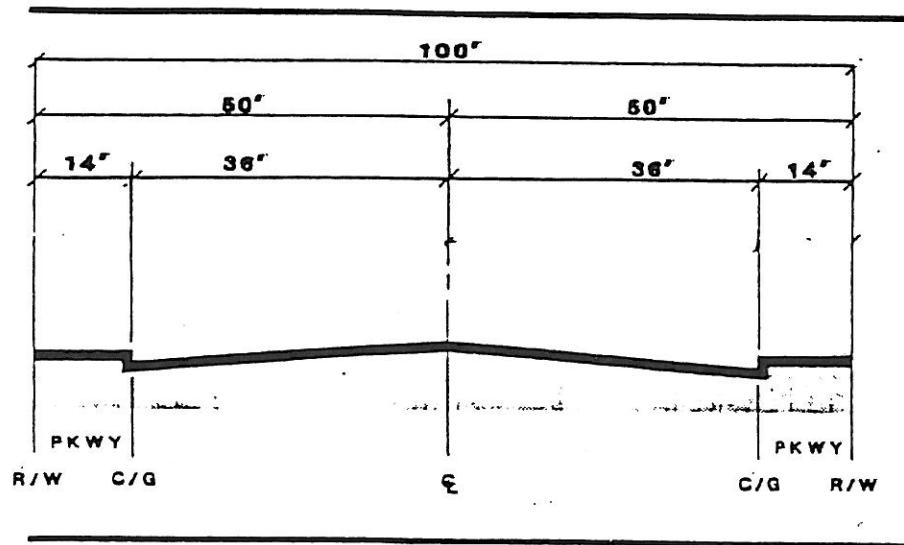
Existing traffic per day (1986) along Etiwanda Avenue is as follows: 9,900 vehicles north of Slover Avenue, 8,400 vehicles north of Jurupa Avenue and 6,700 vehicles south of Jurupa Avenue.



**ETIWANDA AVENUE - BETWEEN SLOVER AVENUE AND JURUPA AVENUE**

### Slover Avenue

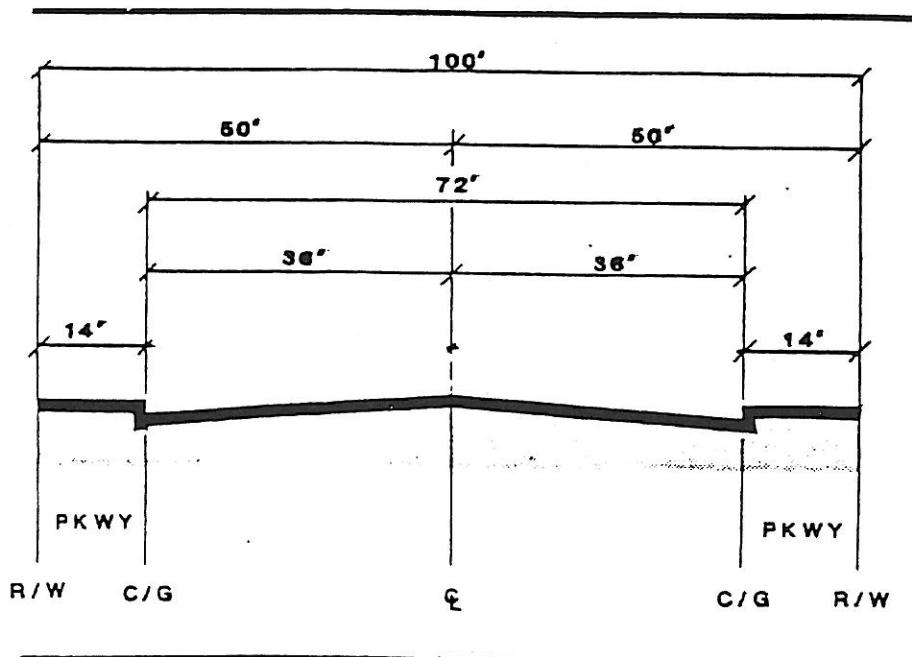
Within the Specific Plan area, Slover Avenue is master planned to be a major highway. Due to intermittent development existing pavement widths range from 26 to 46 feet. This provides for two lanes of traffic (one in each direction) with widening at the signalized intersection of Slover and Etiwanda Avenues. The following illustrations show typical future cross sections along with the proposed street sections indicating ultimate pavement and right-of-way dimensions. Daily traffic volumes (1986) range from 3,800 vehicles per day near Etiwanda Avenue to 3,300 vehicles per day near Mulberry Avenue.



**SLOVER AVENUE - BETWEEN MULBERRY AVENUE  
AND ETIWANDA AVENUE**

### Mulberry Avenue

South of Slover Avenue, Mulberry Avenue is planned as a major highway. Currently it is a two lane roadway with an average pavement width of 26 feet. Minor improvements have taken place between Slover Avenue and Jurupa Avenue. South of Jurupa Avenue, Mulberry Avenue is a three lane roadway (two northbound and one southbound) having an average pavement width of 48 feet. The following illustration shows the proposed street section indicating ultimate pavement and right-of-way dimensions. Daily traffic volumes (1986) range from 3,300 vehicles per day between Slover Avenue and Santa Ana Avenue, 3,000 vehicles per day between Santa Ana Avenue and Jurupa Avenue and 4,100 vehicles per day south of Jurupa Avenue.



**MULBERRY AVENUE - BETWEEN SLOVER AVENUE  
AND JURUPA AVENUE**

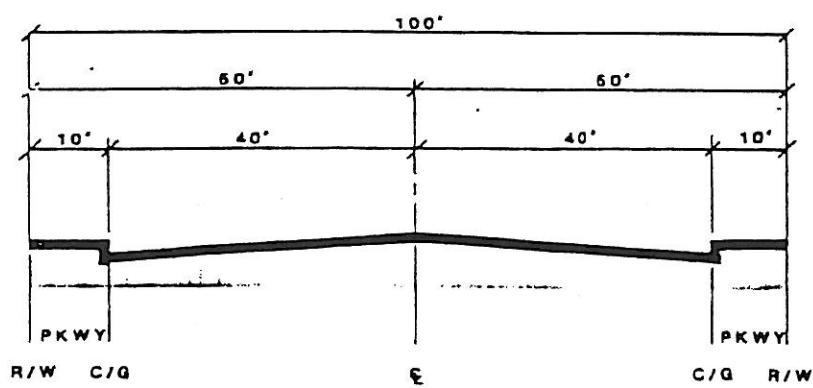
See note to 3.3.2.3 regarding adjustment of standard for Mulberry Avenue.

### Jurupa Avenue

East of Etiwanda Avenue to approximately 400 feet west of the San Sevaine Channel, Jurupa Avenue has been fully improved to an 80 foot pavement section. Daily traffic volumes along this section of roadway amount to 200 vehicles per day. The typical proposed cross section, depicted below, is a six (6) lane roadway, as planned for in the City's General Plan.

Jurupa Avenue, west of Mulberry Avenue to 400 feet west of the San Sevaine Channel has not been constructed and does not exist at the present time. The section below shows the proposed cross section for this section of roadway.

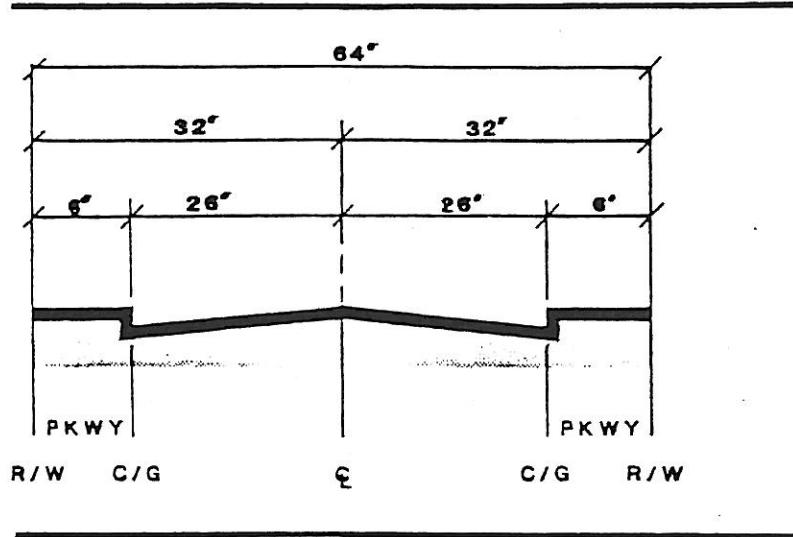
Jurupa Avenue, just east of Mulberry Avenue has been partially improved to an average pavement section of 46 feet. Daily traffic volumes (1986) along this section of roadway amount to 1,900 vehicles.



**JURUPA AVENUE - BETWEEN ETIWANDA AVENUE  
AND MULBERRY AVENUE**

### Industrial Collector

The interior streets in the Specific Plan area are planned as industrial collectors and have an ultimate cross section of a 52 foot pavement section within a 64 foot right-of-way. Santa Ana Avenue between Etiwanda Avenue and Jasmine Street, Jasmine Street between Santa Ana Avenue and Dahlia Street have all been constructed to their ultimate cross section. Santa Ana Avenue between A Street and Mulberry Avenue, Jasmine Street between Santa Ana Avenue and Slover Avenue, A Street, B Street, and C Street will be new streets within this area and will have a cross section as indicated below.



### **INDUSTRIAL COLLECTORS**

Note: A median at the intersection of B Street and Slover Avenue will be developed according to specifications determined by FG Associates.

The summary of future traffic volumes and Levels of Service are provided on the following tables.

### **3.3.2.2 Traffic Signal Installation**

**Traffic signals shall be installed by 1997 at the following intersections:**

- o **Slover Avenue – Mulberry Avenue (new)**
- o **Mulberry Avenue – Jurupa Avenue (new)**
- o **Etiwanda Avenue – Santa Ana Avenue (new)**
- o **Etiwanda Avenue – Airport Drive/Slover Avenue (modify existing)**
- o **Etiwanda Avenue – Jurupa Avenue (new)**

### **3.3.2.3 Roadway Improvements**

**It is estimated that the following streets will be upgraded to their ultimate section by the year 1997, to aid in moving traffic through the surrounding roadway circulation system.**

- o **Etiwanda Avenue – 120 feet right-of-way with two 40 foot pavement sections separated by a 14 foot median including left turn pockets at key intersections from Slover Avenue to Jurupa Avenue.**
- o **Slover Avenue – 100 foot right-of-way with 72 foot pavement section from Etiwanda Avenue to Mulberry Avenue.**
- o **Mulberry Avenue – 100 foot right-of-way with a 72 foot pavement section from Slover Avenue to Jurupa Avenue.**

**Note: Standard may be adjusted following review of existing rights-of-way and fiscal implications of acquisition. It is the intent of the City to establish standards which will allow construction of Mulberry Avenue during the first phase of development (see Section 3.3.4.2).**

- o **Jurupa Avenue – 100 foot right-of-way with an 80 foot pavement section from 2,000 feet east of Etiwanda Avenue to Mulberry Avenue.**
- o **Industrial Collector Roadway – 64 foot right-of-way with 52 foot pavement section.**
  - A Street
  - B Street (including a median at the intersection of B Street and Slover Avenue, according to specifications determined by FG Associates)
  - C Street
  - Jasmine Street (extension)
  - Santa Ana Avenue (extension)

## LIST OF FIGURES

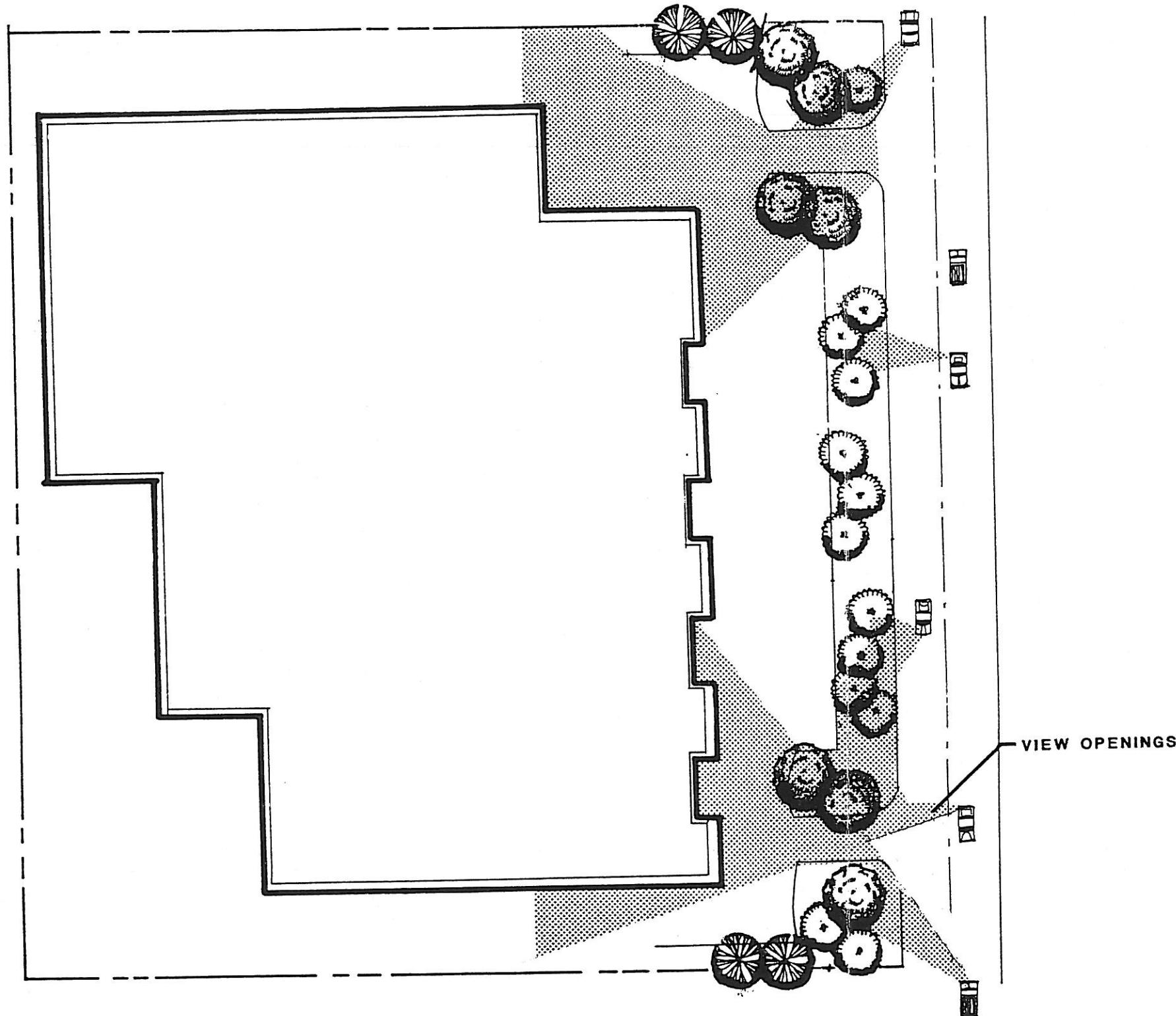
Figure No.	Figure Title	
1	<b>Location Map</b>	3a
2	<b>Vicinity Map</b>	3b
3	<b>Study Area</b>	3c
4	<b>Ownership Plan</b>	3d
5	<b>Opportunities and Constraints Plan</b>	4a
6	<b>Land Use Master Plan</b>	17a
7	<b>Circulation Plan</b>	17b
8	<b>Cumulative Traffic Volumes</b>	17d
9	<b>Trip Generation Analysis</b>	17e
10	<b>Signage Plan</b>	18a
11	<b>Landscape/Signage Plan</b>	21a
12	<b>Typical Streetscape</b>	22a
13	<b>Screening Methods</b>	22b
14	<b>Alternative Screening Techniques</b>	22c
15	<b>Line of Sight Analysis</b>	22d
16	<b>Parking and Landscape Setbacks: Etiwanda, Mulberry, Jurupa, Slover</b>	42a
17	<b>Building Setbacks: Etiwanda, Mulberry, Jurupa, Slover</b>	42b
18	<b>Parking and Landscape Setbacks: Industrial Collector</b>	42c
19	<b>Building Setbacks: Industrial Collector</b>	42d
20	<b>Prototype Architectural Treatments</b>	45a
21	<b>Loading Areas/Screenwall Criteria (Front Loaded)</b>	52a

**List of Figures (con't)**

22	<b>Loading Area/Screenwall Criteria (Front Loaded with Parking Area)</b>	52b
23	<b>Loading Area/Screenwall Criteria (Side Loaded)</b>	52c
24	<b>Loading Area/Screenwall Criteria (Side Loaded with Parking Area)</b>	52d
25	<b>Screening Methods</b>	52e
26	<b>Sewer System</b>	58a
27	<b>Sewer Quantity Generation Analysis</b>	58b
28	<b>Water System</b>	58c
29	<b>Water Quantity Generation Analysis</b>	60a
30	<b>Storm Drain System</b>	60b
31	<b>Drainage Area Flow Comparison</b>	60c
32	<b>Street Improvement Phasing</b>	61a
33	<b>Sewer System Phasing</b>	61b
34	<b>Water System Phasing</b>	61c
35	<b>Storm Drain Phasing</b>	61d
36	<b>Development Phasing Plan</b>	61e

## LIST OF TABLES

Table No.	Title	
1	Statistical Summary	9
2	Roadway Level of Service	17c
3	Street Tree Palette	20
4	Setback Schedule	40
5	On-site Plant Palette	42
6	Signage Criteria	55a

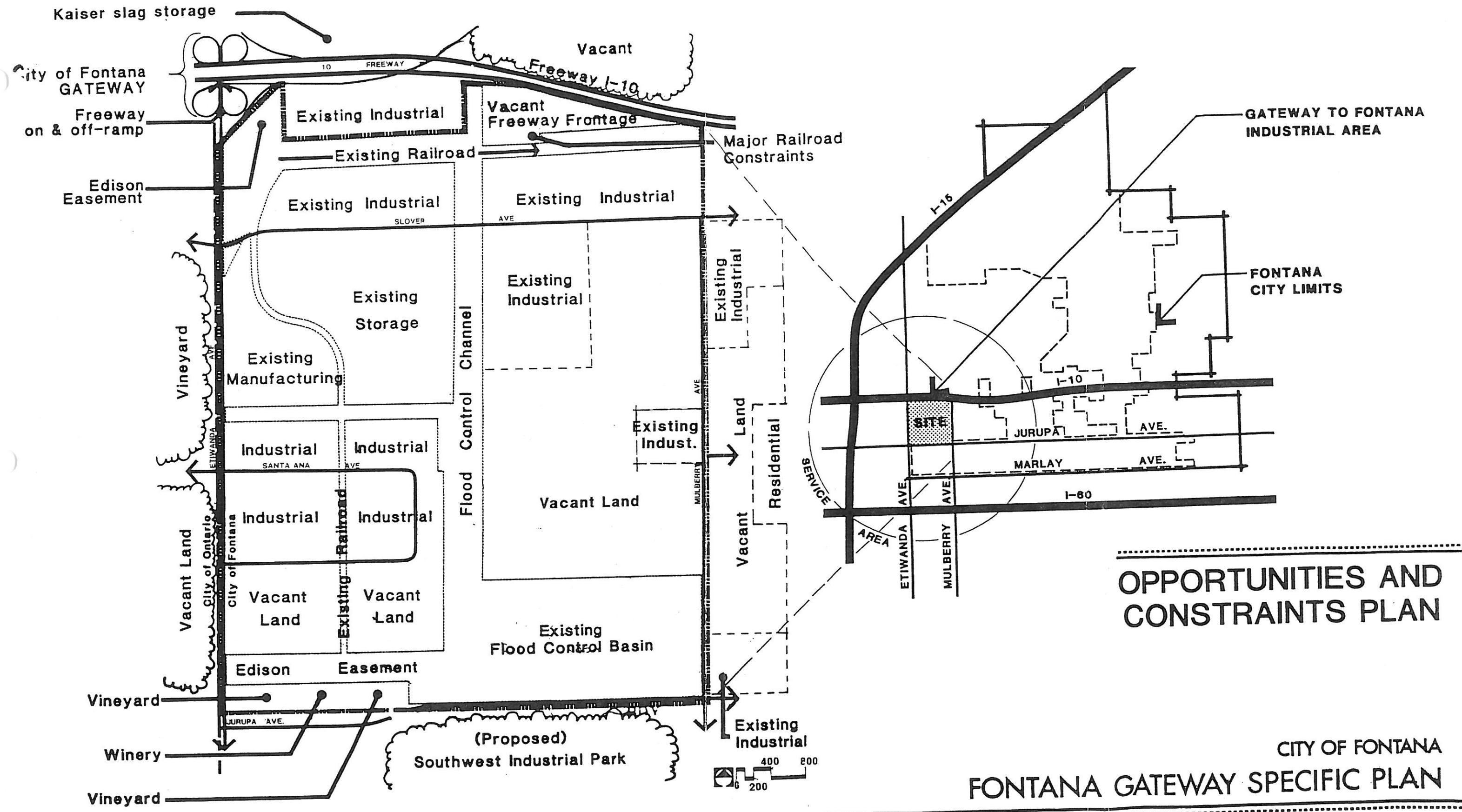


SCREENING TECHNIQUES AVAILABLE  
BERMING  
LANDSCAPING  
STRUCTURAL PLACEMENT  
WALLS

**LINE OF SIGHT  
ANALYSIS**

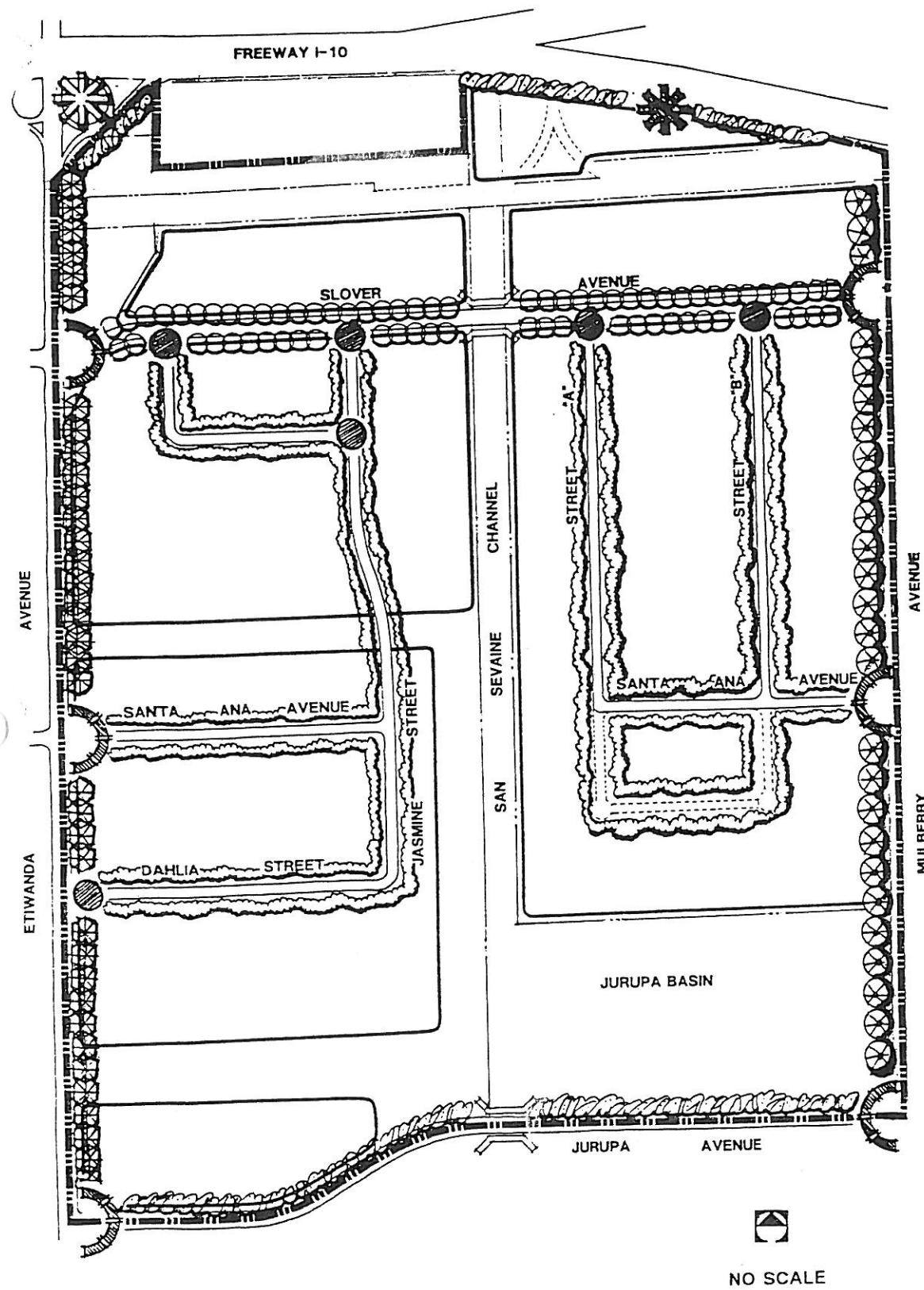
CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



# CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



## LEGEND

SYMBOL	STREET	PLANT MATERIAL
○○○○○	FREEWAY CORRIDOR JURUPA AVE. JURUPA MEDIAN	EUCALYPTUS MACULATA (SPOTTED GUM)
○○○○○	SLOVER AVE.	BRACHYCHITON POPULNEUS (BOTTLE NECK TREE)
○○○○○	ETIWANDA AVE.	PINUS EIDELDERICA
○○○○○	MULBERRY AVE.	PISTACHIA CHINENSIS
○○○○○	INTERIOR COLLECTORS	MELALEUCA LEUCAENDRA (CAJEPUT TREE)
○○○○○	MAJOR GATEWAY TOWER	
○○○○○	IDENTITY TOWER	
○○○○○	PROJECT DIRECTIONAL SIGN (PRIMARY ENTRANCE MONUMENTATION)	
○○○○○	PROJECT IDENTITY SIGN (SECONDARY ENTRANCE MONUMENTATION)	

NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

## LANDSCAPE/ SIGNAGE PLAN

### CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS - KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

### **3.3.2.4 Channel Crossings**

**All-weather crossings of the San Sevaine Channel are to be installed at the following locations.**

- o Slover Avenue**
- o Jurupa Avenue (per the Southridge Village Specific Plan)**

**The construction of these all-weather crossings should be coordinated with the proposed improvements to the San Sevaine Channel and Jurupa Basin.**

### **3.3.2.5 Regional Connectors**

#### **A. Interstate 10 Freeway**

**This eight lane facility is major east/west corridor in this vicinity and is located approximately 0.3 miles north of the Specific Plan area. Access is provided to the study area via the interchange provided at Etiwanda Avenue. Daily traffic volumes for 1985 ranged from 90,000 to 100,000 vehicles.**

#### **B. Interstate 15 Freeway**

**This eight lane facility is a major north/south corridor in this vicinity and is located approximately 1.5 miles west of the study area. An interchange has been provided at Jurupa Avenue. However, Jurupa Avenue is a noncontinuous roadway to and through the plan area, and access via Interstate 15 is not practical at the present time. When Jurupa Avenue is continued from Interstate 15 to Mulberry Avenue, direct access to the freeway will be provided. Daily traffic volumes for 1985 ranged from 50,000 to 55,000 vehicles.**

#### **C. State Route 60 Freeway**

**This six lane facility is a major east/west freeway for the area and is located approximately 2 miles south of the study area. Access is provided to the study area via the "Riverside Avenue/Etiwanda Avenue" interchange. Daily traffic volumes for 1985 ranged from 46,000 to 49,500 vehicles.**

### **3.3.2.6 Access**

**Limited access along Mulberry between Slover and Jurupa is intended to minimize traffic conflicts along Mulberry. All development along Mulberry shall indicate precise locations of all access points which shall be no closer than 300' of each other. In addition, the Circulation Master Plan indicates future access to parcels located between I-10 and the railroad tracks with access from Slover.**

**Reciprocal vehicular access shall be encouraged throughout the planning area, however, only a maximum of two (2) parcels may**

FUTURE LEVEL OF SERVICE  
ULTIMATE ROADWAY CROSS SECTION

Roadway	Section	Existing + Specific Plan			Existing + Specific Plan + Area Growth		
		VOL (1)	CAP (1)	LOS	VOL (1)	CAP (1)	LOS
Etiwanda Avenue	North of Slover	28.7	45.0	0.64/B	43.4	45.0	0.96/E
	South of Slover	19.4	75.0	0.26/A	36.1	75.0	0.48/B
	South of Santa Ana	16.3	75.0	0.22/A	31.8	75.0	0.43/B
	South of Jurupa	14.1	75.0	0.19/A	28.6	75.0	0.38/B
Slover Avenue	East of Etiwanda	18.7	38.0	0.49/B	25.5	38.0	0.67/B
	West of Mulberry	8.3	38.0	0.22/A	14.4	38.0	0.38/A
	East of Mulberry	6.1	38.0	0.16/A	13.6	38.0	0.36/A
Mulberry Avenue	South of Slover	6.1	30.0	0.20/A	12.9	30.0	0.43/B
	South of Santa Ana	7.9	30.0	0.26/A	14.5	30.0	0.48/B
	South of Jurupa	6.2	30.0	0.21/A	16.7	30.0	0.56/B
Jurupa Avenue	West of Etiwanda	6.1	75.0	0.08/A	24.1	75.0	0.32/A
	East of Etiwanda	9.7	38.0	0.26/A	19.5	38.0	0.52/B
	West of Mulberry	5.2	38.0	0.14/A	14.7	38.0	0.39/B
	East of Mulberry	3.1	38.0	0.08/A	18.1	38.0	0.48/B

VOL - Volume

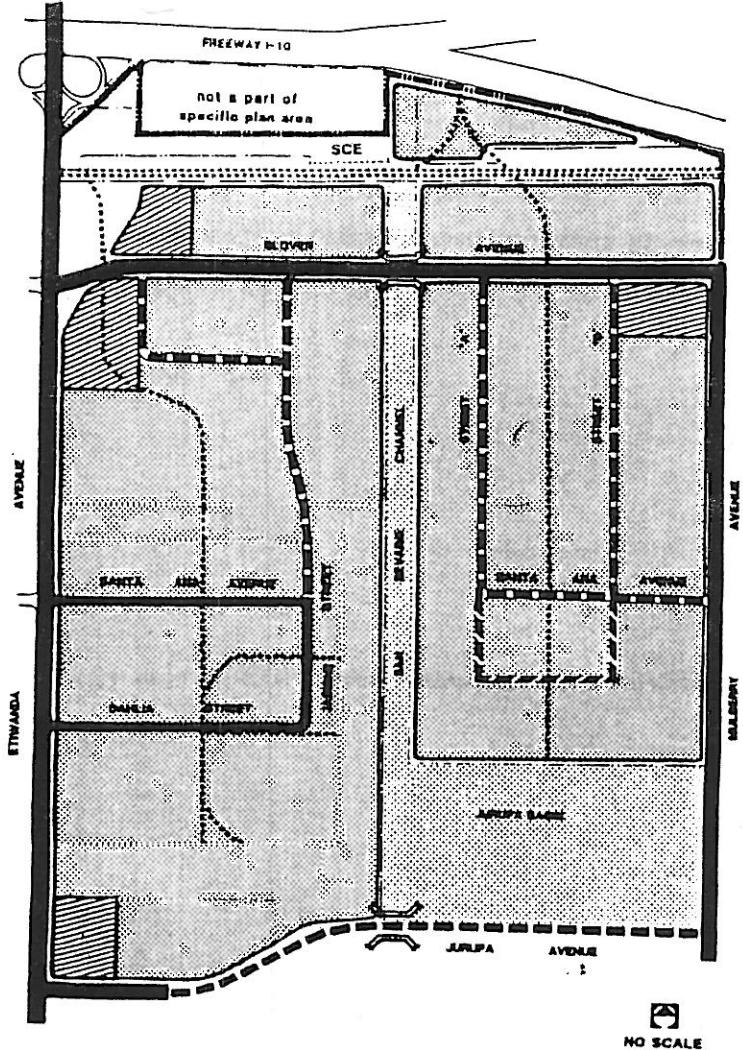
CAP - Capacity

LOS - Level of Service

(1) Volume and capacity values in thousands.

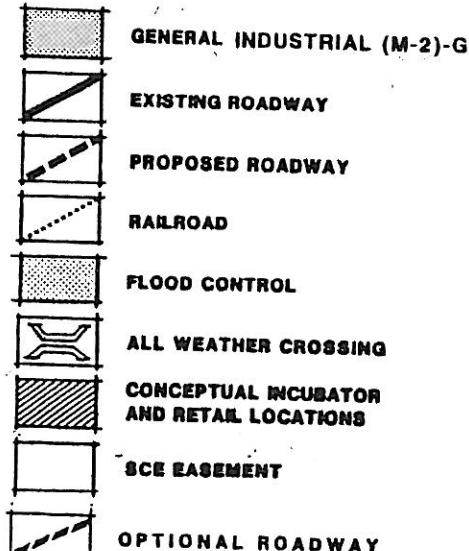
CITY OF FONTANA  
FONTANA GATEWAY · SPECIFIC PLAN

WILLIAMS · KUEBELBECK & Associates, Inc  
FORMA-PLANNING NETWORK



NOTE:  
THE LOCATION OF THESE STREETS AND  
ALL RELATED PUBLIC UTILITY FACILITIES  
ARE CONCEPTUAL ONLY AND WILL BE  
INSTALLED ONLY IN THE DISCRETION OF  
EACH AFFECTED PROPERTY OWNER.  
NO IMPLICATION OF PUBLIC UTILITY  
EASEMENT OR PUBLIC UTILITY FACILITY  
LOCATION IS TO BE DERIVED FROM THE  
LOCATION OF THESE CONCEPTUAL STRE

#### LEGEND



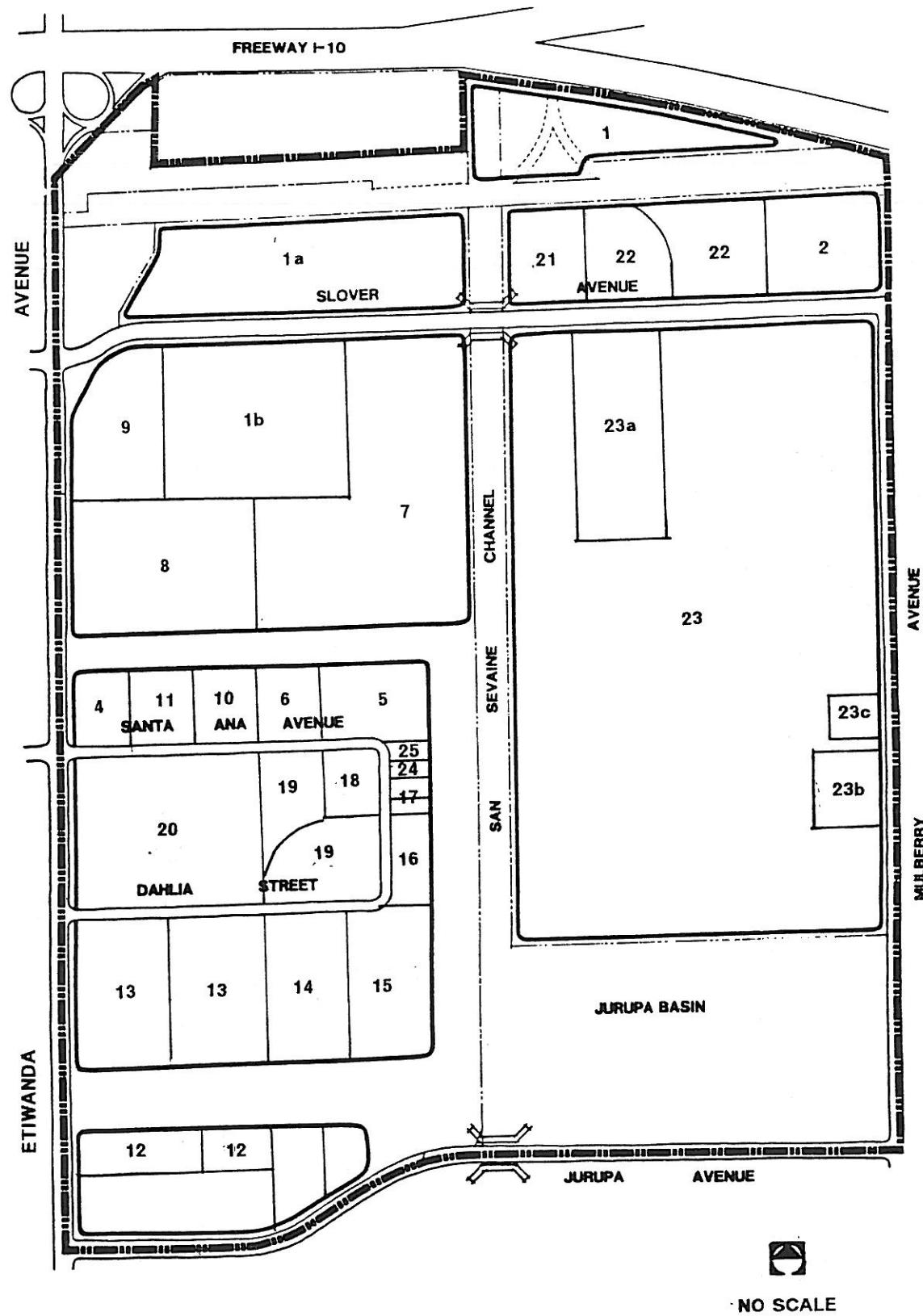
## LAND USE MASTER PLAN

### CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORM-PLANNING NETWORK

FIGURE 6

Figure 6



NO SCALE

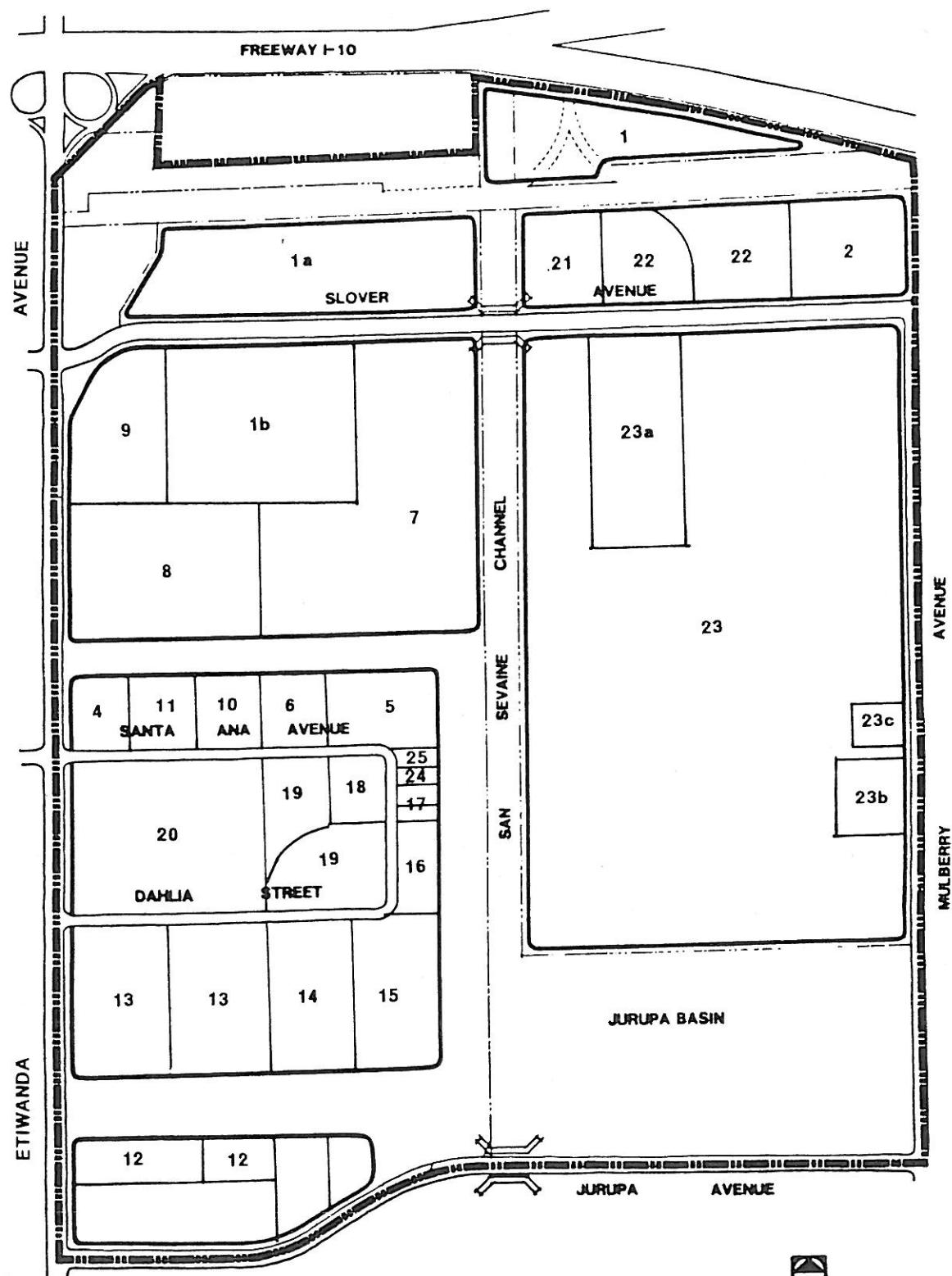
PROPERTY OWNERSHIP

1. Kaiser Steel Corporation
- 1a. The Adaya Family Trust
- 1b. Tecon Pacific
2. W.E. Hall Company
4. Ralston Purina
5. Conrock Company
6. John B. Clark
7. Ameron Pipe Company
8. JJC Fontana
9. Pacific Forge Incorporated
10. Southern Pacific Industrial Development Company
11. Three Sisters Ranch
12. Joseph Filippi
13. George M. Huff Lumber
14. Marshall K. Frankel
15. Vincent J. Stagliano
16. Joseph Pezzner
17. Emmet Laza
18. Glen Dickason
19. Fontana Crane Company
20. NA-CON Services  
(Mineral interest owned by Southern Pacific Railroad)
21. Patrick Industries
22. Tecon Pacific
23. FG Associates
- 23a. Veronica Palenghian
- 23b. Wadco
- 23c. Kaiser Steel Corporation
24. Andel Development
25. Opal Abeling

**OWNERSHIP PLAN**

**CITY OF FONTANA**  
**FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



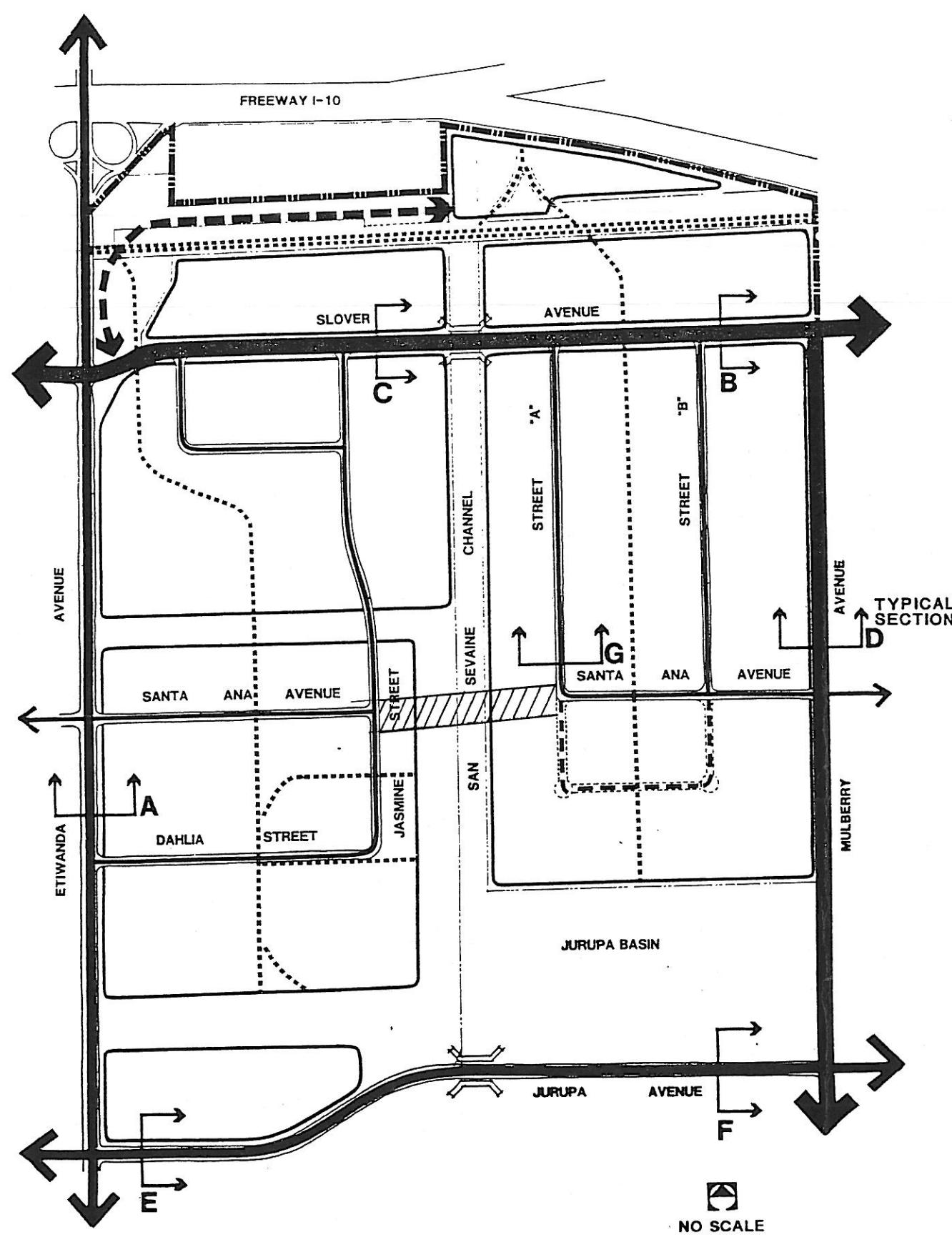
**PROPERTY OWNERSHIP**

1. Kaiser Steel Corporation
- 1a. The Adaya Family Trust
- 1b. Tecon Pacific
2. W.E. Hall Company
4. Ralston Purina
5. Conrock Company
6. John B. Clark
7. Ameron Pipe Company
8. JJC Fontana
9. Pacific Forge Incorporated
10. Southern Pacific Industrial Development Company
11. Three Sisters Ranch
12. Joseph Fillippi
13. George M. Huff Lumber
14. Marshall K. Frankel
15. Vincent J. Stagliano
16. Joseph Pezzner
17. Emmet Laza
18. Glen Dickason
19. Fontana Crane Company
20. NA-CON Services  
(Mineral interest owned by Southern Pacific Railroad)
21. Patrick Industries
22. Tecon Pacific
23. FG Associates
- 23a. Veronica Palenghian
- 23b. Wadco
- 23c. Kaiser Steel Corporation
24. Andel Development
25. Opal Abeling

**OWNERSHIP PLAN**

FIGURE II - A - 3

**CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN**



NOTE: THIS CIRCULATION PLAN REFLECTS THE FOLLOWING ELEMENTS WHICH ARE THE SUBJECT OF GENERAL PLAN AMENDMENT NO. 86-4

1. THE CIRCULATION ELEMENTS OF THE GENERAL PLAN IS HEREBY AMENDED TO DELETE THE BRIDGE CROSSING OF SANTA ANA AVENUE OVER THE SAN SEVAINE CHANNEL.
2. THE DESIGNATION IN THE GENERAL PLAN OF MULBERRY AVENUE AS A SECONDARY STREET IS HEREBY AMENDED TO DESIGNATE IT AS A MAJOR HIGHWAY.

★ THE STREETS SOUTH OF SANTA ANA IN PHASE 1A MAY OR MAY NOT BE CONSTRUCTED, AT THE OPTION OF THE DEVELOPER

GENERAL PLAN AMENDMENT 86-4 DELETION OF SANTA ANA CROSSING THE SAN SEVAINE CHANNEL

NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

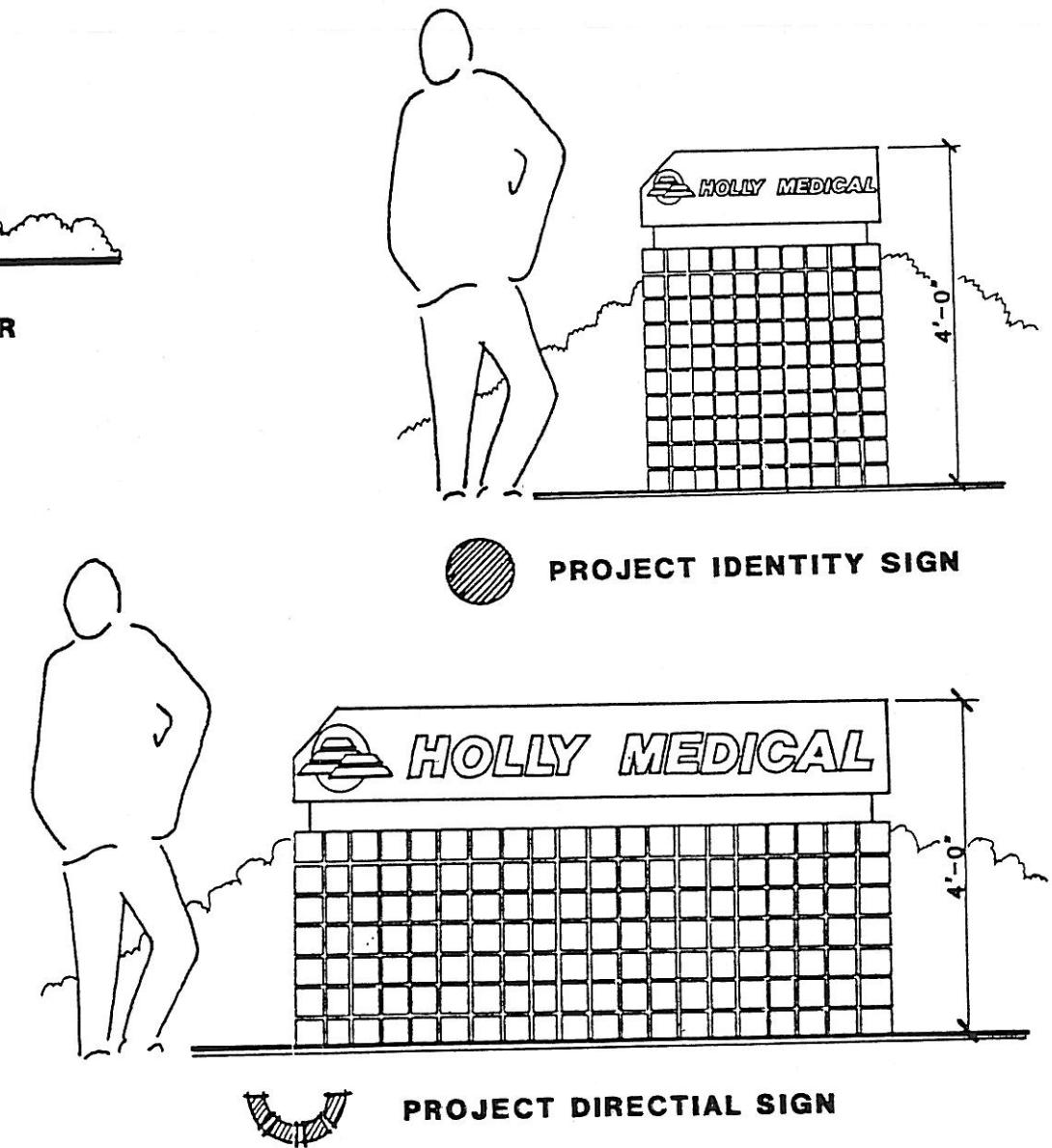
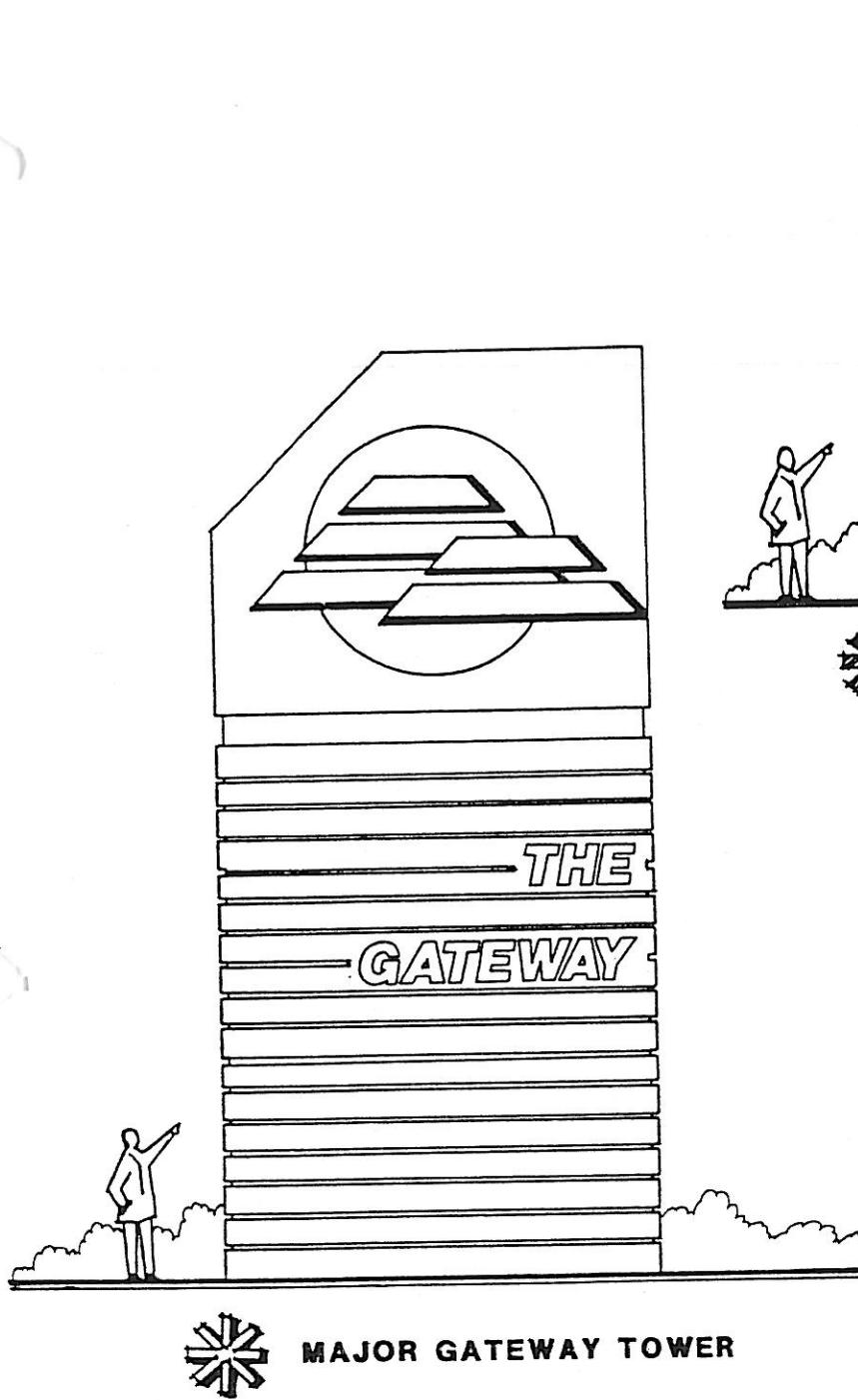
LEGEND

- MAJOR HIGHWAY
- SIX(6) LANE ROADWAY
- INTERIOR INDUSTRIAL COLLECTORS
- PHASED INTERIOR COLLECTOR (OPTIONAL)
- PARCEL ACCESS

CIRCULATION PLAN

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



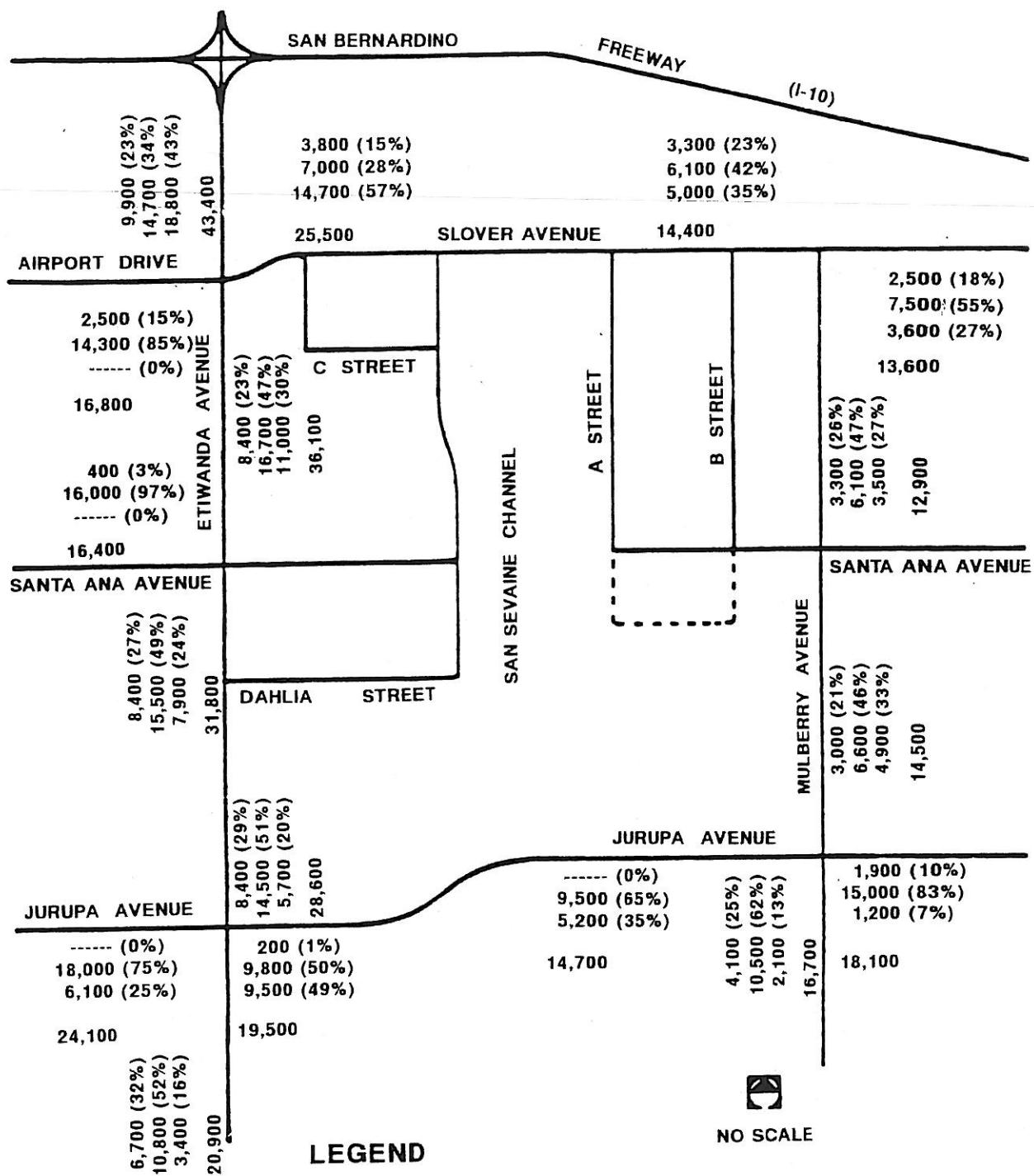
## SIGNAGE PLAN

NOTE: THESE SIGNS ARE CONCEPTUAL ONLY.  
 THEY ARE INTENDED TO SERVE AS GENERAL ILLUSTRATIONS  
 OF WHAT THE FINAL PROJECT SIGNAGE MAY CONSTITUTE.  
 (THE SIERRA INDUSTRIAL PARK WILL PROPOSE PRECISE SIGNAGE  
 STANDARDS AND LOCATIONS, CONSISTENT WITH THESE CONCEPTUAL  
 SIGNAGE ELEMENTS.)

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS - KUEBELBECK & Associates, Inc.  
 FORM-PLANNING NETWORK

## CUMULATIVE TRAFFIC VOLUMES

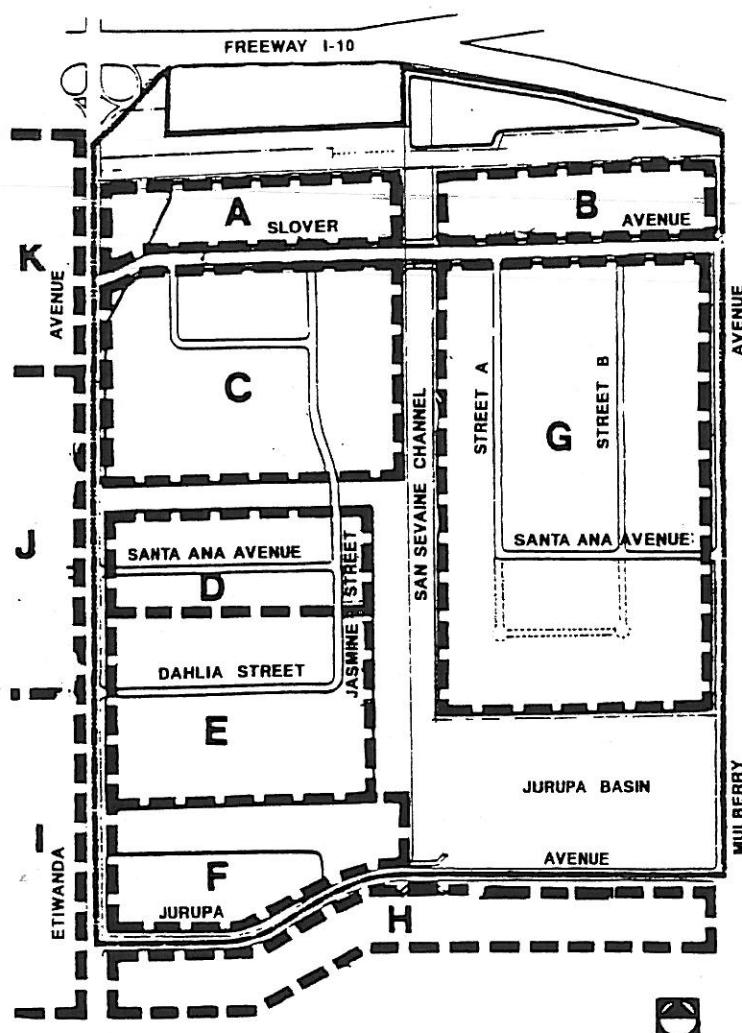


**CITY OF FONTANA**  
**FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
 FORMA-PLANNING NETWORK

# TRIP GENERATION ANALYSIS

## SPECIFIC PLAN AREA



<u>AREA</u>	<u>ACREAGE</u>	<u>24-HOUR VOLUME</u>	<u>PEAK HOUR</u>	<u>NO SCALE</u>
A	39	2200	220	NOTE:
B	36	2000	200	THE LOCATION OF THESE STREETS AND
C	110	6200	620	ALL RELATED PUBLIC UTILITY FACILITIES
D	57	3200	320	ARE CONCEPTUAL ONLY AND WILL BE
E	84	4700	470	INSTALLED ONLY IN THE DISCRETION OF
F	53	3000	300	EACH AFFECTED PROPERTY OWNER.
G	218	12,200	1,220	NO IMPLICATION OF PUBLIC UTILITY
H	42	2400	240	EASEMENT OR PUBLIC UTILITY FACILITY
I	150	8400	840	LOCATION IS TO BE DERIVED FROM THE
J	150	8400	840	LOCATION OF THESE CONCEPTUAL STREETS.
K	150	8400	840	

## CITY OF FONTANA FONTANA GATEWAY·SPECIFIC PLAN

WILLIAMS · KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

have a common access driveway.

#### **3.3.2.7 Rail Access**

Rail access may be provided at the option of property owners, in accordance with Figure 6.

#### **3.3.3 Landscape and Signage Plan**

The Fontana Gateway Specific Plan Area is situated on a site which is relatively flat and very visible from the adjacent freeway (I-10). The lack of trees and vegetation provide an excellent opportunity for developing a system of visual events. The first time visitor will identify with the Specific Plan area through a series of visual sequences along the streetscape forming the major unifying element for the site.

The streetscape element is broken down into three major design elements. They include Gateway Signage, Entries, and Landscaping.

##### **3.3.3.1 Gateway Signage (See Figure 10)**

The first streetscape component that the motorist will encounter upon approaching the Specific Plan area will be a large gateway sign located at the edge of the freeway right-of-way on private property. It is anticipated that this sign will announce "Fontana-Gateway Industrial Park" to freeway (I-10) travelers. However, the exact wording will be finalized at a later date.

The second gateway component, the Major Gateway Tower, will be a sculptured monument type sign. The location is on the eastbound side of the I-10 freeway at the Etiwanda Avenue crossing, on private property at a height of approximately 25 feet. The colors of the major Gateway Tower should be integrated carefully to project a cohesive industrial design statement.

The Third component, the Identity Tower, is similar to the major gateway tower. It will be located on the eastbound side of the freeway, immediately after the Etiwanda Avenue over-crossing. Approximately 30-35 feet in height, this sign is intended to increase exposure to the Specific Plan area.

Examples of types of acceptable Gateway signage located on Figure 10 are for illustrative purposes only. The City, with input from FG Associates on wording and graphics, will be responsible for the design and construction of these Gateway signs. Tax increment funds or assessment district funds shall be used to design, construct and maintain Gateway signs.

##### **3.3.3.2 Entries**

There are seven primary entries and five secondary entries into the Fontana Gateway Specific Plan Area. The signage components for these areas will be designed to identify, unify and create an aesthetic statement for the Specific Plan area through a cohesive

integration of landscape materials and lighting. The entries shall utilize sign monumentation, intense landscape color and formal groves of trees. The entry monumentation signs shall be designed per the illustrations located on Figure 10.

### **3.3.3.3 Landscaping (See Figure 11)**

The landscape concept for the Fontana Gateway Specific Plan Area will reflect the new urban/industrial character of "new" Fontana. The streets within the Specific Plan area will form the next sequential, unifying component.

Generally, a formal planting of equally spaced street trees are proposed for all streets and an informal planting of randomly spaced trees proposed for all landscaped areas adjacent on private property. Linear berms are proposed throughout the project. These berms are to further enhance the landscape character and to create topographic variation and interest to the site. Shrubs, groundcover and grass articulate the ground plane. These streetscape elements collectively give the project a coherence and structure, and an identity achievable only through the development of a large scale project such as this. Table 3 illustrates the street tree palette for the Specific Plan area.

### **3.3.4 Implementation Strategy**

The infrastructure which is necessary for development, as contemplated in the Fontana Gateway Specific Plan, will be fiscally implemented as provided in this section. References to phases are based on Section 6, Development Phasing.

#### **3.3.4.1 Phase 1A Infrastructure**

The Phase 1A infrastructure is anticipated to be financed by a bond anticipation note issue which will later be refinanced by a long-term tax allocation bond issue. These methods will be based on the commitment of FG Associates and its successors to cause private improvements to be developed in the Phase A1 area according to a specific schedule which will provide increased assessed value and, therefore, tax increment revenues from the Phase 1A area. Phase 1A infrastructure is anticipated to be initiated in 1987.

#### **3.3.4.2 Phase 1B Infrastructure**

The Phase 1B infrastructure is anticipated to be financed through the use of assessment districts, which will spread the cost of these regional improvements to all property owners who are benefited by them. The Phase 1B infrastructure is also anticipated to be initiated in 1987.

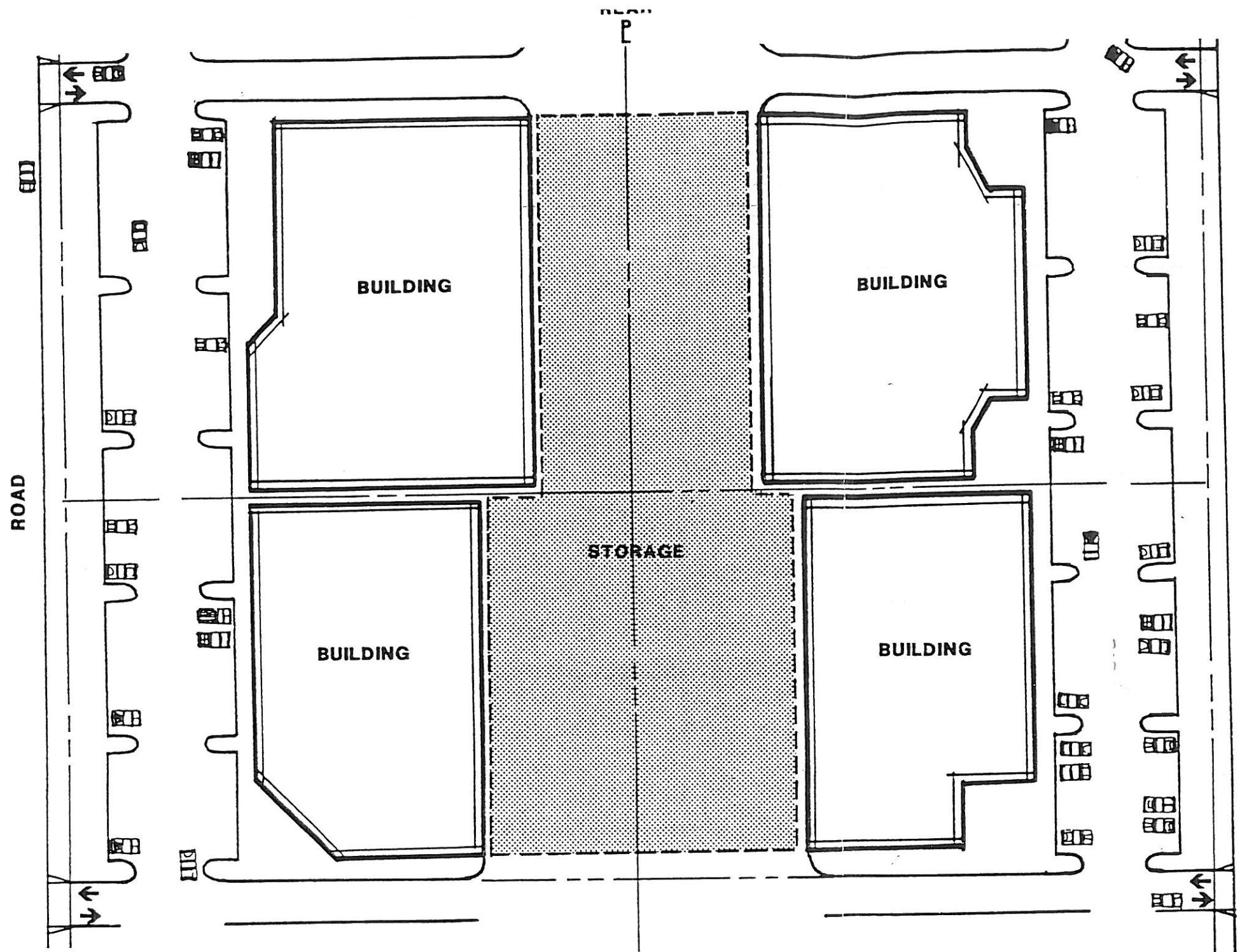
#### **3.3.4.3. Phase 2 Infrastructure**

The Phase 2 infrastructure is anticipated to be financed by a

combination of tax increment based financing, to the extent that development in the Phase 2 area creates a predictable tax increment revenue stream, and assessment district financing. Although market conditions and development in the Phase 2 area will determine the actual timing of the Phase 2 infrastructure, the Phase 2 infrastructure is anticipated to be initiated by 1990.

#### **3.3.4.4 Phase 3 Infrastructure**

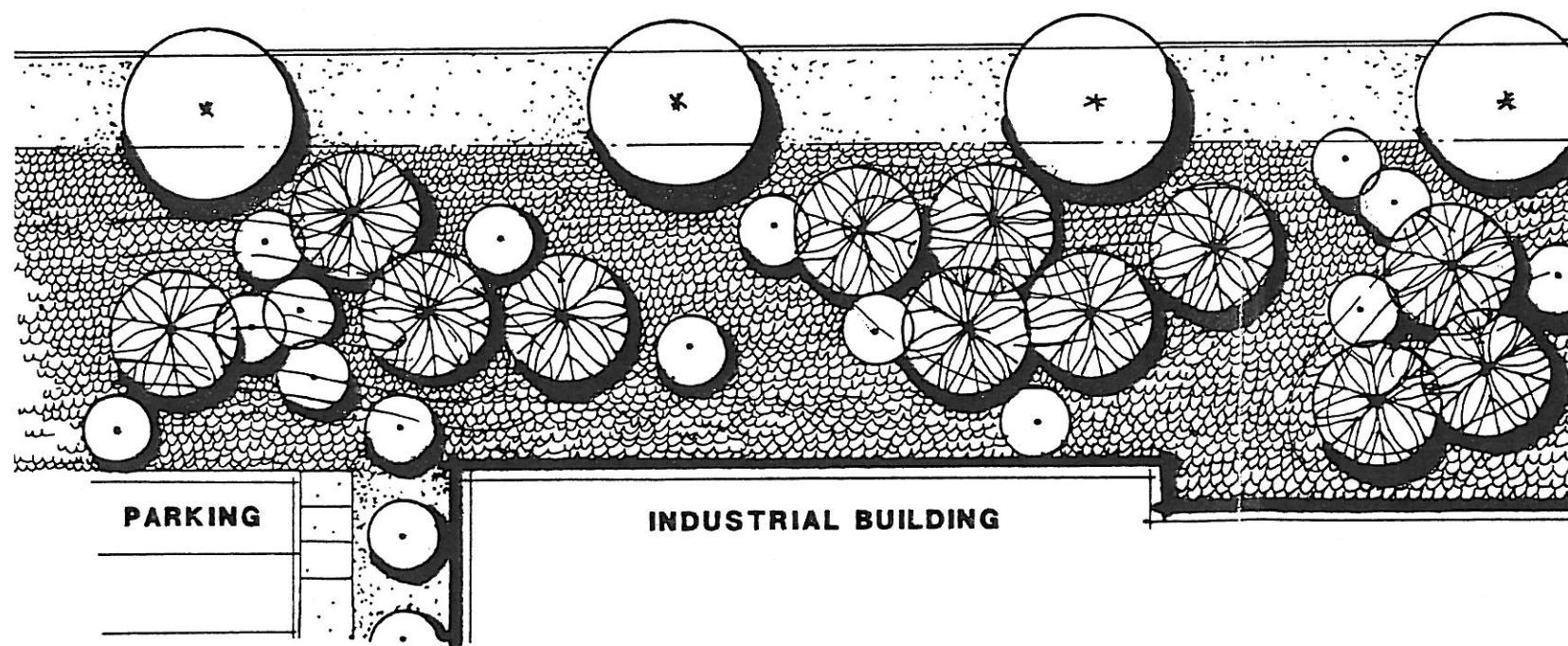
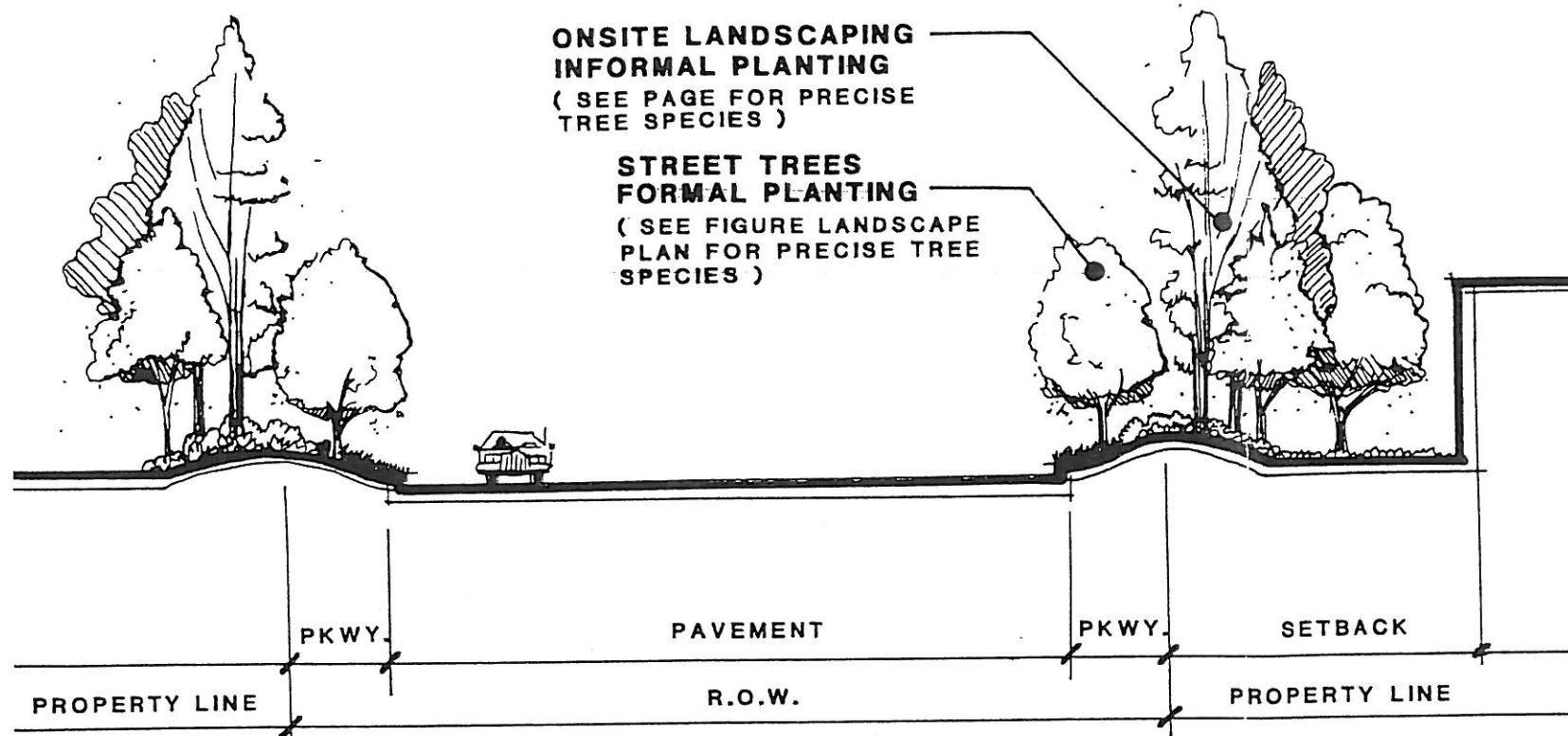
The Phase 3 Infrastructure is intended to be financed by a combination of tax increment based financing, to the extent that development in the Phase 3 area creates a predictable tax increment revenue stream, and assessment district financing. Although market conditions and development in the Phase 3 area will determine the actual timing of the Phase 3 infrastructure, the Phase 3 infrastructure is anticipated to be initiated by 1995.



**SCREENING  
METHODS**  
use of structures to  
screen storage areas

**CITY OF FONTANA**  
**FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

**TABLE 3**  
**SPECIFIC PLAN AREA STREET TREE PALETTE**

Major Street Trees	Botanical Name	Common Name	Size	Number Description and plant spacing
Etiwanda	<i>Pinus Elderica</i>	Mandell Pine	24" box	45' oc
Jurupa Median	<i>Euc. Maculata</i>	Spotted Gum	24" box	45' oc
Mulberry	<i>Pistacia Chinesis</i>	Chinese Pistache	24" box	45'oc
Slover	<i>Brachychiton populaneus</i>	Bottleneck Tree	24" box	45'oc
Interior Industrial Collectors	<i>Melaleculca leucadendra</i>	Cajeput Tree	24" box	one tree per 65 linear ft
1-10 Cor.*	<i>Eucalyptus. Maculata</i>		24" box	plant Informal mass at 25' (min) to 35' (max) oc in triangular spacing pattern

\*Landscaping along I-10 will be planted on private property along the freeway corridor and maintained by those property owners. (See Section 4.2.5)

4.0  
**DEVELOPMENT AND  
DESIGN REGULATIONS**

**CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN**

---

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## **4.0 DEVELOPMENT AND DESIGN REGULATIONS**

### **4.1 INTRODUCTION**

The Development and Design Regulations Section of the Fontana Gateway Specific Plan is intended to provide a balanced framework for the ultimate development of the planning area, by combining specific development and design regulations. The Development Regulation section should foster a mix of industrial land uses designed to develop a sound and diversified economic base, while providing provisions which will respond to changing future conditions.

### **4.2 GENERAL PROVISIONS**

These General Provisions are intended to provide development regulations which are applicable throughout the Specific Plan area.

#### **4.2.1 Lighting**

Lighting shall be used to provide illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways, and working areas. The following shall apply in all areas:

- o The design of light fixtures and their structural support shall be architecturally compatible with main buildings on-site.
- o Freestanding parking lot lighting fixtures shall not exceed 30 feet in height. Building mounted lighting features may be located at a height in proportion to the height of the structure and may exceed 30 feet.
- o Walkway lighting fixtures shall not exceed 15 feet in height.
- o Lighting fixtures shall not project above the roof line of the building. All lighting must provide minimum candle power as required by the Fontana Police Department.
- o All lighting shall be shielded to confine light spread within the site boundaries.

#### **4.2.2 Equipment Screening (See Figures 13–15, 21–25)**

- o All roof and ground mounted equipment, including utility structures and transformers, shall be screened from public view on all sides.
- o All screening shall be integrated with the building design and landscaping. Where possible, a roof parapet wall or other permanent screening device shall be used to screen roof mounted equipment.

4.2.3 Outdoor Storage (See Figures 13-15, 21-25)

- o All materials, supplies, equipment, and operating trucks shall be stored within an enclosed building or a screened area.
- o All uses adjacent to the I-10 freeway shall screen all outdoor storage from public view. The use of dense landscaping, planting and berthing shall be required.
- o Storage area shall not be located in any City required front and side yard setback area except for electrical or telephone equipment installed by the utilities.
- o Outdoor storage of materials or equipment shall not exceed 12 feet in height.
- o Front loading shall be allowed on interior industrial collector streets. In such cases both solid block walls and landscape shall be used to screen such loading areas.
- o All walls shall be solid masonry.
- o Screening for outdoor storage shall be a minimum of 8 feet and a maximum of 12 feet high, the height of which shall be determined by the height of the material being screened. Screening for outdoor storage may be more than 12 feet high with Planning Commission approval and adequate screening based on a line of sight analysis. Chain link fencing with redwood slatting is an acceptable screening material for areas of any lot not visible from the street. Exterior storage shall be confined to portions of the site least visible to public view, where storage areas are visible from public streets abutting subject parcel, screening material shall match building architecture.
- o Landscaping is a preferred method of screening. A screen, as referred to above, shall consist on one, or any combination, of the following:

Walls, Including Retaining Walls

A wall shall consist of concrete, stone, brick, tile or similar type of solid masonry material.

Berms

A berm shall be constructed of earthen material and shall be landscaped.

Landscaping

Vegetation, consisting of evergreen or deciduous trees or shrubs as approved by the Planning Commission. Hedge material to be spaced a maximum of 4'-0" on center, 5 gallon minimum.

#### 4.2.4 Grading

o Prior to the actual development of any portion of the Specific Plan area, a report of a preliminary engineering, geological and soil engineering investigation shall be submitted. This study must show evidence of a recommendation for a safe and stable development. The recommendations by the engineering geologist and soil engineer shall be incorporated into the grading plan design prior to grading permit approval. The soils engineer and engineering geologist must certify the stability of the site prior to the issuance of building permits. Mass and rough grading will be permitted in areas after securing an approved grading/drainage plan and street plan.

#### 4.2.5 Maintenance

o All private property owners shall maintain paved areas in the manner required to eliminate safety hazards, standing water, weeds, inefficient drainage patterns and deterioration of sub-base materials. Paved areas shall be impervious to water and shall be maintained in a sanitary condition free from refuse and debris. Soil, slag, decomposed granite, gravel, or other unpaved surfaces shall not be allowed.

o Property owners are responsible for the continual maintenance of all landscaped areas onsite. All landscaped areas shall be kept free from weeds and debris, maintained in a healthy, growing condition, and shall receive regular pruning, fertilizing, mowing and trimming. In addition, all landscaped areas shall be provided with an approved irrigation system consisting of waterlines and sprinklers designed to provide head-to-head coverage and to minimize overspray onto structures, walks, and windows.

o Property owners shall maintain all structures, including buildings, accessory buildings and signs, in the manner required to protect the health and safety of users, occupants and the general public. The aforementioned requirement shall consist of property owners meeting the applicable standards of all governmental codes, including but not limited to building, mechanical, electrical, fire and health and zoning codes.

o Maintenance assessment districts may be formed for the purpose of providing maintenance of landscape and other improvements within public rights-of-way. Additionally, private property owners may form one or more property owners associations with jurisdiction over all or part if the Specific Plan area (i.e., FG Associates may form a property owners association for Sierra Industrial Park). Such property owners associations may, among other things, have authority to perform the above maintenance and to contract (for a fee) with the above maintenance assessment districts to maintain landscape and other improvements within public rights-of-way, in order to foster a uniform level of maintenance between public right-of-way and private property landscaping and other improvements and to permit a property owners association to effect a higher level of maintenance of landscaping and other improvements than would otherwise be afforded by a public agency.

#### 4.2.6 Utilities

Requirements for undergrounding of utilities shall be consistent with existing City policy.

#### 4.2.7 Land Use Districts

Land use districts within the Specific Plan area are designated by the Land Use Master Plan. District regulations shall maintain the regulatory powers of zoning. Where uncertainty exists as to the Land Use District Boundaries, the following rules shall apply:

- o     Boundaries indicated as following the right-of-way limits of streets, highways, or alleys shall be construed to follow such center lines or right-of-way limits.
- o     Boundaries indicated as following platted lot lines shall be construed to follow such lot lines.
- o     Where a street or alley is vacated or abandoned, the regulations applicable to the property to which it is reverted shall apply to such vacated or abandoned street or alley.
- o     In case any further uncertainty exists, the Planning Commission shall determine the location of such boundaries.
- o     The incubator and retail locations shown on the Land Use Master Plan (See figure 6) are conceptual and permissive. Development of incubator or retail facilities in those areas is at the option of each affected property owner.

#### 4.2.8 Non-conforming Uses

o     The purpose of the Specific Plan is to ensure responsible development of the Specific Plan area through development and design regulations, without creating a detrimental effect on existing uses, even though they may be non-conforming uses. The following regulations are intended to accomplish these objectives:

- o     Repairs, alterations and maintenance

Ordinary nonstructural repairs, alterations or maintenance of a nonconforming structure shall not be deemed an extension of such nonconforming structure and shall be permitted. Alterations or repairs of a structural nature to a nonconforming building or structure shall not be permitted except as approved by the Planning Commission.

- o     Additions, extensions, enlargements and moving

Non-conforming uses or structures may not be enlarged, increased to occupy a greater area of land, or moved in whole or part to any portion of the parcel of land occupied by such non-conforming use or structure, except as approved by the Planning Commission.

- o Restoration of damaged structures
- o Restoration and alteration necessitated by Public Works

Restoration or alteration of a nonconforming building or structure which is necessitated by any public work shall be permitted without conformance with the Specific Plan being required.

A non-conforming building or structure which is damaged or partially destroyed by fire, flood, wind, earthquake or other calamity or by the public enemy to an extent wherein the cost of such reconstruction, repaving or rebuilding does not exceed seventy-five per cent (75%) of the reasonable replacement value, as determined by the Director of Building and Safety Inspection, immediately prior to its damage, may be restored and the occupancy or use may be continued, provided that such reconstruction shall be started within a period of one year from date of damage and diligently prosecuted to completion. In the event such damage exceeds seventy-five per cent (75%) of the reasonable replacement value of such building at the time of such calamity, no repairs or reconstruction shall be made except as approved by the Planning Commission.

- o Change of Use

The non-conforming use of a building or structure may not be changed to other than conforming use, except as approved by the Planning Commission.

- o Change of Zones

The foregoing provisions of this section shall also apply to any nonconforming structures in zones hereafter established on areas not previously covered by the zoning map.

- o Public Utilities

Nothing in this chapter shall be construed or implied so as to prevent the expansion, modernization, replacement, repairing, maintenance, alteration, reconstruction or rebuilding of public service and public utility buildings, structures, equipment and facilities, where there is no change of use or increase in the areas of the property so used.

- o Abandonment

Whenever a non-conforming use has been abandoned, discontinued or changed to a conforming use for a continuous period of six months, the nonconforming use shall not be re-established, and the use of the structure or site thereafter shall be in conformity with the regulations for the district in which it is located.

- o Planning Commission Action

In approving any addition, extension, enlargement, moving, restoration or

other change with respect to a nonconforming use, the Planning Commission shall promote the intent of the Specific Plan as stated in the first dot point of this Section 4.2.8. As an example, in approving any such addition, enlargement, moving, restoration or other change, the Planning Commission shall not require as a condition of approval any conformance with the Specific Plan which is disproportionate to the proposed addition, enlargement, moving, restoration or other change.

**4.2.9 Responsibilities**

It shall be the duty of the Director of Planning to enforce the provisions of this Specific Plan pertaining to the use of land and the erection, construction, moving, conversion, alteration, or addition to any structure or use.

**4.2.10 Penalties**

Any person, firm, or corporation, whether as principal, agent, employee or otherwise, violating any provisions of this specification shall be guilty of a misdemeanor, and upon conviction thereof shall be punishable by a fine of not more than five-hundred dollars (\$500), or by Imprisonment in the County Jail of San Bernardino County for a term not exceeding six (6) months, or by both such fine and Imprisonment. Such person, firm or corporation shall be deemed guilty of a separate offense for each and every day during any portion of which any violation of this specification is pending and shall be punishable as herein provided. All remedies provided for herein shall be cumulative and not exclusive.

**4.2.11 Removal of Public Nuisance**

Any building or structure set up, erected, built, moved or maintained and/or any use of property contrary to the provision of this Specific Plan, is unlawful and a public nuisance. The duly constituted authorities of the City of Fontana shall, upon order of the City Council, immediately commence abatement proceedings.

**4.2.12 Relationship to Fontana City Code**

In the case of a conflict with the Fontana Zoning Ordinance, or municipal code, the regulations established by this Specific Plan shall take precedence. Any aspect of the use or development of property which is not provided for herein shall be subject to the provisions of the Fontana Zoning Ordinance and or Municipal Code.

**4.2.13 Master Design Plans**

All proposed projects must submit and receive approval of a City Design Review. Subareas within the Specific Plan area may apply for and receive approval of the Planning Commission of a Master Design Plan. If projects within a subarea comply with the approved Master Design Plan, then they may receive approval by the Planning Director and not be required to submit such plans to the Planning Commission.

**4.2.14 Hazardous Waste Management Plan**

All uses and/or operations shall comply with the Hazardous Waste Management Plan per Section 33-202 of the Code of the City of Fontana.

4.3 DEFINITIONS

**-A-**

Abutting Land – A parcel of land sharing a common property line with another parcel.

Accessory Structure – A detached building located on the same lot with a principal building, serving an incidental use to the main building or the use of the land.

Agriculture – The tilling of soil, the raising of crops, horticulture, small livestock farming, dairying or animal husbandry and related uses.

Amenity – A natural or man-made feature which enhances a particular property.

Animal Hospital – A place where animals or pets are given medical or surgical treatment, where the boarding of animals is limited to short-term incidental to hospital.

**-B-**

Basement – A story partially (at least one-half) or wholly underground (measured from the average level of the adjoining ground).

Berm – A mound of earth, usually 2 to 6 feet in height.

Buffer Area – An area of land used to visibly separate one use from another or to shield noise, lights or other possible nuisances.

Building Area – The area of a lot remaining after the minimum yard and open space requirements of the development regulations have been met.

Building Coverage – The relationship between the ground floor area of the building and the net area of the site.

**-C-**

Circulation Master Plan – The master plan of the City of Fontana designating adopted and proposed routes for all streets and arterial highways within the City of Fontana.

City – The City of Fontana.

City Council – The City Council of the City of Fontana.

Clinic – An establishment where patients are admitted for examination and treatment by one or more physicians, dentists, psychologists or social workers and where patients are not lodged overnight.

Commercial Use – An activity, normally retail sales, carried out for monetary gain.

Conditional Use – A use that requires special consideration because of its unusual characteristics, so that it may be properly located with respect to the

effects on surrounding properties.

Conditional Use Permit – An approval required for a conditional use to be permitted in a district or zone.

Condominium Development – A building, or group of buildings, in which units are owned individually, and the structure, common areas and facilities are owned by all the owners on a proportional basis, as generally described in the Subdivision Map Act.

-D-

Driveway – An unobstructed paved area providing access to a parking facility.

-E-

Easement – A recorded right or interest in the land or another, which entitles the holder thereof to some use, privilege or benefit out of or over said land.

Educational Institution – Private or public schools, colleges or universities qualified by the State Board of Education to give general academic instruction.

Existing Use – The use of a lot or structure at the time of a zoning ordinance.

Exterior Boundary – The perimeter of any parcel or group of parcels to be developed as an integrated project.

-F-

Facade – The exterior wall of a building exposed to public view or that wall viewed by persons not within the building.

Floor Area, Gross – The total horizontal area, in square feet, including the exterior walls of all floors of a structure.

Floor Area Ratio – The numerical value obtained by dividing the gross floor area of a building or buildings located upon a lot or parcel of land by the total area of such lot or parcel of land. Said total area of a lot or parcel shall exclude dedicated public street rights-of-way, but shall include all other easements and rights-of-way.

Frontage – That side of a lot abutting on a street; the front lot line.

-G-

General Plan (Fontana General Plan) – The adopted general Plan of the City of Fontana.

Grade – The degree of rise or descent of a surface.

Grade, finished – The final elevation of the ground surface after development.

Gross Lot Area – The total horizontal area within the lot lines of a lot or parcel of land before rights-of-way or other areas to be dedicated for public use are

deducted from such lot or parcel.

-I-

Incubator Use – Special uses encouraged to locate in specified areas. Incubator uses are those designed to promote small scale start-up enterprises on minimum sized parcels (1/2 acre).

Industry – Any field or economic activity including forestry, fishing, hunting and trapping, mining, construction, manufacturing, transportation, communication electric, gas and sanitary services; and wholesale trade.

Infrastructure – Facilities and services needed to sustain permitted uses.

Institutional Use – A non-profit or quasi-public use or institution, such as a church, library, public or private school, hospital, or municipally owned or operated building, structure or land, used for public purpose.

Island, Traffic – A raised barrier, sometimes landscaped, located in a vehicle travel path for the purpose of directing circulation patterns.

-K-

Kennel – Any property where four or more dogs or cats, over the age of four months, are kept or maintained.

1. Kennel, commercial: Any kennel maintained for the purpose of boarding, breeding, raising or training dogs or cats for a fee or for sale.

-L-

Land Use Master Plan – The official sectional district map of the City of Fontana which is part of this ordinance.

Lot – Any numbered or lettered parcel shown on a recorded tract map, a record of survey recorded pursuant to an approved division of land, or a parcel map.

Lot Area – The total area within the lot lines of a lot, excluding any street rights-of-ways.

Lot Coverage – That portion of the lot that is covered by buildings and roofed structures.

-M-

Master Design Plan – A plan intended to provide for the resolution of major project related issues by providing for a discretionary review by the Planning Director; typical master design plan issues include: access, architectural and landscape theme, on-site circulation and parking, location of buildings, fencing/screening, and loading areas, drainage, phasing, subdivision/parcel layout and noise.

Multi-phase Development – A development project that is constructed in increments, each increment being capable of existing independently of the others.

-N-

Net Lot Area – The total horizontal area within the property lines of a lot or parcel of land exclusive of all rights-of-way.

Non-Conforming Structure or Building – A structure or building the size, dimensions or location of which was lawful prior to the adoption, revision or amendment to a zoning or district ordinance, but which fails by reason of such adoption, revision or amendment, to conform to the present requirements of the zoning or district regulations.

Non-Conforming Use – A use or activity which was lawful prior to the adoption, revision or amendment of a zoning or district ordinance, but which fails by reason of such adoption, revision or amendment to conform to the present requirements of the zoning or district regulations.

-O-

Off-street Parking Space – A temporary storage area for a motor vehicle that is not located on a dedicated street right-of-way.

On-street Parking Space – A temporary storage area for a motor vehicle which is located on a dedicated street right-of-way.

-P-

Parking Area, private – A parking area other than a street, for the private use of the owners or occupants of the lot on which the parking area is located.

Permitted Use – Any use allowed within district regulations and subject to the restrictions applicable to the Zoning and Land Use District.

Planning Commission – The Planning Commission of the City of Fontana.

Preliminary Site Plan – A preliminary plan developed to identify the location and general relationship between: land uses; improvement, structures, circulation systems, landscaping and design elements, for a particular site or group of sites functioning as a single project.

Premises – A lot or a building site, or a specified portion of a lot or building site, that contains the structures and the open space needed for the location, maintenance and operation of the use of the property.

Private – Belonging to, or restricted for the use or enjoyment of particular persons.

-Q-

Quasi-public – A use owned or operated by a nonprofit, religious or charitable institution and providing education, cultural, recreational, religious or similar types of public programs.

-R-

Retail – The selling of goods, wares or merchandise directly to the ultimate consumer.

Retention Basin – A pond, pool or basin used for the temporary or permanent storage of water run-off.

Right-of-way – A corridor, either public or private, on which a right of passage has been recorded.

-S-

Salvage – The utilization of waste materials.

Screening – A method of visually shielding activities occurring on abutting or nearby parcels from another by fencing, walls, berms or densely planted vegetation.

Service – An act, or any results of useful labor, which does not in itself, produce a tangible commodity.

Setback – The area between the building line and the property line.

Sidewalk – A paved surface or leveled area paralleling and usually separated from the street, used as a pedestrian walkway.

Sign – Any object, device, display or structure, or part thereof, situated outdoors or indoors, which is used to advertise, identify, display, direct or attract attention to an object, person institution, organization business, product, service, event or location by any means, including words, letters, figures, design, symbols, fixtures, colors, illumination or projected images.

Sign, animated or moving – Any sign or part of a sign which changes physical position by any movement or rotation of which gives the visual impression of such movement or rotation.

Sign, area – The entire face of a sign including the advertising surface and any framing, trim or molding, but not including the support structure. The area for signs composed of individual letters shall be measured by forming a rectangle around the letters as measured from the tallest letter.

Sign, identification – A sign giving the nature, logo trademark or other identifying symbol or any combination of the name, and address of a building, business, development establishment on the premises where it is located.

Sign, free standing – Any non-movable sign not affixed to a building.

Site – Any plot or parcel of land or combination of contiguous lots or parcels of land.

Slope – The degree of deviation of a surface from the horizontal, usually expressed in percent or degrees.

Solar Energy Systems – A complete design or assembly consisting of a solar energy collector, an energy storage facility and components for the distribution of transformed energy.

Specific Plan – A fully planned area of the City with all design controls, servicing requirements for financing techniques incorporated in the plan, which is adopted with a self-contained regulatory text.

Storm Drains – Any facilities designed to control, retain or remove surface water runoff.

Story – That portion of building included between the upper surface of any floor and the upper surface of the floor next above, except that the topmost story shall be that portion of a building included between the upper surface of the topmost floor and the ceiling or roof above.

Street – A public or private vehicular right-of-way other than an alley.

Subdivision – The division of a lot, tract or parcel of land into two or more lots, tracts, parcels or other divisions of land for sale, development or lease.

-T-

Temporary structure – A structure without any foundation or footing and which is removed when the designated time period, activity, or use for which the temporary structure was erected has ceased.

Temporary Use – A use established for a fixed period of time with the intent to discontinue such use upon the expiration of the time period.

Topography – The configuration of a surface area showing relative elevations.

Tract – An area, parcel, site, piece of land, or property which is the subject of a development application.

Trailer – A structure standing on wheels, towed or hauled by another vehicle, and used for short-term human occupation, carrying materials, goods or objects, or as a temporary office.

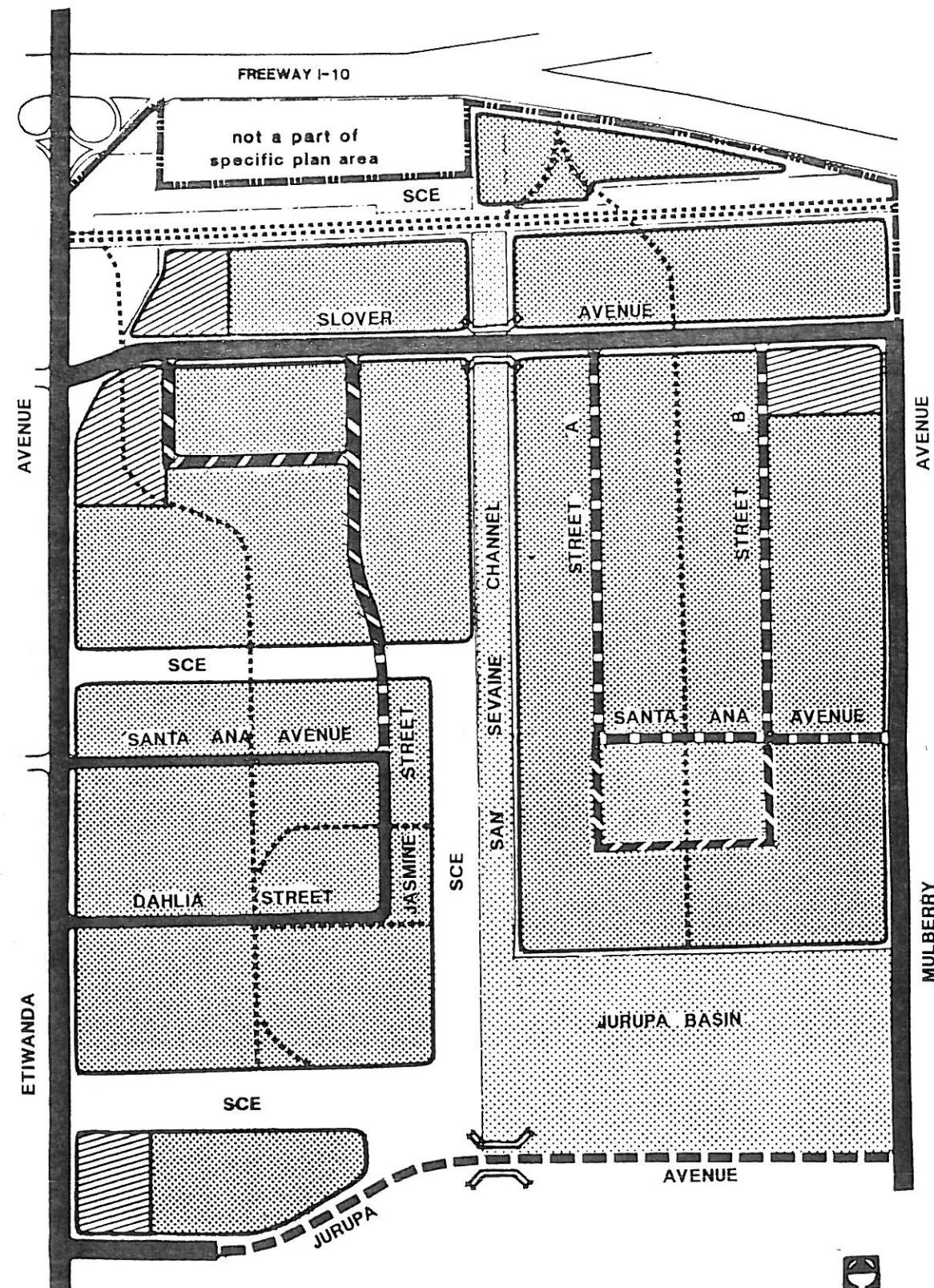
Transportation Services, Accessory – Establishments furnishing services incidental to transportation, such as forwarding and parking services, and the arranging of passenger or freight transportation.

-U-

Undeveloped Land – Land in its natural state before development.

Use – The purpose for which land or a building is occupied, arranged, designed or intended, or for which either land or building is, or may be occupied or maintained.

Utility, Private or Public – (1) Any agency which, under public franchise or ownership, or under certificate of convenience and necessity, provides the public with electricity, gas, heat, steam, communication, rail transportation, water,



THE STREETS SOUTH OF SANTA ANA IN PHASE 1A MAY OR MAY NOT BE CONSTRUCTED AT THE OPTION OF THE DEVELOPER.

NO SCALE

II-C-3

NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATIONS IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND

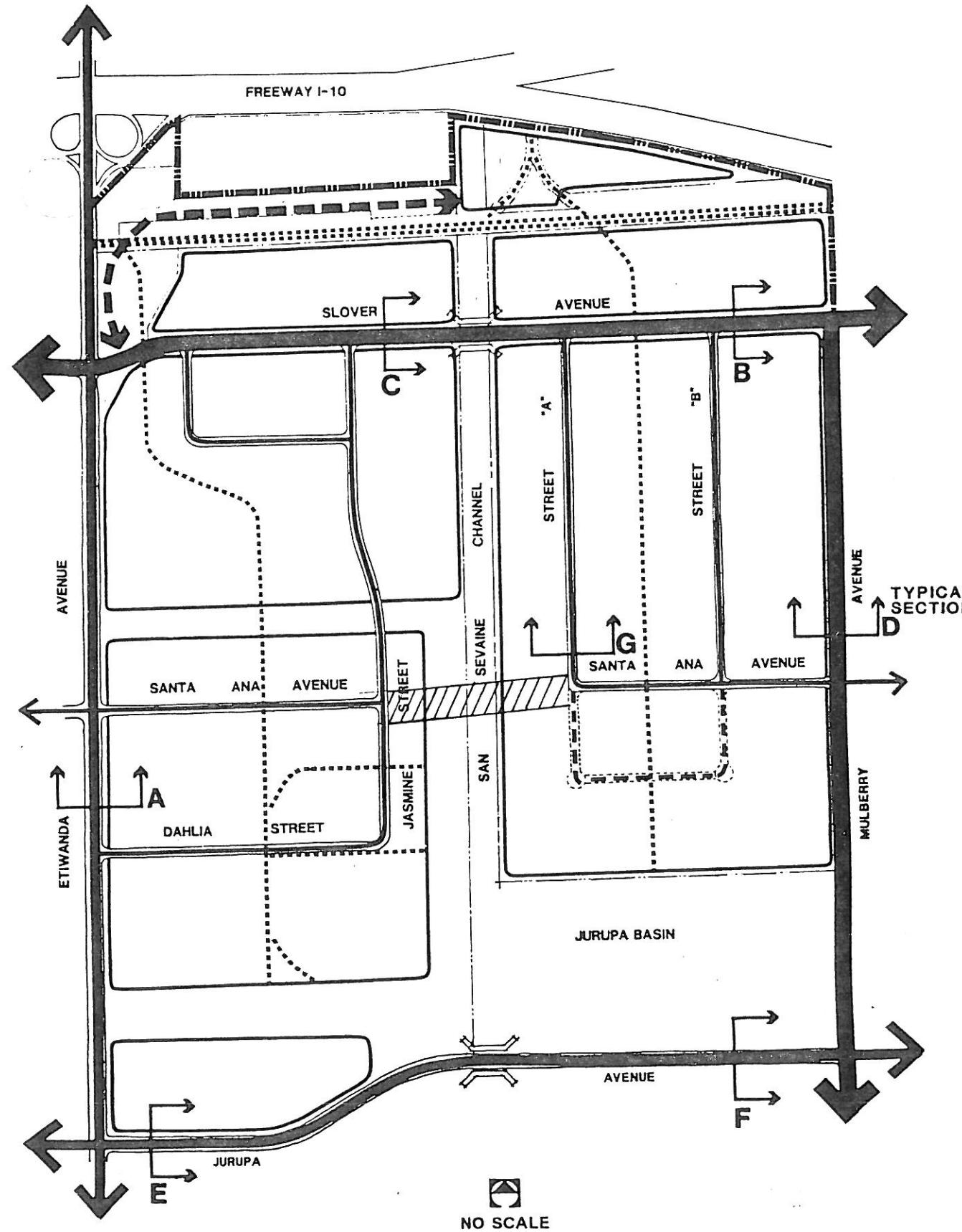
- GENERAL INDUSTRIAL (M-2)-G
- EXISTING ROADWAY
- PROPOSED ROADWAY
- RAILROAD
- FLOOD CONTROL
- CONCEPTUAL INCUBATOR AND RETAIL LOCATIONS
- EDISON EASEMENT
- OPTIONAL ROADWAY

LAND USE  
MASTER PLAN

FIGURE II-C-1

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



NOTE: THIS CIRCULATION PLAN REFLECTS THE FOLLOWING ELEMENTS WHICH ARE THE SUBJECT OF GENERAL PLAN AMENDMENT NO. 86-4

1. THE CIRCULATION ELEMENTS OF THE GENERAL PLAN IS HEREBY AMENDED TO DELETE THE BRIDGE CROSSING OF SANTA ANA AVENUE OVER THE SAN SEVAINE CHANNEL.
2. THE DESIGNATION IN THE GENERAL PLAN OF MULBERRY AVENUE AS A SECONDARY STREET IS HEREBY AMENDED TO DESIGNATE IT AS A MAJOR HIGHWAY.

★ THE STREETS SOUTH OF SANTA ANA IN PHASE 1A MAY OR MAY NOT BE CONSTRUCTED, AT THE OPTION OF THE DEVELOPER

GENERAL PLAN AMENDMENT 86-4 DELETION OF SANTA ANA CROSSING THE SAN SEVAINE CHANNEL

NOTE:  
THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

#### LEGEND

- MAJOR HIGHWAY
- SIX(6) LANE ROADWAY
- INTERIOR INDUSTRIAL COLLECTORS
- PHASED INTERIOR COLLECTOR (OPTIONAL)
- PARCEL ACCESS

#### CIRCULATION PLAN

FIGURE II-C-6

#### CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

sewage collection, or other similar services: (2) A closely regulated private enterprise with an exclusive franchise for providing a public service.

Utility Services - Establishments engaged in the generation, transmission and/or distribution of electricity, gas, steam or other similar services, including water and irrigation water and irrigation systems and sanitary systems used for the collection and disposal of garbage, sewage and other waste by means of destroying or processing materials.

-V-

Variance - Permission to depart from the literal requirements of the zoning ordinance, pursuant to findings required by state law.

Vehicular Accessway - A private, non-exclusive vehicular easement affording access to abutting properties.

-W-

Water Reclamation Facility (WRF) - A facility which treats municipal wastewater, through an arrangement of devices, structures, equipment, processes and controls including primary, secondary and advanced treatment, to a level acceptable for direct beneficial reuse applications.

Wing Wall - An architectural feature in excess of six feet in height, which is a continuation of a building wall projecting beyond the exterior walls of a building.

-Y-

Yard - An open space that lies between the principal or accessory building or buildings and the nearest lot line.

-Z-

Zone - A specifically delineated area or district in a municipality within which regulations and requirements uniformly govern the use, placement, spacing and size of land and buildings.

Zoning Ordinance - The comprehensive zoning ordinance of the City of Fontana.

#### 4.4 LAND USE - PERMITTED/CONDITIONAL USES

Permitted uses are those land uses allowed in the planning area subject to the Development Regulations of the Specific Plan. Conditional Uses are permitted, subject to the granting of a Conditional Use Permit. Because of their unusual characteristics, Conditional Uses require special consideration so that they may be located properly with respect to the objectives of this Plan and with respect to their effects on surrounding properties. In order to achieve these purposes, the Planning Commission is empowered to grant and to deny applications for Conditional Use Permits and to impose reasonable conditions upon granting of Conditional Use Permits, subject to the right of appeal to the City Council.

This section includes a list of the uses and indicates whether or not each use is permitted (P) or conditional permitted (C) in each land use category. In addition, the symbol (I) has been located adjacent to various permitted and conditional uses designed to designate uses which are to be located in Incubator overlay zones, only; however, the location of such uses in incubator overlay zones is at the option of each affected property owner. If a proposed use is not listed, a use determination shall be conducted by the Planning Commission to determine if the

proposed use is similar to a listed use and consistent with the intent of the Specific Plan.

**-A-**

Administrative professional, medical, financial and general business offices including offices for any industry as a primary use.

P

Advertising display manufacture.

P

Continued agricultural uses including field crops, truck gardening, bush and berry crops, tree crops, nurseries, flower culture, chicken and rabbit raising, and the keeping of domestic livestock as regulated in the Fontana Zone Code.

P

Aircraft and aircraft accessories and parts manufacture.

P

Air-reduction/compressed air facilities.

C

Antique sales and refinishing.

P (I)

Assembly of electrical equipment such as radio, stereo, and television receivers, phonographs, other types of sound equipment, and motion picture cameras and projectors.

P

Assembly of small electric appliances such as lighting fixtures, irons, fans, toasters, and electric toys.

P

Automobile, truck, farm equipment, motorcycle, and trailer accessories and parts manufacture and assembly.

P

Automobile, truck, farm equipment, motorcycle, and trailer sales, repairing, rebuilding and painting.

P

**-B-**

Bakeries, wholesale, and distribution.

Banks.

C (I)

Bottling plants.

P

Note: P = Permitted Use

C = Conditional Use

(I) = Uses to be located in incubator overlay zones only.

Box factories and cooperage manufacture.

P

Building materials manufacture and assembly, including composition wallboards, partitions, precast panels, prefabricated structures, and oil-based roofing materials.

P

-C-

Cabinet shops and furniture manufacture

P

Can and metal container manufacture.

P

Carpet and rug manufacture.

P

Ceramic products manufacture using only previously pulverized clay, and kilns fired by electricity or gas.

P

Co-generation facilities.

C

Cold storage plants.

P

Concrete pipe manufacture & assembly

P

Contractors' offices.

P

Creameries and dairy products plants.

P

-D-

Day Care facilities incidental to primary use.

C

-E-

Electric equipment sales and repair shops.

P (I)

Electroplating.

C

Note: P = Permitted Use

C = Conditional Use

(I) = Uses to be located in Incubator overlay zones only.

-F-

**Food and food products manufacture and processing, not including fish, pickles, sauerkraut, vinegar or yeast, or the refining or rendering of fats or oils.**

C

**Freight forwarding terminals.**

P

**Frozen food distributors.**

P

-G-

**Garment manufacture.**

P

**Glass and glass products manufacture.**

P

-H-

**Home improvement centers.**

P (I)

-L-

**Laboratories, testing and research.**

P

**Laundries, commercial.**

P

**Lumber and building materials yards.**

P

-M-

**Machinery manufacture, including heavy electrical, agricultural, and construction machinery, and light machinery and equipment such as air conditioning, dishwashers, dryers, furnaces, heaters, refrigerators, ranges, stoves, ovens, and washing machines.**

P

**Machinery sales and rentals.**

P

**Machine shops.**

P

Note: P = Permitted Use

C = Conditional Use

(I) = Uses to be located in Incubator overlay zones only.

Machine tools manufacture, including metal lathes, metal presses, metal stamping machines, and woodworking machines.

P

Major commercial outlet.

P (I)

Manufacture and assembly of business machines, including electronic data processing equipment, accounting machines, calculators, typewriters, communications and testing equipment, and related equipment.

P

Manufacture and assembly of electrical supplies such as coils, condensers, crystal holders, solid state circuitry, lamps, switches, and wire and cable assembly.

P

Manufacture and maintenance of electric and neon signs, commercial advertising structures, and light sheet metal products including heating and ventilating ducts and equipment, and similar uses.

P

Manufacture of cutlery, hardware, and hand tools, die and pattern making, metal stamping and extrusion of small products.

P

Manufacture of scientific, medical, dental, and drafting instruments, orthopedic and medical appliances, optical goods, watches and clocks, electronics equipment, precision instruments, and musical instruments.

P

Manufacturing, assembling, compounding, packaging and processing of cosmetics, drugs and pharmaceutical.

P

Mobile home manufacturing.

P

Motor and generator manufacture.

P

Moving agencies and storage warehouses.

P

-O-

Outdoor storage incidental to a primary use.

C

Note: P = Permitted Use

C = Conditional Use

(I) = Uses to be located in Incubator overlay zones only.

-P-

Paint manufacture not employing a boiling process.

C

Paper products manufacture, including shipping containers, pulp goods, coated paper stencils, and similar uses.

P

Parcel delivery terminals.

P

Photographic processing.

P

Pipe lining, coating, processing and assembly

P

Printing, publishing, lithographing and engraving.

P

-R-

Railroad freight stations.

P

Recycling operations.

C

Refrigeration equipment sales and repair shops.

P

Research and development.

P

Restaurants.

P (I)

Restaurants and cafeterias incidental to a business of industrial establishment for the use of employees and visitors.

P

Retail and wholesale establishments for the sale or distribution of products raised, manufactured, or produced on the site. (No more than ten percent of the total building area may be used for retail sales).

P

Rug and carpet cleaning plants.

P

Note: P = Permitted Use

C = Conditional Use

(I) = Uses to be located in Incubator overlay zones only.

Service stations. C(I)

Sheet metal shops. P

Steel products manufacture and assembly, including steel cabinets, lockers, doors, fencing, furniture, and metal pipe, including corrugated metal pipe. P

Storage buildings for household goods and mini-warehouses. P

Toys and novelties manufacture. P

Warehousing. P

Water Reclamation Facility. C(1)

Wholesale business establishments. P

Welding shops. P

Wood and lumber processing and woodworking, including planing mills, sawmills, plywood, veneer, and wood preserving treatment. P

#### 4.5 DEVELOPMENT AND DESIGN REGULATIONS

The Development and Design Regulations portion of The Fontana Gateway Specific Plan establish regulations for all specific land use types within the Specific Plan area. The Development and Design Regulations section consists of the following components:

- parcel sizes
- setbacks
- parking and loading
- rail service
- architectural design
- landscape design
- signing design
- wall and fencing design

Note: P = Permitted Use

C = Conditional Use

(I) = Uses to be located in Incubator overlay zones only.

(1) = Use allowed in an Incubator overlay zone with a CUP.

sewage collection, or other similar services: (2) A closely regulated private enterprise with an exclusive franchise for providing a public service.

Utility Services - Establishments engaged in the generation, transmission and/or distribution of electricity, gas, steam or other similar services, including water and irrigation water and irrigation systems and sanitary systems used for the collection and disposal of garbage, sewage and other waste by means of destroying or processing materials.

-V-

Variance - Permission to depart from the literal requirements of the zoning ordinance, pursuant to findings required by state law.

Vehicular Accessway - A private, non-exclusive vehicular easement affording access to abutting properties.

-W-

Water Reclamation Facility (WRF) - A facility which treats municipal wastewater, through an arrangement of devices, structures, equipment, processes and controls including primary, secondary and advanced treatment, to a level acceptable for direct beneficial reuse applications.

Wing Wall - An architectural feature in excess of six feet in height, which is a continuation of a building wall projecting beyond the exterior walls of a building.

-Y-

Yard - An open space that lies between the principal or accessory building or buildings and the nearest lot line.

-Z-

Zone - A specifically delineated area or district in a municipality within which regulations and requirements uniformly govern the use, placement, spacing and size of land and buildings.

Zoning Ordinance - The comprehensive zoning ordinance of the City of Fontana.

#### 4.4 LAND USE - PERMITTED/CONDITIONAL USES

Permitted uses are those land uses allowed in the planning area subject to the Development Regulations of the Specific Plan. Conditional Uses are permitted, subject to the granting of a Conditional Use Permit. Because of their unusual characteristics, Conditional Uses require special consideration so that they may be located properly with respect to the objectives of this Plan and with respect to their effects on surrounding properties. In order to achieve these purposes, the Planning Commission is empowered to grant and to deny applications for Conditional Use Permits and to impose reasonable conditions upon granting of Conditional Use Permits, subject to the right of appeal to the City Council.

This section includes a list of the uses and indicates whether or not each use is permitted (P) or conditional permitted (C) in each land use category. In addition, the symbol (I) has been located adjacent to various permitted and conditional uses designed to designate uses which are to be located in Incubator overlay zones, only; however, the location of such uses in incubator overlay zones is at the option of each affected property owner. If a proposed use is not listed, a use determination shall be conducted by the Planning Commission to determine if the

END OF PAGE

Service stations.

C(I)

Sheet metal shops.

P

Steel products manufacture and assembly, including steel cabinets, lockers, doors, fencing, furniture, and metal pipe, including corrugated metal pipe.

P

Storage buildings for household goods and mini-warehouses.

P

-T-

Toys and novelties manufacture.

P

-W-

Warehousing.

P

*Water Reclamation Facility.*

C(I)

Wholesale business establishments.

P

Welding shops.

P

Wood and lumber processing and woodworking, including planing mills, sawmills, plywood, veneer, and wood preserving treatment.

P

#### 4.5 DEVELOPMENT AND DESIGN REGULATIONS

The Development and Design Regulations portion of The Fontana Gateway Specific Plan establish regulations for all specific land use types within the Specific Plan area. The Development and Design Regulations section consists of the following components:

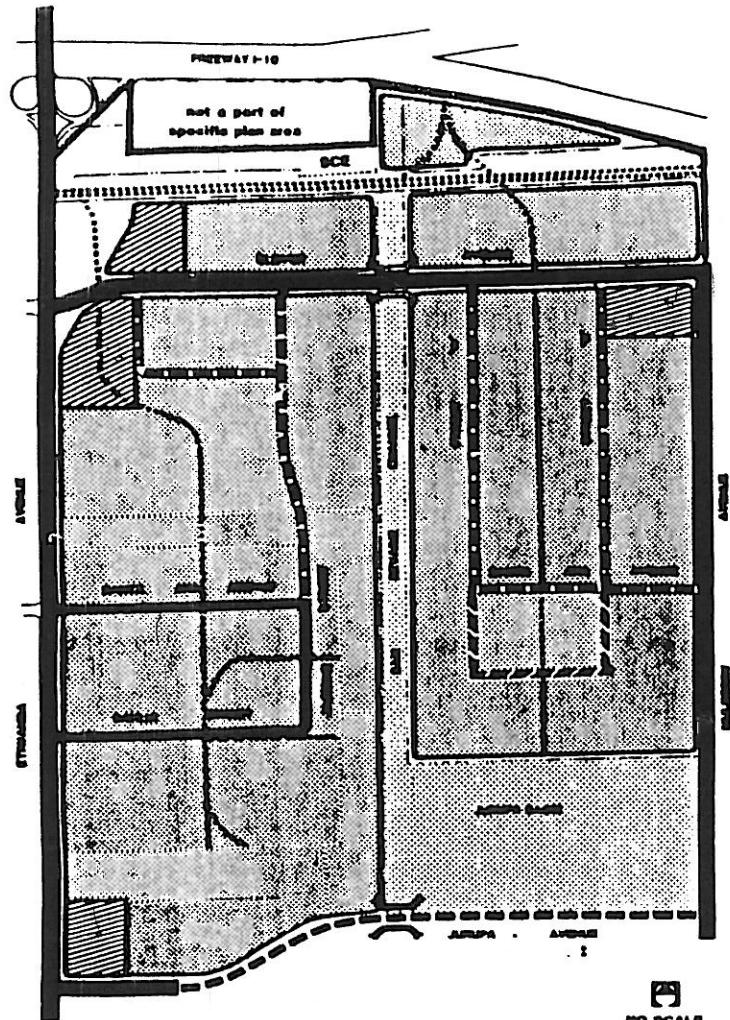
- o parcel sizes
- o setbacks
- o parking and loading
- o rail service
- o architectural design
- o landscape design
- o signing design
- o wall and fencing design

Note: P = Permitted Use

C = Conditional Use

(I) = Uses to be located in Incubator overlay zones only.

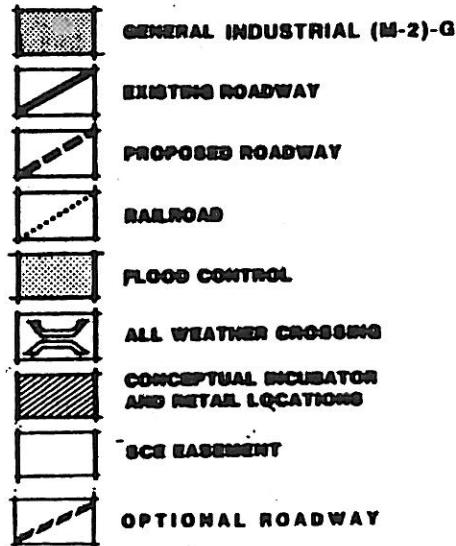
(I) = *Use allowed in an Incubator overlay zone with a CUP.*



NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

LEGEND



LAND USE  
MASTER PLAN

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS - KUEBELBECK & Associates, Inc.  
FORMA PLANNING NETWORK

#### 4.5.1 Parcel Sizes

- A. The minimum net parcel size: 1 acre  
Rail service: 1 acre  
Service Commercial/Incubator: 1/2 acre
- B. Minimum parcel width shall not be less than 100'.
- C. All existing lots of record are allowed to develop according to the development regulations contained within this Specific Plan.

#### 4.5.2 Setbacks (See Figures 16-19)

Building setback is defined as the distance from the property line to the building face. Average on-site setbacks will provide diversity along the streetscape. In no case shall the setback be less than the minimum setback allowed, except where a variance is granted as provided in the law.

Note: P = Permitted Use  
C = Conditional Use  
(I) = Uses to be located in incubator overlay zones only.

**Setback Schedule**  
**(Measured from ultimate property/ROW line)**

	BUILDING	PARKING AND LANDSCAPING
Etiwanda	35 ft (av) (25 ft min)	30 ft min
Jurupa	35 ft (av) (25 ft min)	30 ft min
Mulberry	35 ft (av) (25 ft min)	30 ft min
Slover	35 ft (av) (25 ft min)	30 ft min
Industrial Collector	20 ft av (15 ft min)	15 ft min
Interior rear	6 ft	6 ft
Interior side	6 ft	6 ft
Rear abutting street	(per street classification above)	
Corner side	(per street classification above)	

**4.5.3 Building Height: No maximum**

Buildings exceeding 35'; adjacent to Etiwanda, Jurupa, Mulberry, and Slover, an additional one (1) foot setback measured from the right-of-way shall be required for each additional one (1) foot in height.

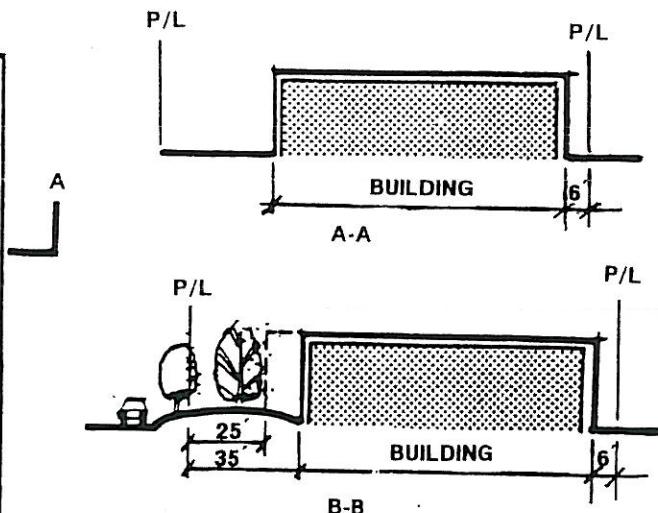
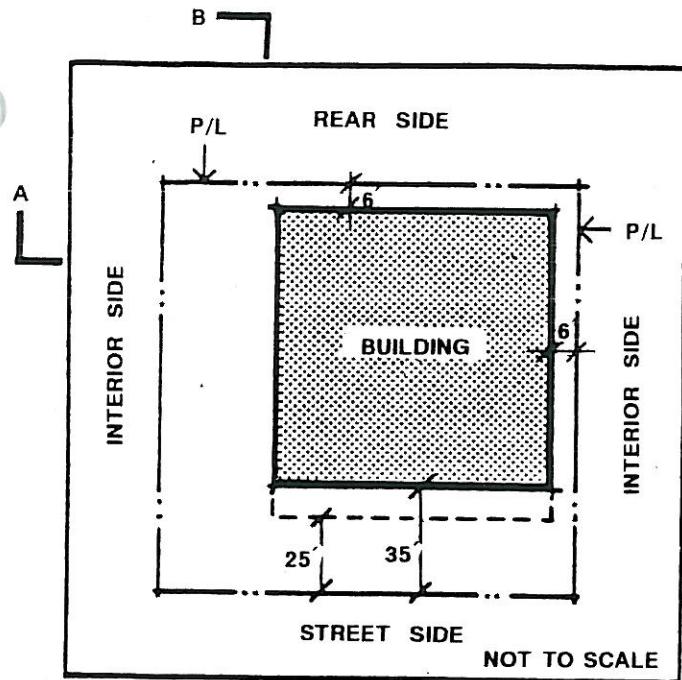
**4.5.4 Floor Area Ratio: .70**

**4.6 ON-SITE LANDSCAPING DESIGN REGULATIONS (See Figures 13-15, 21- 25)**

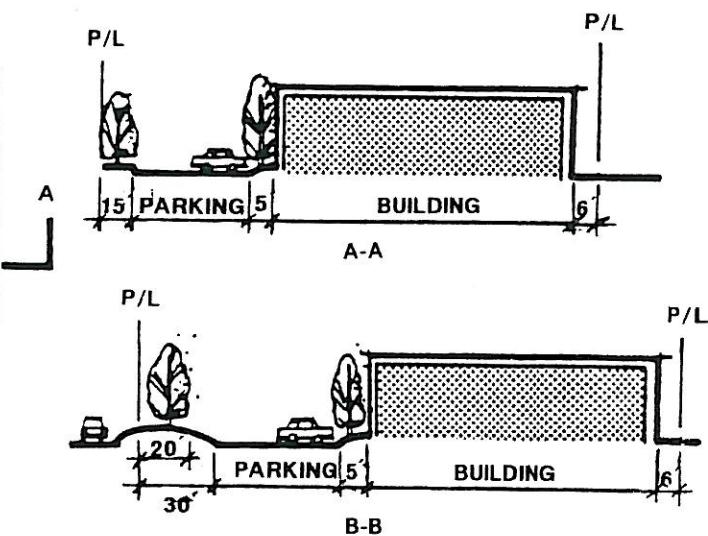
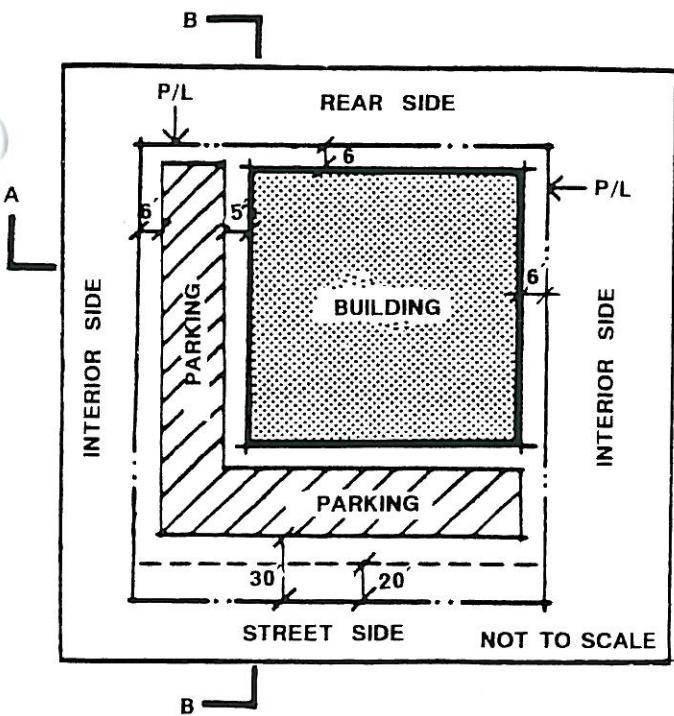
**4.6.1 Applicable Areas**

The following regulations are to be applied to on-site landscaping. The landscape palette contained within this section is purposely limited so that the Fontana Gateway Specific Plan Area is readily recognizable.

# SETBACKS: ETIWANDA, MULBERRY, JURUPA, SLOVER



## MINIMUM BUILDING SETBACKS

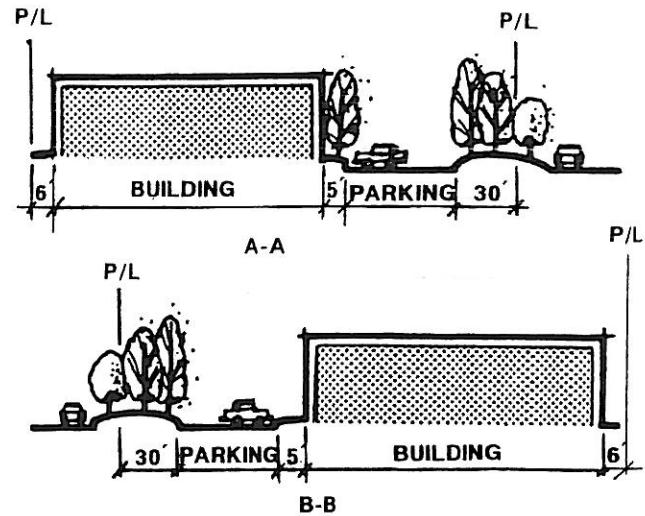
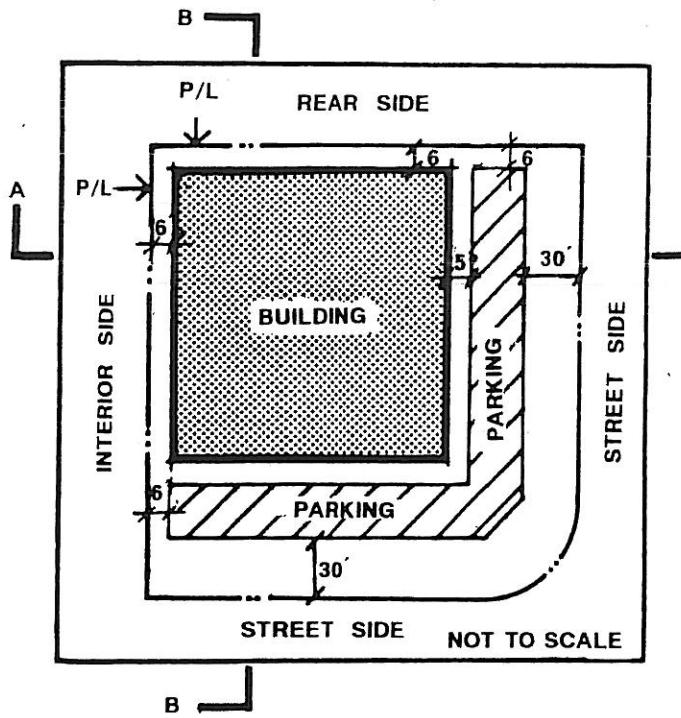


## MINIMUM PARKING AND LANDSCAPE SETBACKS

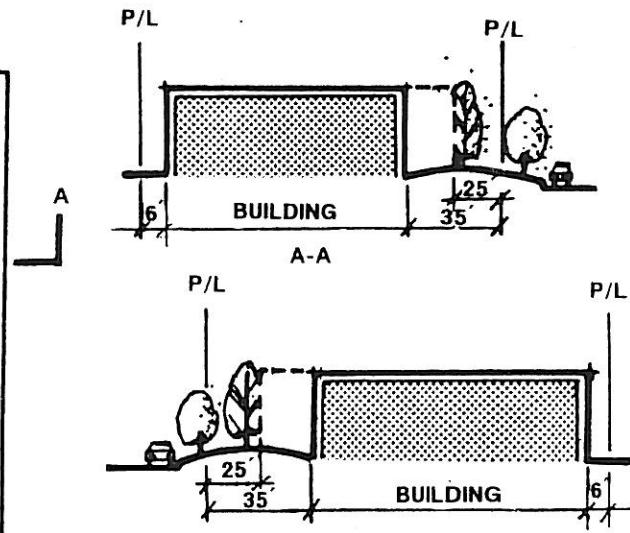
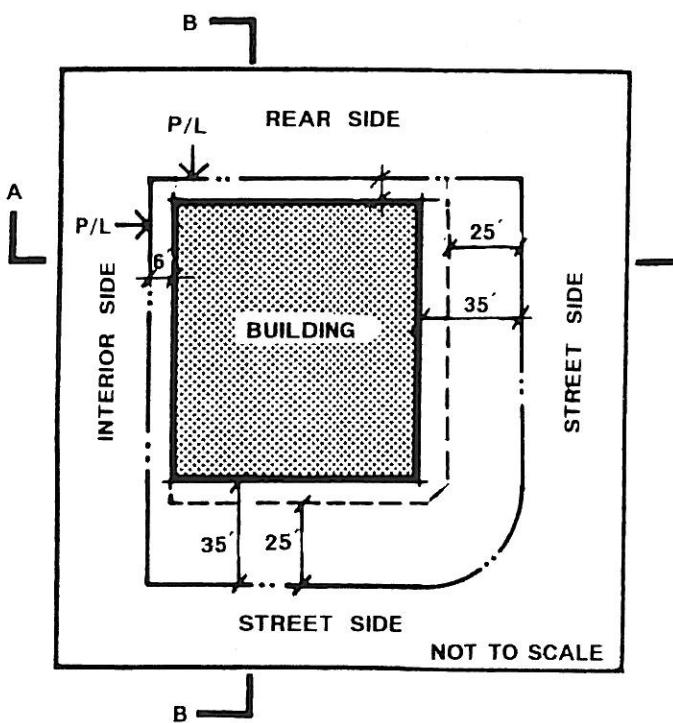
# CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## SETBACKS: ETIWANDA, MULBERRY, JURUPA, SLOVER



**MINIMUM PARKING AND  
LANDSCAPE SETBACKS  
(CORNER PARCEL)**

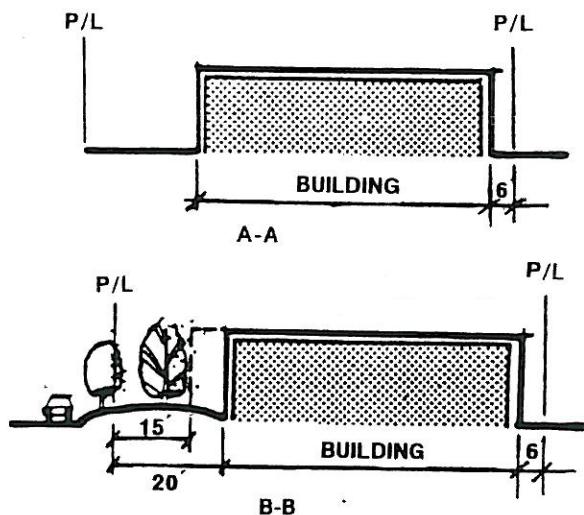
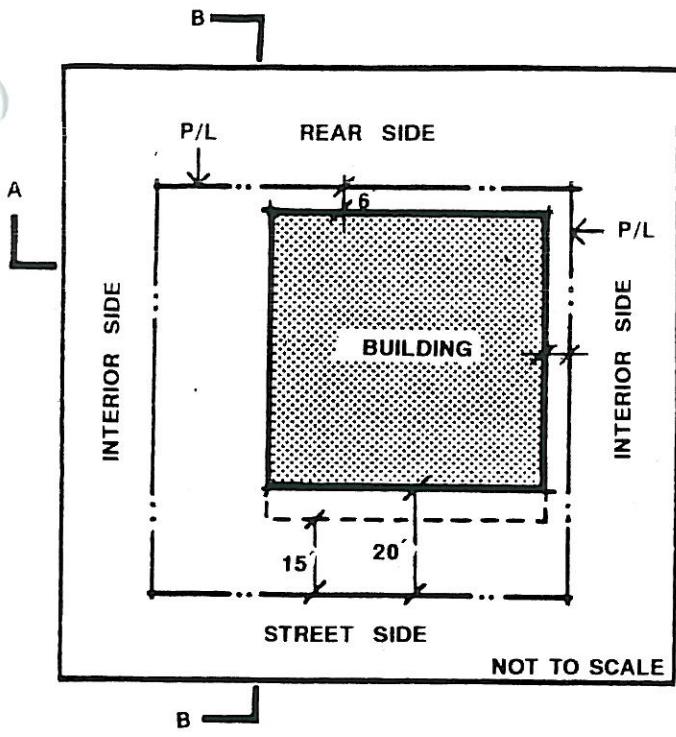


**MINIMUM BUILDING  
SETBACKS  
(CORNER PARCELS)**

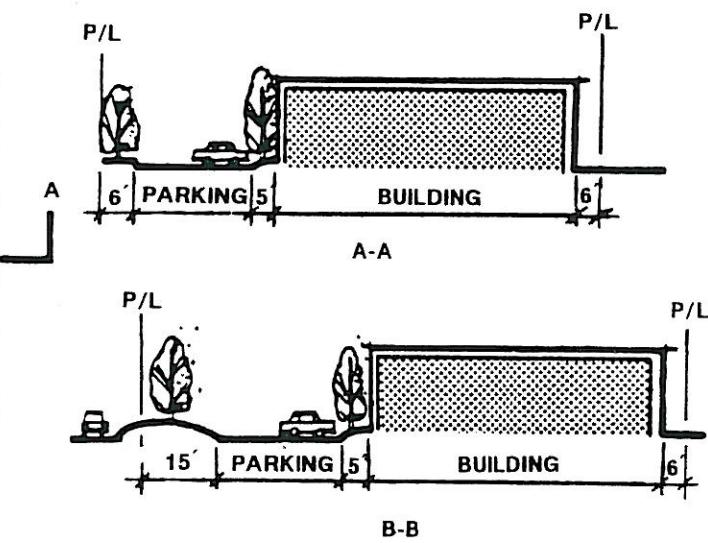
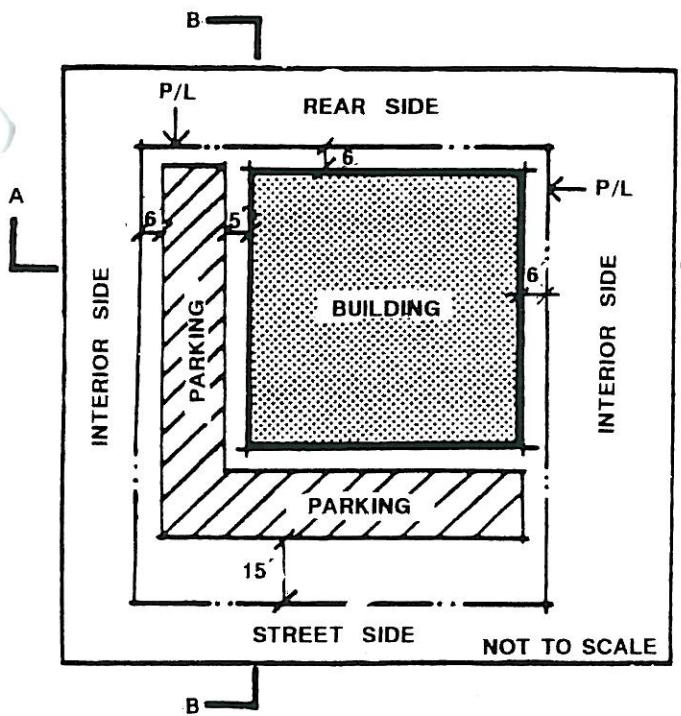
## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# SETBACKS: INDUSTRIAL COLLECTOR STREETS



## MINIMUM BUILDING SETBACKS

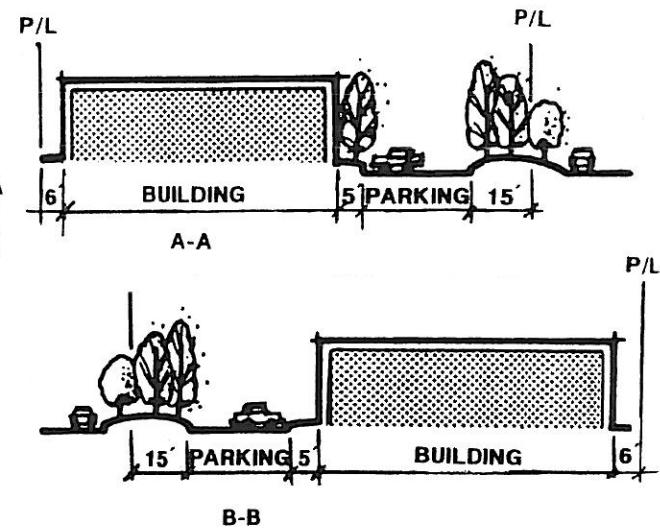
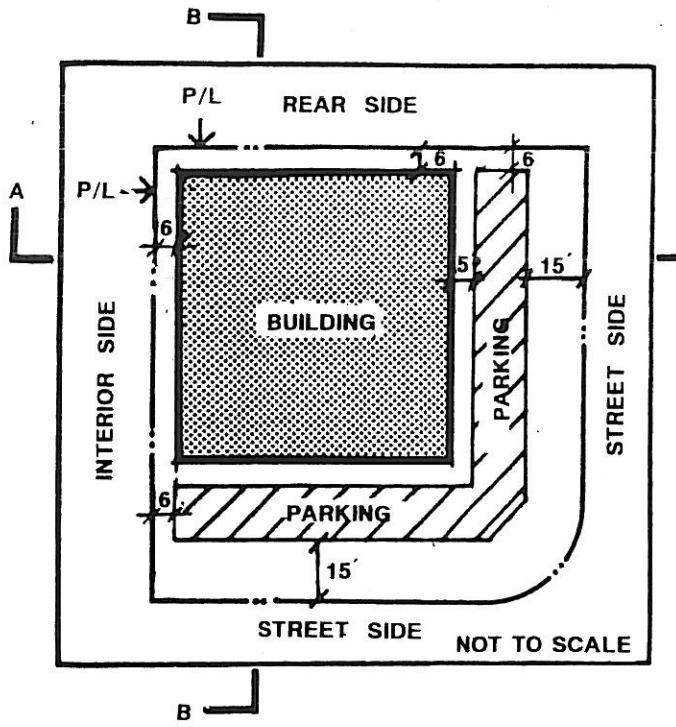


## MINIMUM PARKING AND LANDSCAPE SETBACKS

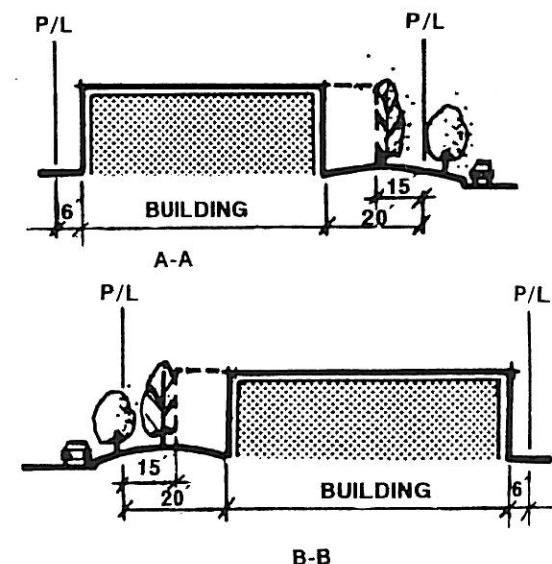
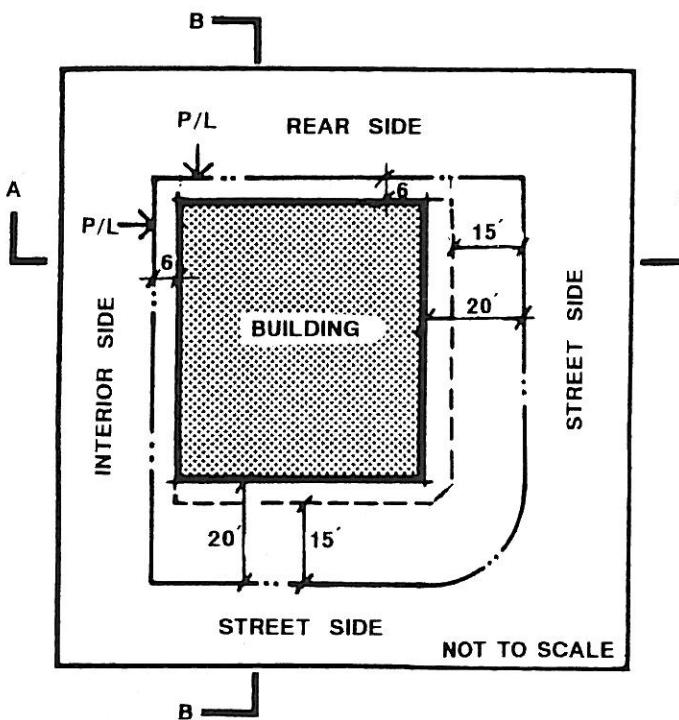
# CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# SETBACKS: INDUSTRIAL COLLECTOR STREETS



**MINIMUM PARKING AND  
LANDSCAPE SETBACKS  
(CORNER PARCEL)**

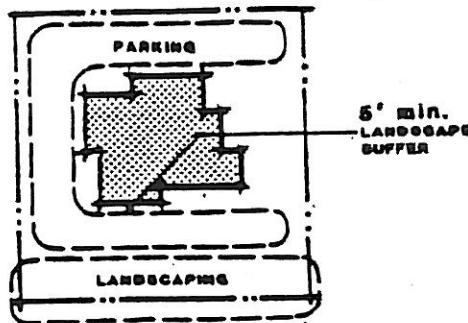


**MINIMUM BUILDING  
SETBACKS  
(CORNER PARCELS)**

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

Buildings should be located on "turf islands", where the office portion of the building does not directly abut paved parking areas. A 5' to 7' minimum landscape strip shall be required between parking areas and the office portion of a structure.



- Minimum landscape coverage for parcels lying within the planning area shall be fifteen (15 %) percent of the net lot area (minus ROW, building foot print, and areas used for outdoor storage or loading).
- Contiguous to streets within the Fontana Gateway Specific Plan, parking areas will be screened with berms an average height of three feet and have a maximum slope not to exceed 3:1.
- All landscaped areas shall be served by a City approved automated irrigation system.
- In the event that a lot or parcel is not totally developed, the developer/user will be responsible for irrigating, seeding, and maintaining the undeveloped portion of the site. Areas of such lot or parcel proposed for development in the future phase shall be temporarily turfed, seeded, and irrigated with an automatic sprinkler system for dust and soil erosion control, if construction will not begin within six (6) months. This requirement shall apply to developer/user.
- All on-site landscaping shall conform to the City of Fontana, Parks Department, Standard Landscape Specifications Manual, 1986 edition. If the Specific Plan conflicts with a City Standard, then the Specific Plan shall control.
- Provide berthing in conjunction with landscaping at the building edge to reduce building mass and height along facades.
- To screen parking, a consistent, rolling earth berm, low screen wall or combination thereof shall be provided.
- Non-living landscaping, such as gravel, bark, or "astro-turf" are not allowed as a substitute for plant materials in landscape areas.
- Landscaping around the entire visible base of buildings is required. A minimum of 1 - 15 gallon tree shall be provided for every 30 linear feet of building face which has public exposure. The trees shall be

located in a 5 to 7 foot wide planting area which includes shrubs and ground cover.

- o A minimum of 1 – 15 gallon tree shall be provided for every 30 linear feet of interior boundary on all side property lines. The trees shall be located in a minimum 6 foot wide planting area.
- o Landscape materials should enhance the major architectural design elements through the coordinated use of shrub and leaf colors, tree plant material masses and lighting.
- o No planting area shall be considered a "landscape area" unless it contains at least 24 square feet of area and is a minimum of 4 feet in width.

#### **4.6.2 Plant Palette: On-site Landscaping**

**The following list is a primary planting palette for on-site landscaping including both the botanical and common name of plants recommended for use in the Specific Plan area.**

**TABLE 5**  
**ON-SITE AREAS AND BUILDING AREAS**

#### **Trees**

<i>Arbutus unedo</i>	Strawberry Tree
<i>Cinnamomum camphora</i>	Camphor Tree
<i>Koelreuteria paniculata</i>	Goldenrain Tree
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Platanus aceriflora</i>	London Plane
<i>Prunus serrulata</i>	Japanese Flowering Cherry
<i>Populus Hybrid</i>	Lombardy Poplars
<i>Pyrus calleryana 'Bradford'</i>	Bradford Pear
<i>Pyrus kawakami</i>	Evergreen Pear
<i>Ulmus parvifolia</i>	Evergreen Elm
<i>Cypressocyparis 'Leylandia'</i>	Leylandia Cypress
<i>Eucalyptus malculata</i>	Spotted Gum

#### **Shrubs**

<i>Abelia grandiflora</i>	Abelia
<i>Agapanthus africanus</i>	Lily of the Nile
<i>Asparagus sprengeri</i>	Asparagus
<i>Cissus rhombifolia</i>	Grape Ivy
<i>Hemerocallis hybrid</i>	Day Lily
<i>Nandina domestica</i>	Heavenly Bamboo
<i>Raphiolepis indica</i>	India Hawthorne
<i>Pittosporum tobira</i>	
<i>Pittosporum t. variegata</i>	

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF  
FONTANA APPROVING FONTANA GATEWAY SPECIFIC PLAN  
AMENDMENT #1 TO AMEND THE TEXT REQUIRING  
DEVELOPMENT TO COMPLY WITH THE COUNTY HAZARDOUS  
WASTE MANAGEMENT PLAN.

WHEREAS, the City Council has found this amendment to be consistent with the adopted Fontana Area General Plan; and

WHEREAS, in consideration of the evidence received at the hearing, and for the reasons discussed by the City Council, the City Council now finds as follows:

1. The proposed Amendment is consistent with the General Plan which designates this property as Specific Plan #13 Fontana Gateway. Reference is made to the County Hazardous Waste Management Plan, page 11 of the Safety Element of the General Plan. This amendment will assist in implementation of the County Hazardous Waste Management Plan.
2. Maximum flexibility will still be maintained at the point of Design review.
3. Increasing awareness of developments that use hazard waste or material by increasing the noticing requirement and complying with the CHWMP will help to assure proper services are available and emergency response plans are instituted.
4. Each individual project proposed in any of the three Specific Plans will require its own circulation review at time of submittal. Circulation patterns are not affected by this amendment.
5. Compliance with the CHWMP assists in proper location of facilities. Chapter 5 of the CHWMP contains recommended siting criteria.
6. Adequate aesthetic and quality control will be assured at the time of design review application.
7. Both compliance with CHWMP, the increased noticing requirement and any environmental work to be

performed per CEQA on a particular project should address and mitigate environmental concerns. The natural environment will not be effected by this Amendment.

8. Increased noticing and compliance with the CHWMP should have little effect on unique or innovative land use. Development techniques may be more innovative in order to protect the surrounding community from exposure to hazardous materials.
9. Implementation of the amendment will occur during the design review. Continued implementation will occur with the review that is required by the CHWMP.
10. The urban character outlined in this amendment will occur at the time of design review consideration as well as during review of the landscape plans.
11. The General Plan's Safety Element addresses the Hazardous Waste Management Plan (HWMP). This amendment will help assure implementation of the HWMP.
12. A Negative Declaration is recommended for this project. The Fish & Game De Minimis finding is also being recommended as there is no evidence before the city that the proposed project will have potential for an adverse effect on wildlife resources.

NOW, THEREFORE, be it resolved, determined and ordered by the City Council of the City of Fontana that the Fontana Gateway Specific Plan is hereby amended as follows:

1. The Fontana Gateway Specific Plan shall be amended to add subsection 4.2.14 to the Table of Contents to read:

4.2.14 Hazardous Waste Management Plan

2. The Fontana Gateway Specific Plan shall be amended to add subsection 4.2.14 to Chapter 4.0 Development and Design Regulations, section 4.2 General Provisions to read:

4.2.14 Hazardous Waste Management Plan

All uses and/or operation shall comply with the HAZARDOUS WASTE MANAGEMENT PLAN per Section 33-202 of the Code of the City of Fontana.

APPROVED AND ADOPTED this 6th day of August 1991.

READ AND APPROVED:

Stephen P. Aertsch  
City Attorney

I, Kathy Montoya, City Clerk of the City of Fontana, California, and Ex-Officio of the City Council do hereby certify that the foregoing resolution is the actual resolution duly and regularly adopted by the City Council at a regular meeting on the 6th day of August 1991, by the following vote, to-wit:

AYES: Mayor Kragness, Council Members Abernathy, Eshleman, Murray

NOES: None

ABSENT: Council Member Boyles

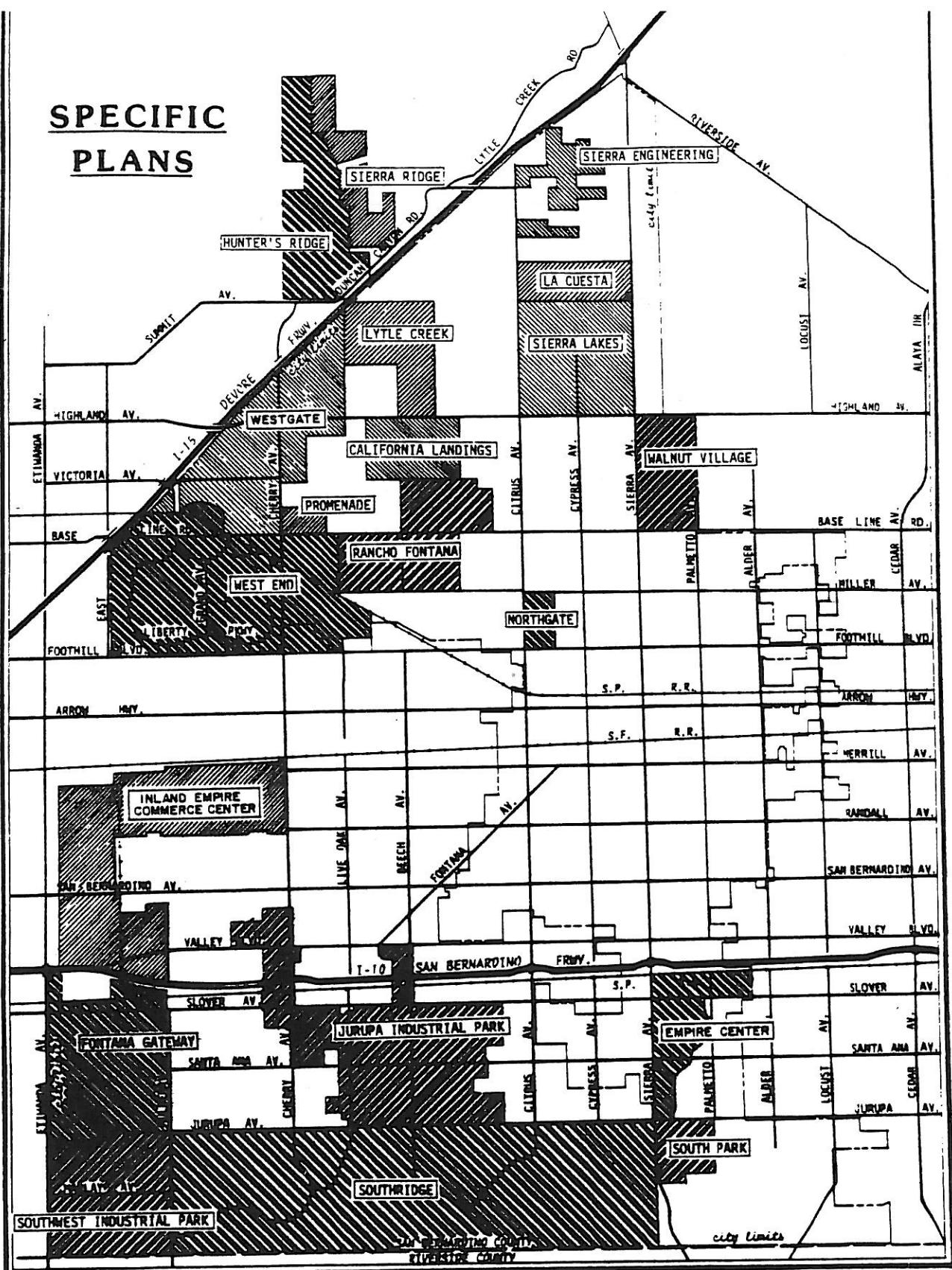
Kathy Montoya  
City Clerk of the City of Fontana

Mark J. Kragness  
Mayor of the City of Fontana

ATTEST:

Kathy Montoya  
City Clerk

# SPECIFIC PLANS



## CITY OF FONTANA CALIFORNIA



Scale: 0 miles  
2000' 0' 2000' 4000'  
Scales

**SPECIFIC PLANS**

**PROPOSED SPECIFIC PLANS**

**ATTACHMENT 1**

3 by 1990

**APPENDIX M  
NOTICE OF NEGATIVE DECLARATION**

City of Fontana  
Planning Department  
8353 Sierra Avenue  
Fontana, CA 92335

**Date of Declaration:** April 26, 1991  
**Effective Date:** June 10, 1991  
**Applicant:** City of Fontana  
**Address:** 8353 Sierra Avenue  
Fontana, CA 92335  
**Project Title:** Fontana Gateway Amendment #1  
**Env. Log:** Env. #91-09

**Description of Project:** To amend the Fontana Gateway Specific Plan to provide a clause for compliance to the County Hazardous Waste Management Plan.

(Plans and specifications are available for public inspection at the public counter of the Planning Division)

Pursuant to the provisions of the California Environmental Quality Act of 1970 (Public Resources Code, Sections 1500. et seq.) the City of Fontana has determined that the above-referenced project will not have a significant effect upon the environment. An environmental impact report will not be required.

The City of Fontana has also determined that the proposed project will not have potential for an adverse effect on wildlife resources and a Fish & Game De Minimis finding is being recorded.

**Appeal Period**

This decision may be appealed to the City Council by any person aggrieved, or by an officer, board, department, or commission of the City. Any appeal shall be filed with-in fourteen (14) days after the publication of this decision or it shall be dismissed by the City Council. The notice of Appeal shall be in writing and shall be filed in duplicate in the office of the City Clerk upon forms provided by the Planning Department.

---

Kurt Anderson  
Deputy Planning Manager

CONDITIONAL APPROVAL GRANTED: YES(  ) NO(  )

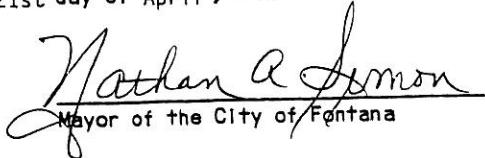
AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF FONTANA  
ADOPTING THE FONTANA GATEWAY SPECIFIC PLAN (SPECIFIC  
PLAN NO. 13)

THE CITY COUNCIL OF THE CITY OF FONTANA DOES ORDAIN AS  
FOLLOWS:

SECTION 1: The Fontana Gateway Specific Plan (Specific Plan No. 13) in its entirety is hereby adopted by reference, a copy of which is on file in the City Clerk's office.

SECTION 2: This ordinance shall take effect thirty (30) days after the date of its adoption and prior to the expiration of fifteen (15) days from the passage thereof shall be published at least once in the Herald News, a newspaper of general circulation, published and circulated in the City of Fontana and thenceforth and thereafter the same shall be in full force and effect.

APPROVED AND ADOPTED this 21st day of April, 1987

  
Nathan A. Simon  
Mayor of the City of Fontana

ATTEST:

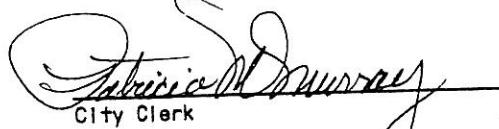
  
Patricia M. Murray  
City Clerk

I, Patricia M. Murray, City Clerk of the City of Fontana and Ex-Officio Clerk of the City Council, do hereby certify that the foregoing ordinance, which was introduced at a regular meeting of said City Council on the 7th day of April, 1987 was finally passed and adopted not less than five days thereafter on the 21st day of April, 1987, by the following vote, to-wit:

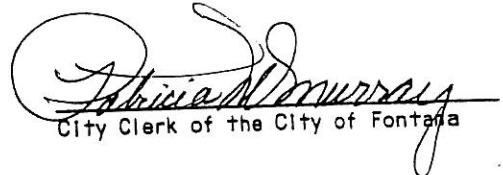
AYES: Mayor Simon, Councilmen Boyles, Day, Koehler, Kragness

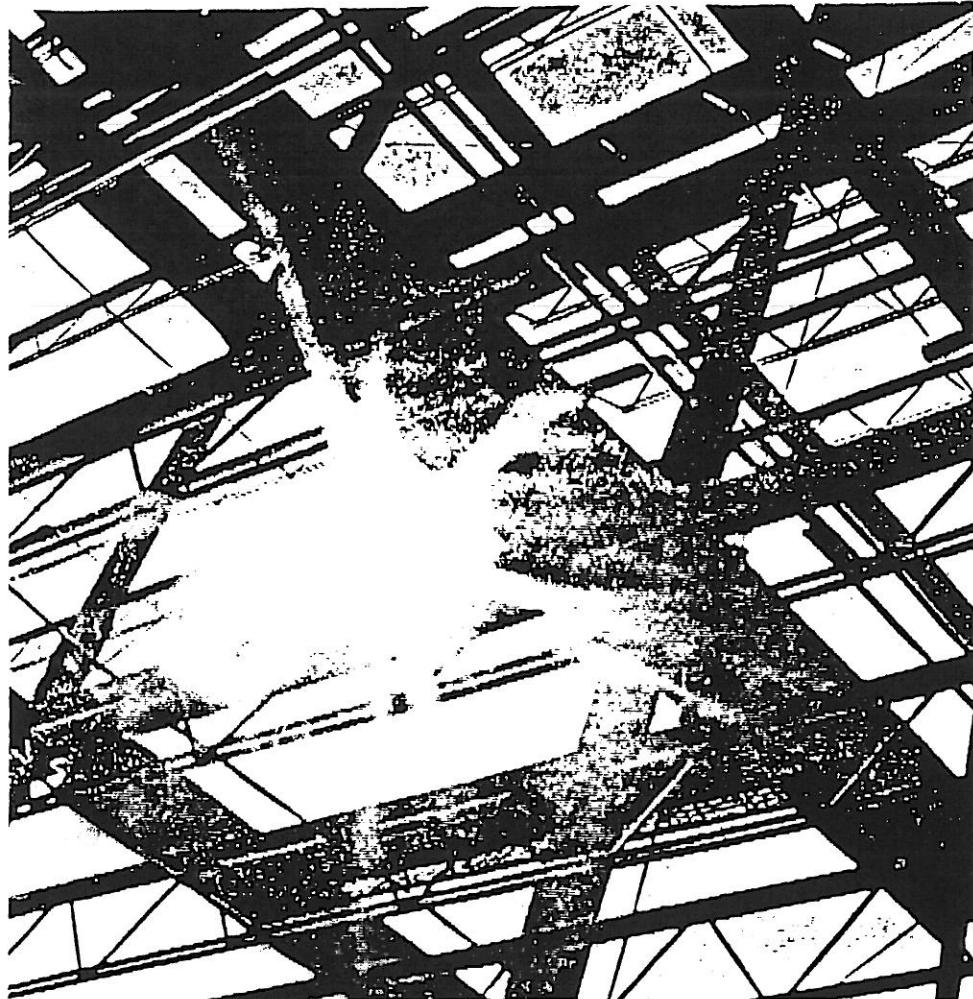
NOES: None

ABSENT: None

  
Patricia M. Murray  
City Clerk

I further testify that said ordinance was thereupon signed by the Mayor of the City of Fontana.

  
Patricia M. Murray  
City Clerk of the City of Fontana



(714) 350-7629



**CITY OF FONTANA**

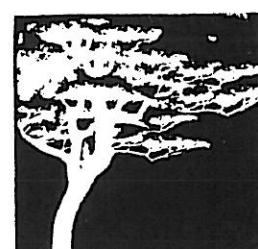
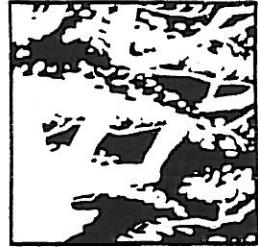
CALIFORNIA

PLANNING DEPARTMENT

A Division of the Development Agency

CRAIG L. BRUORTON  
Senior Planner  
Current Planning

8353 Sierra Ave.  
Fontana, CA 92335



---

## **CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN**

---

**WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK**

**FONTANA GATEWAY SPECIFIC PLAN  
INCLUDING SIERRA INDUSTRIAL PARK  
AND  
GENERAL PLAN AMENDMENT 86-4**

**Prepared for:**

**THE CITY OF FONTANA**

**Prepared by:**

**WILLIAMS - KUEBELBECK & ASSOCIATES**

**FORMA - PLANNING NETWORK**

**JULY 1986**

**FIFTH DRAFT: APRIL 1987**

## FONTANA GATEWAY SPECIFIC PLAN

### ACKNOWLEDGEMENTS

#### CITY COUNCIL

**Nathan A. Simon**  
**Gary Boyles**  
**Don Day**  
**Charles A. Koehler**  
**William Kragness**

#### PLANNING COMMISSION

**Byron Hunsaker, Chairman**  
**E. H. Cain**  
**Harold W. Clark, Jr.**  
**Charles Dattilo**  
**Thomas D. Mesa**  
**Gil Tokuhara**  
**Tammy Wolfinbarger**

#### CITY STAFF

**Jack Ratelle II, City Manager**  
**Nell Stone, Development Agency Director**  
**Peter Broy, Planning Director**  
**Gary Van Osdel, Project Manager**

#### PROJECT SPONSOR

**FG Associates**

#### CONSULTANTS

**City consultant team:**  
**Williams Kuebelbeck & Associates, Inc.**  
**FORMA/Planning Network**

**Developer consultant team:**  
**Gary Mitchell & Associates**  
**Krueper Engineering & Associates**  
**Realvest Inc.**

**FONTANA GATEWAY SPECIFIC PLAN AMENDMENTS**

**FINAL TEXT APPROVED:**

<b>AMENDMENT NUMBER</b>	<b>DESCRIPTION</b>	<b>DATE</b>	<b>PAGE NUMBERS</b>
1	Development to comply with CHWMP	CC Resolution 91-171 8/8/91	Table of Contents Page 27

### Vines and Groundcover

<i>Bougainvillea brasiliensis</i>	<i>Bougainvillea</i>
<i>Calliandra inaequilatera</i>	<i>Pink Powder Puff</i>
<i>Distictus buccinatorius</i>	<i>Trumpet Vine</i>
<i>Gelsemium sempervirens</i>	<i>Carolina Jessamine</i>
<i>Hedete helix</i>	<i>English Ivy</i>
<i>Jasminum mesnyi</i>	<i>Primrose Jasmine</i>
<i>Macfadyena unguis-cati</i>	<i>Cat's Claw</i>
<i>Lonicera halliana</i>	<i>Honeysuckle</i>
<i>Trachelospermum jasminoides</i>	<i>Star Jasmine</i>

### Parking Lot Trees

<i>Ceratonia siliqua</i>	<i>Carob</i>
<i>Cinnamomum camphora</i>	<i>Camphor Tree</i>
<i>Magnolia grandiflora</i>	<i>Southern Magnolia</i>
<i>Platanus acerifolia</i>	<i>London Plane Tree</i>
<i>Populus hybrid</i>	<i>Hybrid Poplar</i>
<i>Schinus terebinthifolius</i>	<i>Brazilian Pepper</i>
<i>Tipuana tipu</i>	<i>Tipu Tree</i>
<i>Brachychiton populneus</i>	<i>Bottleneck Tree</i>

### Screening Trees

<i>Cupressocyparis leylandii</i>	<i>Leland Cypress</i>
<i>Pinus halepensis</i>	<i>Aleppo Pine</i>
<i>Pinus pinea</i>	<i>Italian Stone Pine</i>
<i>Brachychiton populneas</i>	<i>Bottleneck Tree</i>
<i>Eucalyptus nicholii</i>	<i>Willow Leaf</i>

## 4.7 ARCHITECTURAL DESIGN REGULATIONS

The following design regulations will be considered and evaluated when a new development is proposed in the Fontana Gateway Specific Plan Area. By describing the variety of site improvements, providing examples of appropriate and inappropriate design solutions and illustrating design techniques available, the following design criteria categories will serve to direct development into a cohesive design statement.

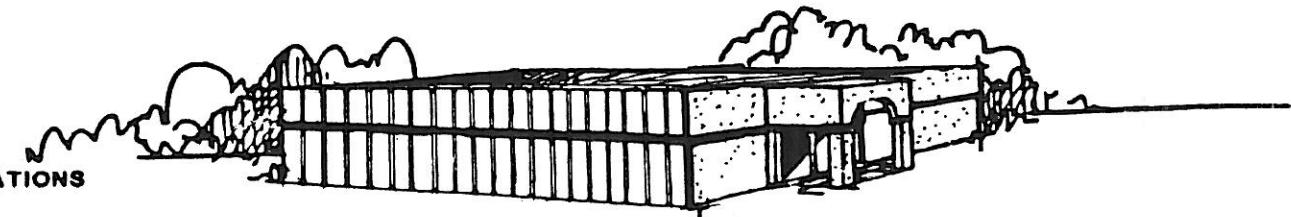
### 4.7.1 Basic Form (See Figure 20)

- o Employ variety in building forms, to create visual character and interest.
- o Avoid long, "unarticulated" building facades. Building facades with varied front setbacks are strongly encouraged.
- o Avoid blank front and side wall elevations on street frontages.
- o Entries to industrial buildings should portray a quality office appearance while being architecturally tied into the overall building mass and building composition.

## APPROPRIATE

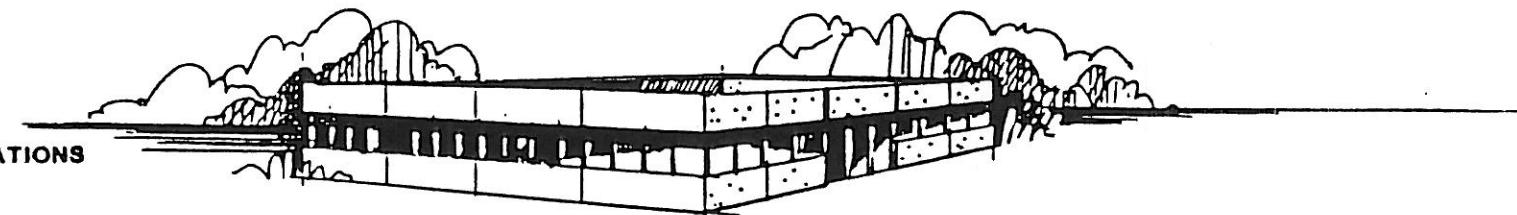
METAL BUILDING WITH:

- ENTRY INDENTATION OR PROJECTION
- COLOR BANDS
- SOME TEXTURED WALL ELEVATIONS (SAND BLASTED, RIBBED CONCRETE, CONCRETE AGGREGATE, METAL)



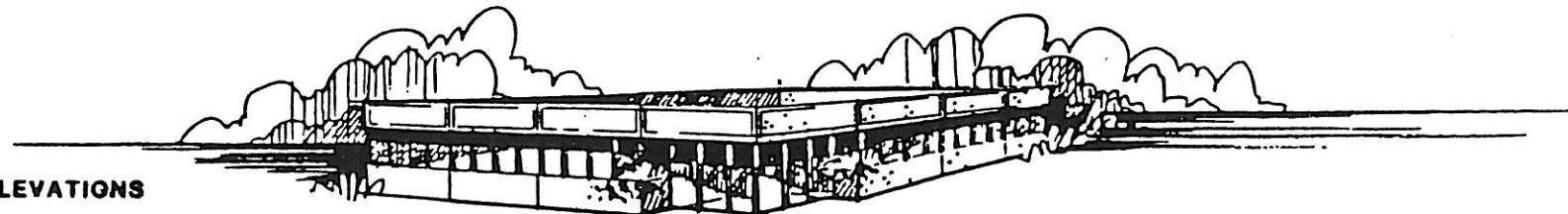
## APPROPRIATE

- VERTICAL SEAMS
- WINDOW GLAZING BAND
- SOME TEXTURED WALL ELEVATIONS (SAND BLASTED, RIBBED CONCRETE, CONCRETE AGGREGATE)



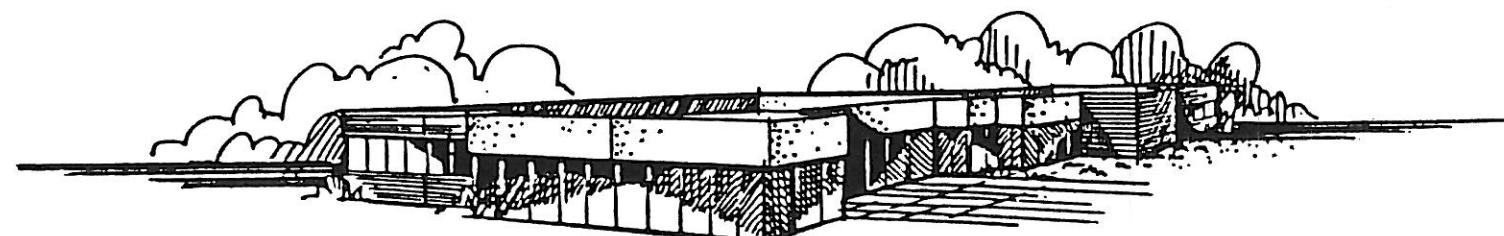
## APPROPRIATE

- WINDOW GLAZING
- COLOR BANDS
- SOME TEXTURED WALL ELEVATIONS



## APPROPRIATE

- MIXES COMPLIMENTARY MATERIALS
- PROVIDES ARTICULATION
- INCREASES WINDOW AREA
- WINDOW GLAZING
- SOME TEXTURED WALL ELEVATIONS



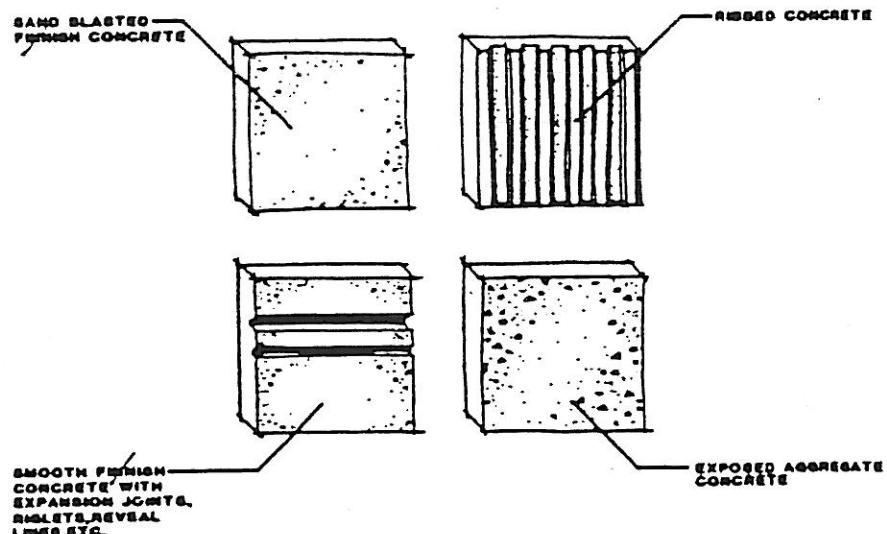
- o Windows and doors are key elements of any building's form, and should relate to the scale of the elevation on which they appear. Windows and doors can establish character by their rhythm and variety. Recessed openings help to provide depth and contrast on elevation planes.
- o Sensitive alternation of colors and materials can produce diversity and enhance architectural forms.
- o The staggering of planes along an exterior wall elevation creates pockets of light and shadow, providing relief from monotonous, uninterrupted expanses of wall.

#### 4.7.2 Style

- o Although no particular "style" is being suggested for the specific plan, use a clear consistent design solution. Avoid confusion of forms, scales, materials and details.
- o Avoid mixing styles, i.e. rustic wood shingle and polished chrome or a "colonial" front on a large and otherwise industrial building.

#### 4.7.3 Material

- o The sensitive use of various siding materials, whether metal, masonry, concrete texturing, cement or plaster can produce effects of texture and relief that provide character. The direction and interval of linear elements, such as joints and ribs in architectural metal wall panels are important factors in establishing rhythm.
- o Avoid exterior materials with high maintenance responsibilities such as stained wood, clapboard, or shingles.
- o Where appropriate, choose wall materials that will withstand abuse by vandals or accidental damage from machinery or inclement weather conditions such as wind generated sand erosion.
- o Provide variety in the surface of exterior walls with pilasters, deep reveals at construction joints, staggering of wall surfaces and other measures.
- o All Metal Buildings shall be architecturally designed providing variety and visual interest to the streetscape.



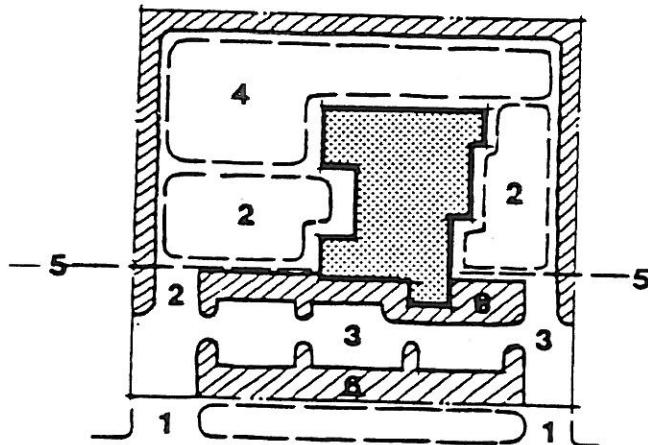
**EXAMPLES OF ARCHITECTURAL TEXTURES**

#### 4.7.4 Color

- o Blending of compatible colors in a single facade or composition is a good way to add character and variety while reducing or breaking up building scale.
- o Utilize lower wall wainscots or horizontal color bands to add interest and break up vertical monotony.
- o Avoid "Day-Glo" colors, fluorescent reds and greens and unusual patterns such as zebra stripes and color schemes.
- o Coordinate color and finishes on exteriors of all elevations to provide a total continuity of design. Exterior finishes shall be required only on walls visible from streets.

#### 4.8 SITE DESIGN REGULATIONS (See Figures 13-15, 21-25 )

- o The main elements of sound industrial site design are illustrated below and include:



- 1 -Controlled site access
- 2 -Service areas are acceptable at the sides and rear of buildings
- 3 -Convenient public access and visitor parking
- 4 -Screening of storage, work areas, and utility equipment
- 5 -Storage setbacks screen wall locations
- 6 -Emphasis on the main building entry and landscaping

- o Avoid long linear building expanses within the development envelope and along the streetscape through the provision of varied setbacks.

- o Buildings shall be sited in a manner that will complement the adjacent buildings and landscape. Building sites should be developed in a coordinated manner to provide order and diversity along the street scene.

##### 4.8.1 Parking and Loading Requirements (See Figures 13-15, 21-25)

Off-street parking and loading requirements have been provided in order to achieve adequate area for vehicular parking and circulation. The following parking and circulation criteria shall apply to the Specific Plan area:

- o Required parking spaces shall be determined at the following rates:

**-Warehousing or building for storage:**

**1 space per 1200 square feet of gross floor area or 1 space per employee whichever is greater**

**-Industrial/Manufacturing:**

**1 space per 600 square feet of gross floor area or 1 space per employee whichever is greater**

**-Office and Administration:**

**1 space per 250 square feet of gross floor area or 1 space per employee whichever is greater**

**-Multi-use tenant where office use does not exceed 35% of building area or "space" buildings:**

**1 space per 250 square feet of gross floor area or 1 space per employee whichever is greater**

**-Research and development**

**1 space per 250 square feet of gross floor area or 1 space per employee whichever is greater**

**-Service Commercial**

**1 space per 250 square feet of gross floor area or 1 space per employee whichever is greater**

**o Required parking shall be located on the same site with the main use of the building, or on premises contiguous thereto, or in a location in accordance with an approved master plan.**

**o Twenty-five percent (25%) of all required parking stalls may be devoted to compact car use. Minimum stall dimension shall be 7 feet in width and 16 feet in length and marked for compact cars.**

**o All parking areas shall be screened from public view through the use of berms, landscaping materials and/or low screen walls.**

**o Each parking space shall be accessible from a street or alley, provided that no parking space shall be designed to require vehicles to back into a street. Parking facilities shall be designed to insure that no vehicle need enter a public street in order to progress from one aisle to another within the same lot.**

**o Ingress and egress design shall be subject to approval of the parking lot design. The minimum standards for curb cut dimensions shall coincide with City standards. Along Mulberry Avenue, curb cuts shall be limited to 300 feet on center. Reciprocal access shall be encouraged, however, no more than two parcels may share an access drive.**

**o Minimum turning radius for two-way drives is 25 feet.**

**o One- and two-way access drives may be reduced for diagonal parking lot design in conformance with the City standards.**

- o Minimum parking space dimensions shall be 9 feet by 19 feet (compact spaces: 7' x 16').
- o Employee parking lots for cars shall be paved with hard, durable, plant mix asphaltic concrete at least 2 inches thick after compaction or with Portland concrete at least 3 inches thick.
- o Bumper guards, wheel stops, pavement markings and other vehicle control devices shall be provided.
- o Parking spaces shall not block access for waste removal pick-up.

#### 4.8.2 Loading Facilities (See Figure 13-15, 21-25)

- o All loading facilities and maneuvering areas must be on site with the use. Vehicular maneuvering, for the purpose of loading or unloading, shall not be permitted on any public right-of-way.
- o All loading facilities (roll-up doors, dock high doors) shall be permitted only in the rear and interior side yard of parcels located contiguous to Etiwanda, Jurupa, Slover and Mulberry. Roll-up doors and dock high doors shall not be located on front building elevations facing Etiwanda, Jurupa, Slover, and Mulberry.
- o Loading docks shall be set back and located on-site in a manner which does not use a public street for back-up.
- o Loading facilities shall be screened from public view.
- o Entrances and exits for off-street loading facilities shall be provided at locations approved by the Public Works Department and Development Agency.
- o Loading facilities including dock-high and roll-up doors, shall be allowed contiguous to parcels fronting interior streets only, provided that proper screening amenities are provided. (See Figures 21-25 for prototypical examples.)
- o Loading docks shall be setback a minimum of 100 feet from required screen walls. (See Figures 21-25 for prototypical examples.)

#### 4.8.3 Rail Service

- o Parcels lying adjacent to railroad lines shall be designed in a manner which ensures the full potential for future rail access. However, none of such parcels shall be required to have rail access. Regulations related to the provision of railroad spur lines shall be per the Public Utilities Commission and each respective railroad.

#### 4.8.4 Parking Lot Design Standards

- o All parking spaces shall be clearly marked on the surface of the parking facility.

- o Parking facilities shall be designed so that a car within a facility will not have to enter a street to move from one location to another within the same parking facility.
- o Parking facilities shall be designed in such a manner that any vehicle on the property will be able to maneuver as necessary so that it may exit from the property traveling in a forward direction.
- o Entry statements (landscaping, monumentation) shall be encouraged for entry into parking lots.

#### 4.8.5 Parking Lot Landscaping Design Standards

- o The following delineates how many trees are to be located in parking lot areas. This landscaping is to be credited toward the landscaping percentages required for each property.

<u>Parking Area Type</u>	<u>Trees per Five Parking Spaces</u>
All	One
	<ul style="list-style-type: none"> <li>o Trees may be clustered for design effect throughout the entire parking lot.</li> <li>o Each tree must be located in a planter area of not less than 24 square feet.</li> <li>o Trees shall be located throughout the parking lot and not simply at the ends of parking aisles. In order to be considered within the parking lot, the trees must be located in planters that are bounded on at least 3 sides by parking area paving. Only trees located in landscaped "fingers" or "islands" can count towards parking lot landscaping.</li> <li>o The public periphery of parking areas shall be screened from view using any of the following methods; building masses, plants/landscaping, berms, walls.</li> <li>o Landscaping shall be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, and the use of curbs.</li> <li>o All trash enclosures shall be constructed of solid masonry or concrete tilt-up walls (8 feet high maximum) with a 6" high concrete interior curb. Trash enclosure doors shall be solid metal.</li> </ul>

#### 4.9 WALL AND FENCE DESIGN REGULATIONS (See Figures 14, 21-25)

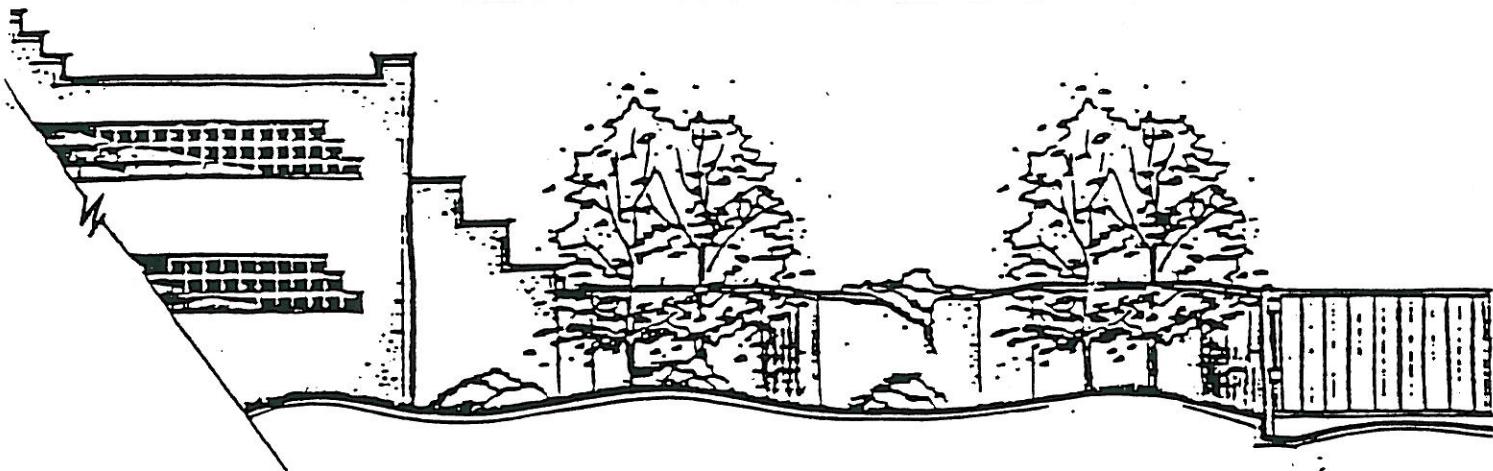
Walls will serve a major function in the landscape and will be used to screen automobiles, loading and storage areas, and utility structures. The intent is to keep the walls as low as possible while performing their screening function.

##### 4.9.1 Material

- o All walls will be of natural color concrete or concrete masonry with

a grid pattern and light sandblast finish. Variations, such as split face concrete block, ribbed concrete, stucco finish block, or heavy sandblasted concrete are acceptable. In no case shall unfinished common construction block be provided.

- o Alternative screening methods, such as the use of architectural wing walls, or building massing shall be allowed if in substantial architectural conformance with the main structure.



#### 4.9.2 Configuration

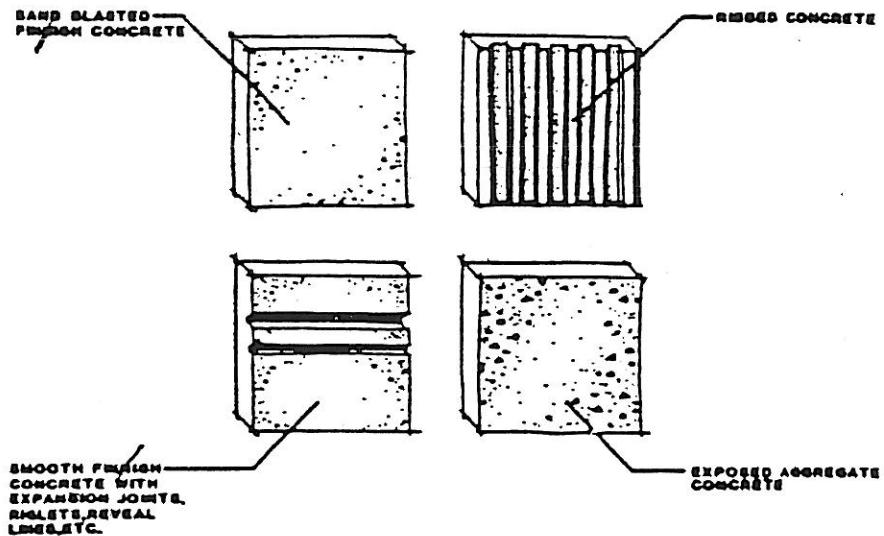
- o Walls shall not be continuous around a property if they are not necessary for screening purposes.

#### 4.9.3 Height

The following are acceptable height ranges for screening walls from street side as measured from the highest adjacent grade:

- o Parking walls: 2' to 8'
- o Loading Areas: 6' to 12'
- o Garbage Dumpster: 8' or the height of the dumpster, whichever is higher
- o Utility Structures: As necessary to screen. (Maximum 12')
- o Signage Walls: To meet heights specified in signage section of these guidelines.

o Storage/Screen Walls: 8' to 12'



**EXAMPLES OF SCREENWALL TEXTURES**

**4.10 SIGN REGULATIONS**

**4.10.1 Permitted Signs**

Signs shall be used for identification, direction and establishing uniform sign regulation for the project area. The intent is to permit adequate signing while discouraging unnecessary and unsightly signs. All signage within each planning area shall conform to the sign standard matrix (Table 6).

**4.10.2 Sign Programs**

Sign programs shall be required and shall comply with the above criteria.

- o A Sign Program is intended to encourage creativity and innovativeness; expressing variety and diversity in design. At the same time, however, the program should foster the consistent application of materials and styles.
- o In addition to the requirements of this section, the application for a Sign Program shall be accompanied by the following documents:
  - Coverage area: A map, drawn to scale, delineating the site proposed to be included within the Sign Program.
  - Building elevation: Drawings and/or sketches indicating the exterior surface details of all structures on the site.
  - Signage: Drawings of a uniform scale shall be used to indicate

the sign copy size, method and intensity of illumination, height, sign area and general location of all signs.

- o Applications for monument signs shall be accompanied by scale drawings indicating the size, materials, sign copy, colors, method and intensity of illumination, height, sign area and general location of all signs on the building site.

#### **4.10.3 Prohibited Signs**

- o Outdoor advertising signs
- o Outdoor advertising structures
- o Roof signs
- o Freestanding signs, except as provided in this text
- o Advertising devices and advertising displays
- o Rotating, revolving, flashing or moving signs
- o Banners
- o Flagpoles used for advertising
- o Vehicles or other signs or devices in the public right-of-way used as advertising devices or displays
- o Vehicles or other signs or devices not permitted by this section used as advertising devices or displays.

#### **4.10.4 General Requirements**

- o No freestanding sign or structure shall be permitted closer than five (5) feet from the property line.
- o Any illuminated sign or lighting device shall employ only lights emitting a light of constant intensity, and no sign shall be illuminated by or contain flashing, intermittent, rotating or moving lighting or lights. In no event shall an illuminated sign or light device be so located or so directed as to permit the illumination to be directed upon a public street, highway, sidewalk or adjacent premises so as to cause glare or reflection that may constitute a traffic hazard or nuisance.

### **4.11 SIGN DESIGN REGULATIONS**

#### **4.11.1 General Requirements**

Signs and other graphic devices should be used to take advantage in defining architectural scale, adding accent color, variety and character to buildings within the Specific Plan area. These Design Standards are based on the following characteristics of signs:

- o **CHARACTER** – The overall character of the signage should reflect a quality, industrial image ("high tech"), with a clean, uncluttered appearance.
- o **SIGN TYPE AND SIZE** – There are two basic types of identification signs allowed in the Fontana Gateway Specific Plan Area; wall and monument.

- o **DESIGN AND CONFIGURATION** – All signs, except traffic regulation signs, for each parcel will be designed as a single system of like materials and construction methods and shall follow these guidelines.

–Architectural Style. Each sign is to be designed to relate to the architectural style of the main building or buildings upon the site, and shall incorporate at least one of the predominantly visual design elements common or similar to all such buildings.

Each sign will be composed of one piece. It will have no moving parts. (See Graphics)

- o **COLOR** – All signs, except temporary signs, traffic regulating signs and logos, will have a maximum of two colors, one for the face, posts and base, and another for the lettering. A third color may be introduced for a complex with two or more buildings, when used in a consistent manner to distinguish buildings or phases. When a business name is in itself a business logo, it is exempt from color restrictions.

- o **LETTERING STYLE** – All lettering on all signs, except traffic regulating signs and corporate logos, will be of the same lettering style on each parcel. Clean, sleek, industrial oriented lettering such as italicized Helvetica shall be encouraged.

- o **MATERIAL** – All sign posts shall be of a permanent, quality construction of: aluminum, copper, brass or sheet metal; if painted, paint with a fade-resistant, durable, exterior finish; edges shall be eased with no visible welds; natural stone or concrete with free-standing letters or letters cast into sign are also allowed. Exposed bases shall have a smooth, durable, even architectural finish.

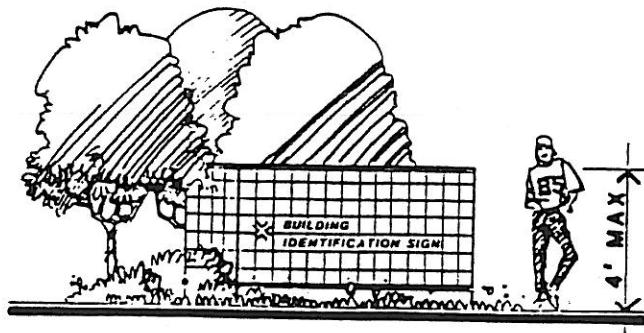
- o **ILLUMINATION** – Detached identification signs may be illuminated by continuous and uniform internal illumination, or ground lighting.

Identification wall signs may be illuminated by internal illumination (individual channel letters) provided that the color and intensity of such lighting appears as an integral part of the overall architectural and site concept.

No sign illumination should cast a glare which will be visible from any street or access drive.

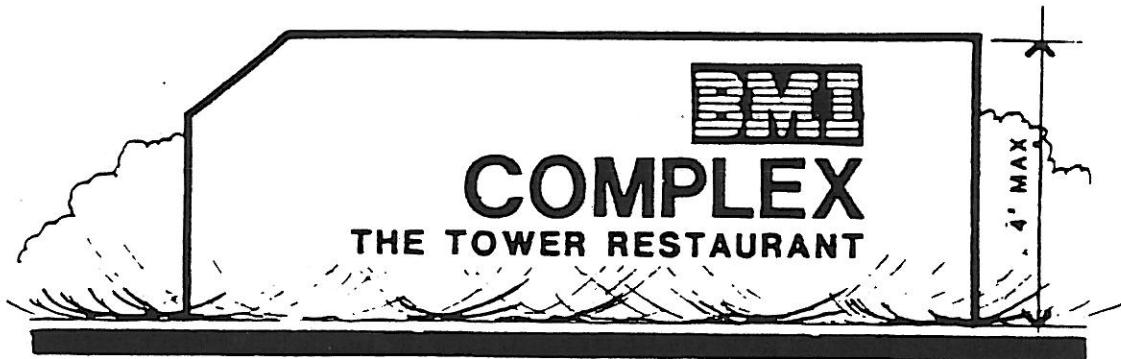
- o **SETTING** – Monument signs shall be integrated into the landscape. Turf berms and colorful accent plantings are encouraged to define and

enhance monument signage.



#### **4.11.2 Business Identification Monument Sign**

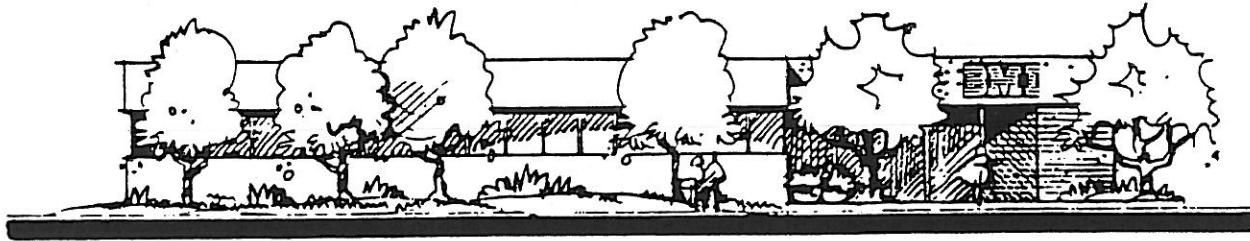
- o This sign identifies a single building or a cluster of buildings which is an entity. It may identify no more than one tenant in addition to the complex namesake (i.e. BMI Building and The Tower Restaurant.)**



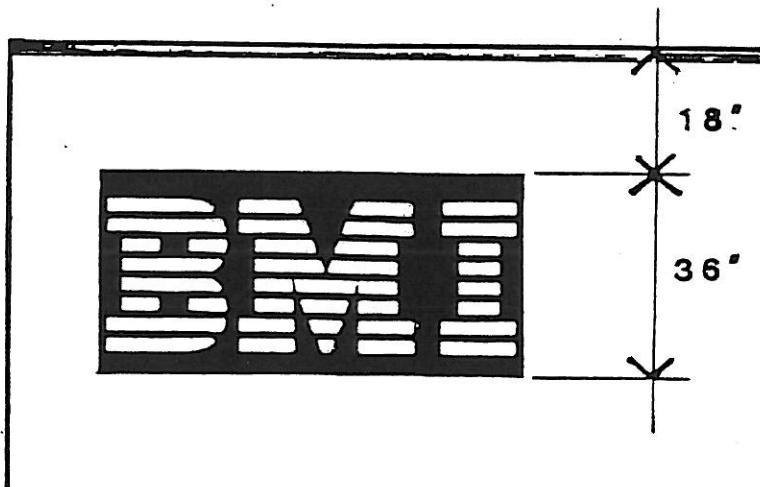
#### **4.11.3 Wall Signs (Building Mounted)**

- o A building mounted sign is limited to the display of the building name and addresses or the name and/or symbol (logo) of the tenant or business occupying the building, and the building address. No more than the name of one tenant or**

the building name may be displayed, together with the building address.



- o Only the name and/or symbol in free standing individual channel type letters (no sign board or canned signs) or letters integral with the architecture style should be allowed. The symbol or logo and letters may be used. Symbols or logos, and names should be integral and proportional with the architecture and other building signage.



- o In no case may the top of the sign be closer to the roofline than one half the vertical dimension of the sign.

TABLE 6

1 OF 4

CLASS	SIGN TYPE	MAXIMUM NUMBER	MAXIMUM SIGN AREA	MAXIMUM HEIGHT	PLACEMENT FORM	MATERIAL	MESSAGE/LAYOUT	TYPE-COLOR	ILLUMINATION	LANDSCAPE	REMARKS	
											OGRAPHY	
BUSINESS IDENTIFICATION (SINGLE TENANT, SINGLE PARCEL)	WALL	ONE PER BUILDING FACE NOT TO EXCEED TWO PER BUSINESS.	10% OF BUILDING FACE NOT TO EXCEED 100 SF.	SHALL NOT EXCEED ANY OF THE FOLLOWING: A. THE EAVE OF THE ROOF. B. THE PARAPET OF THE ROOF. C. 20 FEET.	INDIVIDUAL CHANNEL LETTERS.	METAL, ALUMINUM, FIBERGLASS, BRASS, ACRYLIC SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, AND STREET ADDRESS ONLY.	STYLE MAY VARY. MAY BE CONSISTENT WITH OTHER ON-SITE SIGNAGE.	INTERIOR LIT, INDIVIDUAL ILLUMINATED CHANNEL LETTERS.		WALL SIGNS SHALL BE LIMITED TO IDENTIFICATION OF BUSINESS AND TO BE PLACED ON BUILDINGS ONLY. NO WALL SIGN MAY BE PLACED ON OTHER STRUCTURES SUCH AS SILOS OR TOWERS. NO EXPOSED WIRING, RACERWARP, BALLASTS, CONDUIT OR TRANSFORMERS.	
	MONUMENT	ONE PER BUSINESS ENTRY, NOT TO EXCEED TWO PER BUSINESS	50 SF EACH SIDE	4 FEET.	PERPENDICULAR TO THE STREET DRIVEWAY. MIN. 20' FROM DRIVEWAY. MIN. 5' FROM ROW ADJACENT TO ENTRY.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY. SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, AND STREET ADDRESS ONLY (MAY BE 2-SIDED).	STYLE MAY VARY.	GROUND LIT OR INTERIOR LIT, WITH INDIVIDUAL CHANNEL LETTERS.	INTEGRATED WITH BOUNDING AND PLANT MATERIALS.	A COMBINATION OF WALL AND MONUMENT SIGNS MAY BE USED, HOWEVER, OTHER ON-SITE MAY NOT EXCEED 3 PER BUSINESS. NO EXPOSED WIRING, RACERWARP, BALLASTS, CONDUIT OR TRANSFORMERS.
BUSINESS IDENTIFICATION (MULTI-TENANT SITES)	WALL	ONE PER BUILDING FACE NOT TO EXCEED TWO PER BUSINESS	10% OF CUMULATIVE BUILDING FACE NOT TO EXCEED 100 SF.	SHALL NOT EXCEED ANY OF THE FOLLOWING: A. THE EAVE OF THE ROOF. B. THE PARAPET OF THE ROOF. C. 10 FEET.	ADJACENT TO ENTRY.	INDIVIDUAL CHANNEL LETTERS OR WALL MOUNTED PLAQUES.	METAL, ALUMINUM, FIBERGLASS, BRASS, ACRYLIC SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	WALL SIGNS SHALL IDENTIFY THE INDIVIDUAL BUSINESS AND BE LOCATED AT THE MAIN PEDESTRIAN ENTRANCE AND STREET ADDRESS (MAY BE 2-SIDED).	ALL MULTI-TENANT SIGNS SHALL BE CONSISTENT IN TERMS OF BACKGROUND COLOR AND GRAPHIC COLOR.	STYLE MAY VARY.	INTERIOR LIT INDIVIDUAL CHANNEL LETTERS.	A MONUMENT IDENTIFICATION SIGN MAY BE PERMITTED IN LIEU OF A BUSINESS DIRECTORY SIGN.

CLASS	SIGN TYPE	MAXIMUM NUMBER	MAXIMUM SIGN AREA	MAXIMUM HEIGHT	PLACEMENT FORM	MATERIAL	MESSAGE/LAYOUT	TYPE-COLOR	ILLUMINATION	LANDSCAPE	REMARKS		
	MONUMENT	ONE PER STREET FRONTAGE, NOT TO EXCEED TWO PER DEVELOPMENT.	60 SF EACH SIDE.	4 FEET.	PERPENDICULAR TO MIN. 20' DRIVEWAY, MIN. 5' FROM ROW.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY, THAT RELATE TO THE ARCHITECTURAL STYLE OF THE COMPLEX.	IDENTIFICATION OF INDIVIDUAL BUSINESS AND STREET ADDRESS.	ALL MULTITENANT SHALL BE CONSISTENT.	STYLE MAY VARY.	GROUND LIT OR INTERIOR LIT WITH INDIVIDUAL CHANNEL LETTERS.	INTEGRATED WITH SCAFFOLDING AND PLANT MATERIALS.	ONLY ONE MONUMENT SIGN, OR BUSINESS DIRECTORY MONUMENT SIGN SHALL BE ALLOWED PER STREET FRONTAGE.
	BUSINESS DIRECTORY	ONE PER PARKING AREA.	24 SF.	4 FEET.	THE BUSINESS DIRECTORY MONUMENT SIGN SHALL BE LOCATED IN THE PARKING AREA OF THE DEVELOPMENT.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY, WOOD SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE COMPLEX.	IDENTIFICATION OF INDIVIDUAL BUSINESS MEMBER.	ALL MULTITENANT SIGNS BE CONSISTENT	STYLE SHALL BE CONSISTENT IN TERMS OF TYPE FACE BACKGROUND.	GROUND LIT OR INTERIOR ILLUMINATED.	INTEGRATED WITH SCAFFOLDING AND PLANT MATERIALS.	THE BUSINESS DIRECTORY MONUMENT SIGN SHALL LIST ONLY THE ADDRESS, NAME OF THE ON-SITE ACTIVITIES AND DIRECTIONAL MAP OR DIAGRAM.
SUPPORT COMMERCIAL	WALL	ONE PER BUILDING FACE, MAX-MIN OF 3 FOR ANY 1 BUSINESS.	10% OF BUILDING FACE NOT TO EXCEED 60 SF.	SHALL NOT EXCEED ANY OF THE FOLLOWING: A. THE EAVE OF THE ROOF. B. THE PARAPET OF THE ROOF C. 30 FEET.	INDIVIDUAL CHANNEL LETTERS.	METAL, ALUMINUM, FIBERGLASS, ACRYLIC SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, ONLY.	SHALL BE CONSISTENT WITH THE ENTIRE COMPLEX.	SHALL BE CONSISTENT WITH THE ENTIRE COMPLEX.	INTERIOR LIT INDIVIDUAL CHANNEL LETTERS.		A COMBINATION OF MONUMENT AND WALL SIGNS MAY BE USED, HOWEVER, ONLY A MAXIMUM OF THREE SIGNS MAY BE USED TO IDENTIFY ANY ONE BUSINESS. A UNIFORM APPROVAL MUST BE APPROVED.	
	MONUMENT	ONE PER STREET FRONTAGE, NOT TO EXCEED TWO PER DEVELOPMENT.	60 SF	4 FEET.	PERPENDICULAR TO THE STREET, MIN. 20' FROM DRIVEWAY, MIN. 5' FROM ROW.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY, SHALL RELATE TO THE ARCHITECTURE OF THE COMPLEX.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, ONLY.	SHALL BE CONSISTENT WITH THE WALL SIGNAGE (MAY BE 2-SIDED).	SHALL BE CONSISTENT WITH WALL SIGNAGE.	GROUND LIT OR INDIVIDUAL ILLUMINATED CHANNEL LETTERS.	INTEGRATED WITH SCAFFOLDING AND PLANT MATERIALS.	WALL SIGNS ARE LIMITED TO BUSINESS IDENTIFICATION ONLY.

TABLE 6

1 OF 4

CLASS	SIGN TYPE	MAXIMUM NUMBER	MAXIMUM SIGN AREA	MAXIMUM HEIGHT	PLACEMENT FORM	MATERIAL	MESSAGE/LAYOUT	TYPE-COLOR	ILLUMINATION	LANDSCAPE	REMARKS	
											OGRAPHY	
BUSINESS IDENTIFICATION (SINGLE TENANT, SINGLE PARCEL)	WALL	ONE PER BUILDING FACE NOT TO EXCEED TWO PER BUSINESS.	10% OF BUILDING FACE NOT TO EXCEED 100 SF.	SHALL NOT EXCEED ANY OF THE FOLLOWING: A. THE EAVE OF THE ROOF. B. THE PARAPET OF THE ROOF. C. 20 FEET.	INDIVIDUAL CHANNEL LETTERS.	METAL, ALUMINUM, FIBERGLASS, BRASS, ACRYLIC SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, AND STREET ADDRESS ONLY.	STYLE MAY VARY. MAY BE CONSISTENT WITH OTHER ON-SITE SIGNAGE.	INTERIOR LIT, INDIVIDUAL ILLUMINATED CHANNEL LETTERS.		WALL SIGNS SHALL BE LIMITED TO IDENTIFICATION OF BUSINESS AND TO BE PLACED ON BUILDINGS ONLY. NO WALL SIGN MAY BE PLACED ON OTHER STRUCTURES SUCH AS SILOS OR TOWERS. NO EXPOSED WIRING, RACERWARP, BALLASTS, CONDUIT OR TRANSFORMERS.	
	MONUMENT	ONE PER BUSINESS ENTRY, NOT TO EXCEED TWO PER BUSINESS	50 SF EACH SIDE	4 FEET.	PERPENDICULAR TO THE STREET DRIVEWAY. MIN. 20' FROM DRIVEWAY. MIN. 5' FROM ROW ADJACENT TO ENTRY.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY. SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, AND STREET ADDRESS ONLY (MAY BE 2-SIDED).	STYLE MAY VARY.	GROUND LIT OR INTERIOR LIT, WITH INDIVIDUAL CHANNEL LETTERS.	INTEGRATED WITH BOUNDING AND PLANT MATERIALS.	A COMBINATION OF WALL AND MONUMENT SIGNS MAY BE USED, HOWEVER, OTHER ON-SITE MAY NOT EXCEED 3 PER BUSINESS. NO EXPOSED WIRING, RACERWARP, BALLASTS, CONDUIT OR TRANSFORMERS.
BUSINESS IDENTIFICATION (MULTI-TENANT SITES)	WALL	ONE PER BUILDING FACE NOT TO EXCEED TWO PER BUSINESS	10% OF CUMULATIVE BUILDING FACE NOT TO EXCEED 100 SF.	SHALL NOT EXCEED ANY OF THE FOLLOWING: A. THE EAVE OF THE ROOF. B. THE PARAPET OF THE ROOF. C. 10 FEET.	ADJACENT TO ENTRY.	INDIVIDUAL CHANNEL LETTERS OR WALL MOUNTED PLAQUES.	METAL, ALUMINUM, FIBERGLASS, BRASS, ACRYLIC SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	WALL SIGNS SHALL IDENTIFY THE INDIVIDUAL BUSINESS AND BE LOCATED AT THE MAIN PEDESTRIAN ENTRANCE AND STREET ADDRESS (MAY BE 2-SIDED).	ALL MULTI-TENANT SIGNS SHALL BE CONSISTENT IN TERMS OF BACKGROUND COLOR AND GRAPHIC COLOR.	STYLE MAY VARY.	INTERIOR LIT INDIVIDUAL CHANNEL LETTERS.	A MONUMENT IDENTIFICATION SIGN MAY BE PERMITTED IN LIEU OF A BUSINESS DIRECTORY SIGN.

CLASS	SIGN TYPE	MAXIMUM NUMBER	MAXIMUM SIGN AREA	MAXIMUM HEIGHT	PLACEMENT FORM	MATERIAL	MESSAGE/LAYOUT	TYPE-COLOR	ILLUMINATION	LANDSCAPE	REMARKS		
	MONUMENT	ONE PER STREET FRONTAGE, NOT TO EXCEED TWO PER DEVELOPMENT.	60 SF EACH SIDE.	4 FEET.	PERPENDICULAR TO MIN. 20' DRIVEWAY, MIN. 5' FROM ROW.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY, THAT RELATE TO THE ARCHITECTURAL STYLE OF THE COMPLEX.	IDENTIFICATION OF INDIVIDUAL BUSINESS AND STREET ADDRESS.	ALL MULTITENANT SHALL BE CONSISTENT.	STYLE MAY VARY.	GROUND LIT OR INTERIOR LIT WITH INDIVIDUAL CHANNEL LETTERS.	INTEGRATED WITH SCAFFOLDING AND PLANT MATERIALS.	ONLY ONE MONUMENT SIGN, OR BUSINESS DIRECTORY MONUMENT SIGN SHALL BE ALLOWED PER STREET FRONTAGE.
	BUSINESS DIRECTORY	ONE PER PARKING AREA.	24 SF.	4 FEET.	THE BUSINESS DIRECTORY MONUMENT SIGN SHALL BE LOCATED IN THE PARKING AREA OF THE DEVELOPMENT.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY, WOOD SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE COMPLEX.	IDENTIFICATION OF INDIVIDUAL BUSINESS MEMBER.	ALL MULTITENANT SIGNS BE CONSISTENT	STYLE SHALL BE CONSISTENT IN TERMS OF TYPE FACE BACKGROUND.	GROUND LIT OR INTERIOR ILLUMINATED.	INTEGRATED WITH SCAFFOLDING AND PLANT MATERIALS.	THE BUSINESS DIRECTORY MONUMENT SIGN SHALL LIST ONLY THE ADDRESS, NAME OF THE ON-SITE ACTIVITIES AND DIRECTIONAL MAP OR DIAGRAM.
SUPPORT COMMERCIAL	WALL	ONE PER BUILDING FACE, MAX-MIN OF 3 FOR ANY 1 BUSINESS.	10% OF BUILDING FACE NOT TO EXCEED 60 SF.	SHALL NOT EXCEED ANY OF THE FOLLOWING: A. THE EAVE OF THE ROOF. B. THE PARAPET OF THE ROOF C. 30 FEET.	INDIVIDUAL CHANNEL LETTERS.	METAL, ALUMINUM, FIBERGLASS, ACRYLIC SHALL RELATE TO THE ARCHITECTURAL STYLE OF THE STRUCTURE.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, ONLY.	SHALL BE CONSISTENT WITH THE ENTIRE COMPLEX.	SHALL BE CONSISTENT WITH THE ENTIRE COMPLEX.	INTERIOR LIT INDIVIDUAL CHANNEL LETTERS.		A COMBINATION OF MONUMENT AND WALL SIGNS MAY BE USED, HOWEVER, ONLY A MAXIMUM OF THREE SIGNS MAY BE USED TO IDENTIFY ANY ONE BUSINESS. A UNIFORM DESIGN MUST BE APPROVED.	
	MONUMENT	ONE PER STREET FRONTAGE, NOT TO EXCEED TWO PER DEVELOPMENT.	60 SF	4 FEET.	PERPENDICULAR TO THE STREET, MIN. 20' FROM DRIVEWAY, MIN. 5' FROM ROW.	FREE-STANDING MONOLITH	POURED-IN-PLACE CONCRETE, TILE, STONE, MASONRY, SHALL RELATE TO THE ARCHITECTURE OF THE COMPLEX.	IDENTIFICATION OF BUSINESS AND/OR BUSINESS LOGO, ONLY.	SHALL BE CONSISTENT WITH THE WALL SIGNAGE (MAY BE 2-SIDED).	SHALL BE CONSISTENT WITH WALL SIGNAGE.	GROUND LIT OR INDIVIDUAL ILLUMINATED CHANNEL LETTERS.	INTEGRATED WITH SCAFFOLDING AND PLANT MATERIALS.	WALL SIGNS ARE LIMITED TO BUSINESS IDENTIFICATION ONLY.

CLASS	SIGN TYPE	MAXIMUM NUMBER	MAXIMUM SIGN AREA	MAXIMUM HEIGHT	PLACEMENT FORM	MATERIAL	MESSAGE/LAYOUT	COLOR	TYPE-OGRAPHY	ILLUMINATION	LANDSCAPE	REMARKS
SERVICE STATION	WALL	ONE PER STREET FRONTAGE MAXIMUM OF TWO.	10% OF BUILDING FACE NOT TO EXCEED 50 SF.	SHALL NOT EXCEED ANY OF THE FOLLOWING:  A. THE SLOPE OF THE ROOF.  B. THE PARAPET OF THE ROOF.  C. 20 FEET.		TRANSLUCENT FACE, VACUUM FORM PLASTIC, ACRYLIC, FIBERGLASS.	IDENTIFICATION OF COMPANY AND/OR LOGO, ONLY.	COMPANY LOGO.	COMPANY LOGO.	INTERIOR LIT.		A CENTER IS ONE IN WHICH BUSINESSES AND STRUCTURES ARE DESIGNED IN AN INTEGRATED AND INTERRELATED DEVELOPMENT. SUCH DESIGN IS INDEPENDENT OF THE NUMBER OF STRUCTURES LOCATED ON PARCELS MAKING UP THE CENTER.
	MONUMENT	ONE PER STREET FRONTAGE, NOT TO EXCEED TWO PER STATION.	24 SF.	4 FEET.	PERPENDICULAR TO THE STREET, MIN. 20' FROM DRIVEWAY, MIN. 6' FROM ROW.	FREE-STANDING MONOLITH	TRANSLUCENT FACE, VACUUM FORM PLASTIC, ACRYLIC, FIBERGLASS.	IDENTIFICATION OF COMPANY AND/OR LOGO, ONLY.	COMPANY LOGO.	COMPANY LOGO.	INTERIOR LIT.	INTEGRATED WITH SOUNDING AND PLANT MATERIALS.
SPECIAL SERVICE SIGNS	WALL/POLE	ONE PER EACH PUMP ISLAND, NOT TO EXCEED 4 PER STATION.	2 SF.	WALL/POLE OF THE CANOPY: 8 FEET.	MOUNTED ON WALL OR POLE CANOPY SUPPORT	METAL OR ALUMINUM.	SIGNS SHALL BE LIMITED TO SUCH ITEMS AS SELF-SERVE, FULL SERVE, AIR, WATER, CASH.	CONSISTENT WITH COMPANY LOGO COLORS.	CONSISTENT WITH COMPANY LOGO TYPEFACE.	SMALL IN NON-ILLUMINATED.		LIMITED VARIATION IS PERMITTED, SUCH AS INCORPORATING INDIVIDUAL LOGOS, AS LONG AS THE REMAINING COMPONENTS OF THE PROGRAM REMAIN THE SAME.

CLASS	SIGN TYPE	MAXIMUM NUMBER	MAXIMUM SIGN AREA	MAXIMUM HEIGHT	PLACEMENT FORM	MATERIAL	MESSAGE/LAYOUT	COLOR	TYPE-OGRAPHY	ILLUMINATION	LANDSCAPE	REMARKS
SPECIAL ADVERTISING	WINDOW	TWO PER STATION	4 SF.	6 FEET.	INTERIOR WINDOW FACE.		SPECIAL ADVERTISING SHALL BE LIMITED TO ADVERTISING SPECIAL SALES OR SERVICE. ONLY.					
TEMPORARY SIGNS												
*SALE/LEASE			32 SF. (EXCL. BASE)	8 FEET. (10 FT. HORIZ.)								
*OWNER/DEVELOPER/ETC.			32 SF. (EXCL. BASE)	8 FEET. (10 FT. HORIZ.)								
*FUTURE CONSTRUCTION			22 SF. (EXCL. BASE)	8 FEET. (10 FT. HORIZ.)								
PUBLIC USE FACILITY			100 SF.	8 FEET.								
REAL ESTATE SIGNS	WALL OR POLE	4 PER SITE	60 SF.	10 FEET.	PARALLEL TO STREET MINIMUM 30 FEET.	MONOLITH	WOOD.	FOR SALE OR LEASE INFORMATION.	CONSISTENT WITH SUB-BOUNDING IMPROVEMENTS	SHALL NOT BE ILLUMINATED.		

5.0  
**INFRASTRUCTURE  
MASTER PLAN**

CITY OF FONTANA  
**FONTANA GATEWAY SPECIFIC PLAN**

---

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## 5.0 INFRASTRUCTURE MASTER PLAN

### 5.1 Sewer

#### 5.1.1 Existing Sewer System

Chino Basin Municipal Water District provides domestic sewer treatment for the Specific Plan area. The only existing sewer main within the Specific Plan area is in Jurupa Avenue. This 8 inch main drains to a north/south 8 inch main in Pacific Avenue to the 33 inch trunk line located in Marlay Avenue. This 33 inch trunk line is the main interceptor for the Specific Plan area. The existing sewer system is shown in Figure 26.

#### 5.1.2 Proposed "Backbone" Sewer System

At the present time, the City is designating a 36 inch sewer main for Etiwanda Avenue from Marlay Avenue to Slover Avenue then northerly north of Interstate 10. Also, the City of Fontana has designated Mulberry Avenue a main carrier for sewage and will require a 21 inch sewer main in Mulberry Avenue between Marlay Avenue and Slover Avenue due to development to the north.

#### 5.1.3 Projected Sewage Generation

It is estimated that the Specific Plan area will generate 377,500 gallons of sewage per day or 1,405 cubic feet per second. Figure 27 provides a summary of the subarea generation calculations for the above mentioned total. The average flow generation factor used for those calculations was 0.0022 cfs/acre.

#### 5.1.4 Proposed Sewer System

Utilizing the quantities obtained through above mentioned calculations and a review of the topography of the Specific Plan area the overall sewer system as shown on Figure 26 has been developed. This figure shows main locations and their relative sizes for the ultimate build-out of the Specific Plan area.

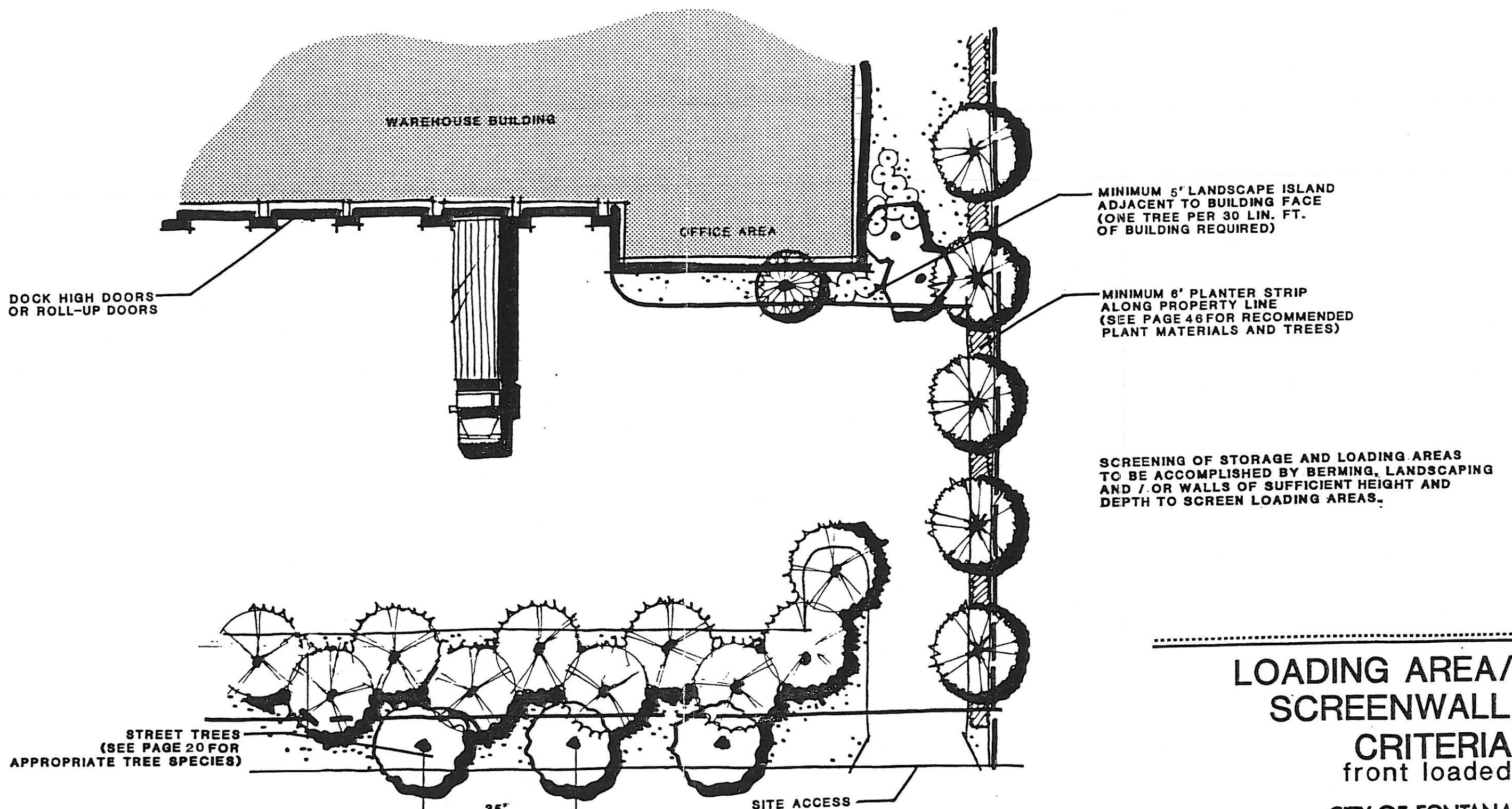
### 5.2 Water

#### 5.2.1 Existing Water System

Water is provided to the Specific Plan area by Fontana Water Company, a division of San Gabriel Water Company. Locations of existing mains and their respective sizes are shown on Figure 28. A series of fire flow tests have been performed by Fontana Water Company with results and locations of the tests also shown on Figure 29.

#### 5.2.2 Projected Water Demand

The projected water demand for the Specific Plan area was calculated by subarea as shown in Figure 29. The daily water demand for the Specific Plan area was calculated to be 480,000 gallons per day (797 gpm average flow). However, in terms of sizing mains within the areas, fire flow



## **CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN**

**WILLIAMS • KUEBELBECK & Associates, Inc  
FORMA-PLANNING NETWORK**

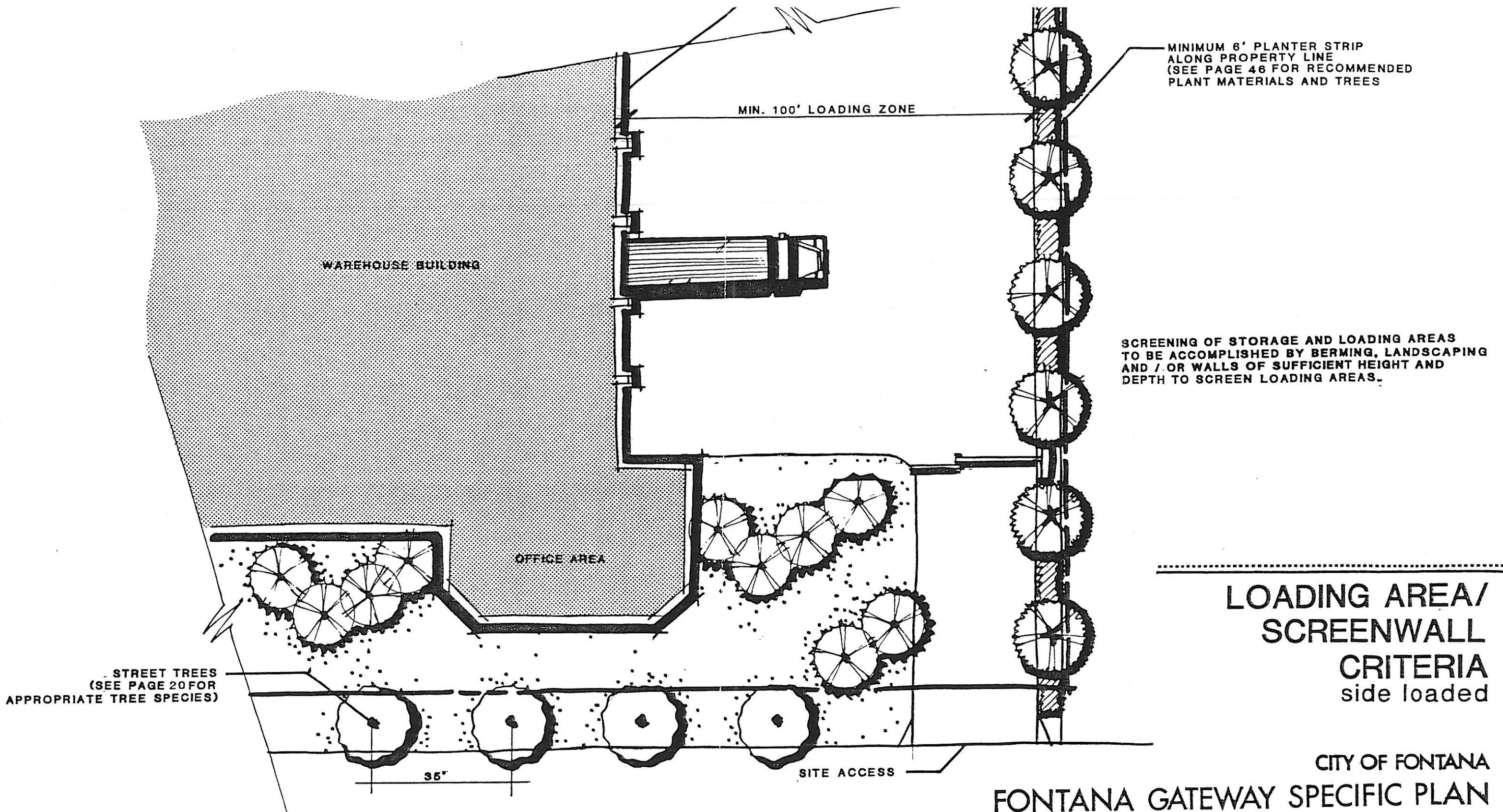
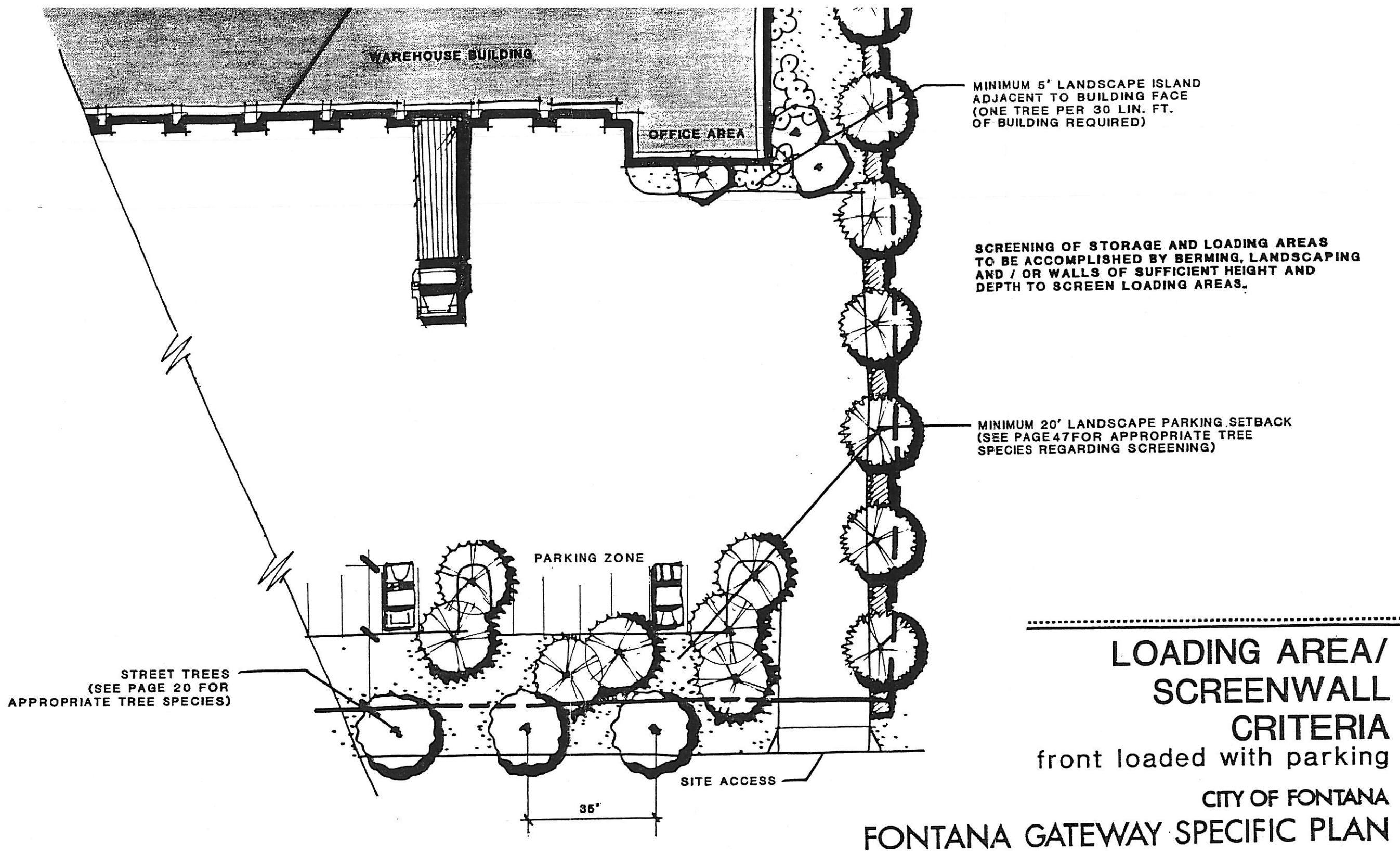
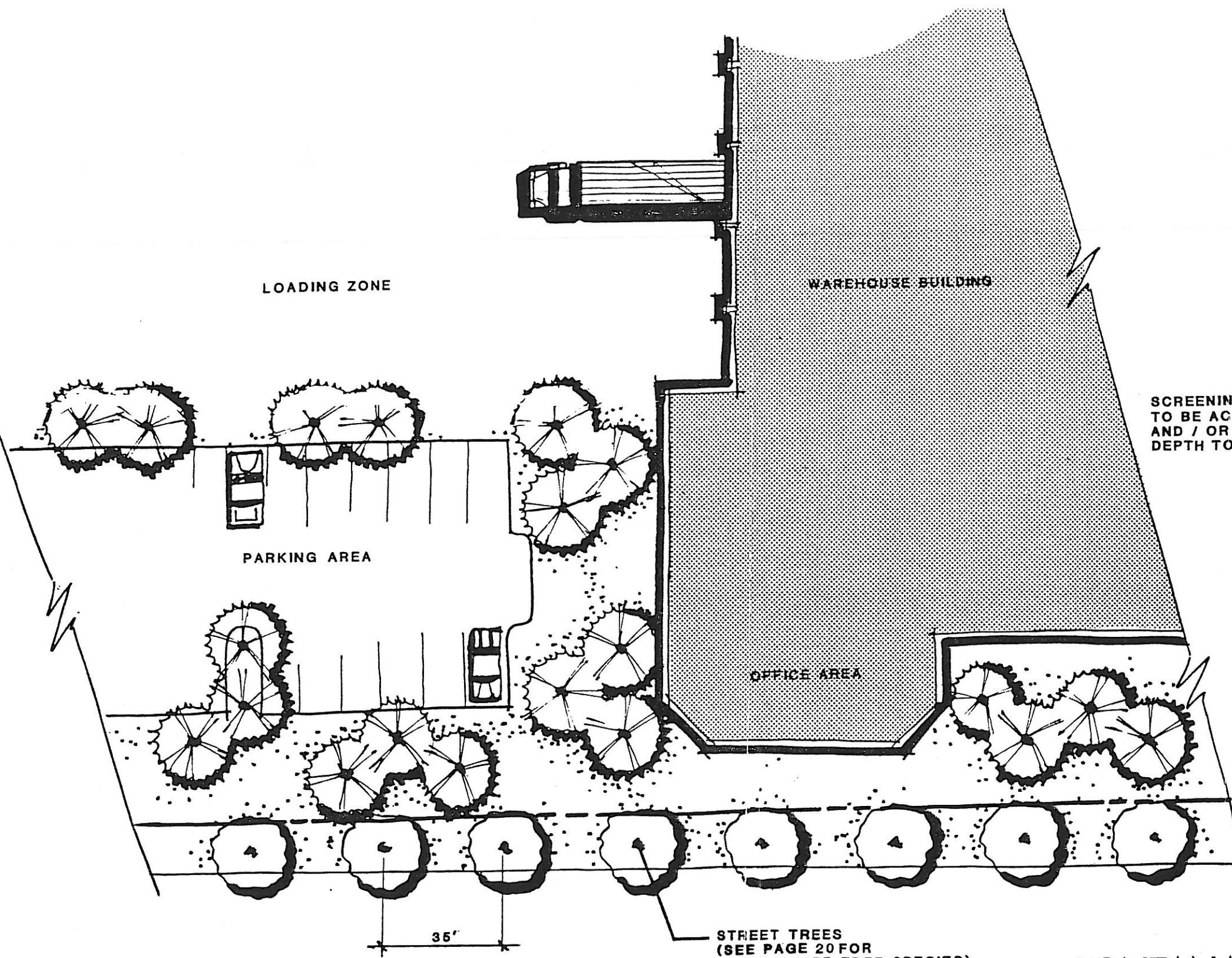


FIGURE 28



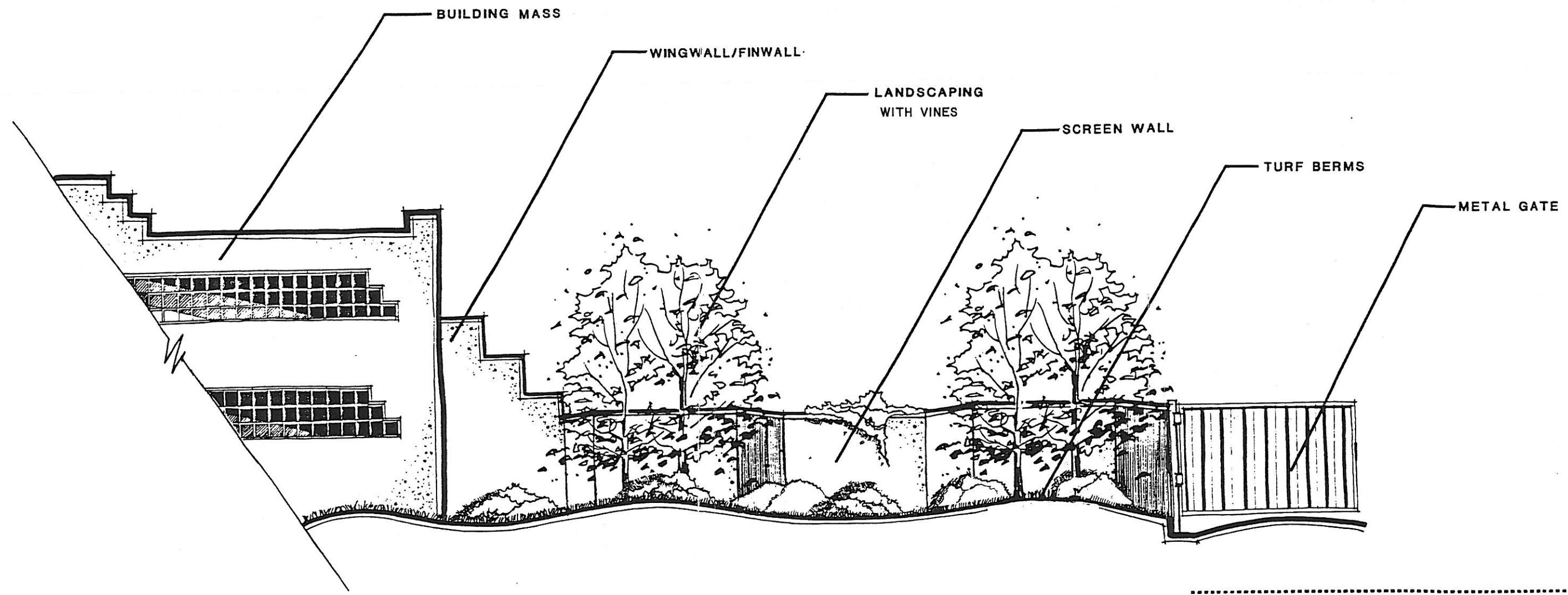
WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA PLANNING NETWORK



**LOADING AREA/  
SCREENWALL  
CRITERIA**  
side loaded with parking

**CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORM-PLANNING NETWORK

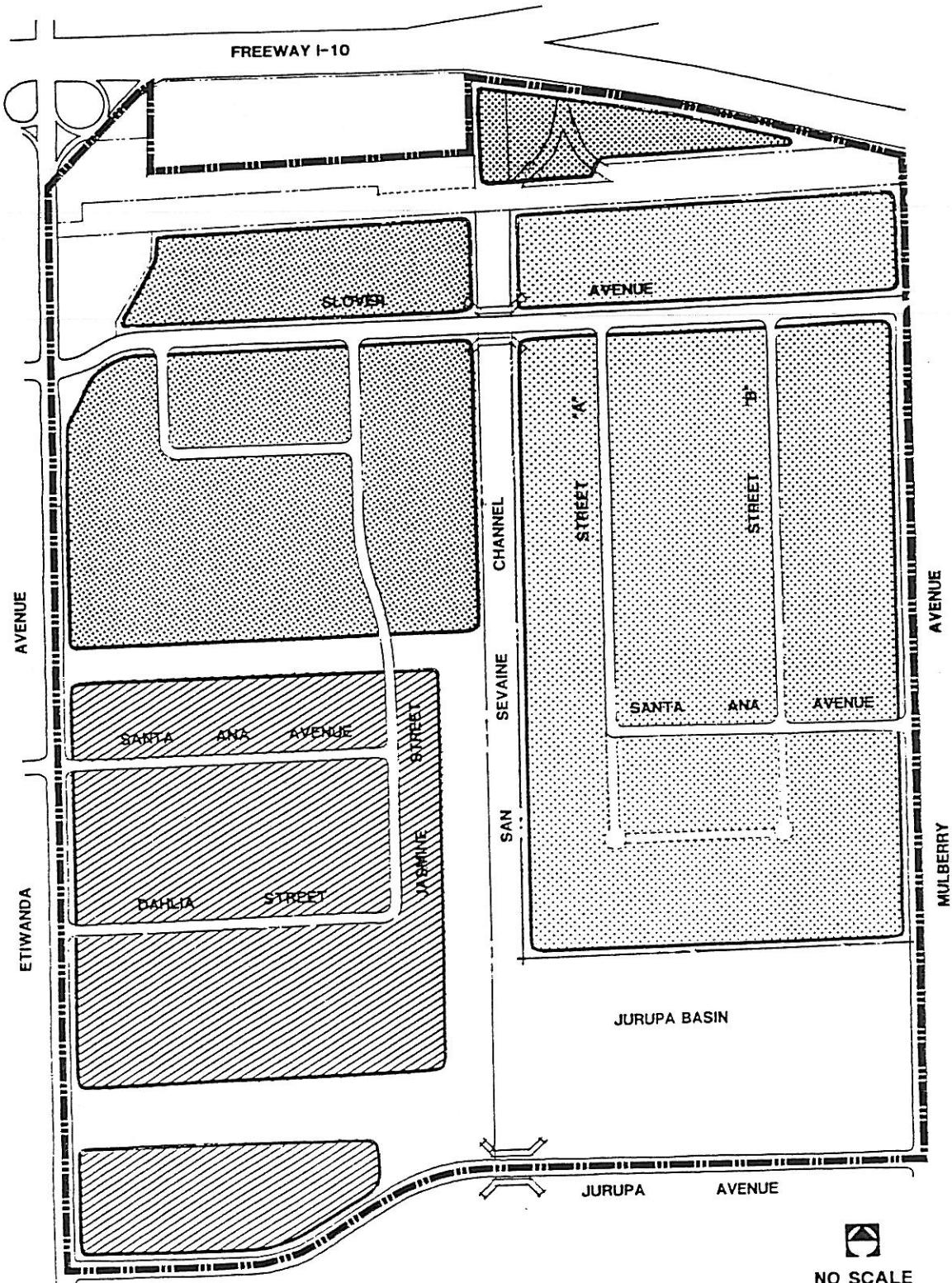


ANY OR ALL OF THE ABOVE SCREENING ELEMENTS MAY BE PERMITTED,  
INCLUDING STRUCTURAL EXTENSIONS OF MAIN BUILDINGS  
WITH SUITABLE ARCHITECTURAL TREATMENTS.

## SCREENING METHODS

### CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



### LEGEND

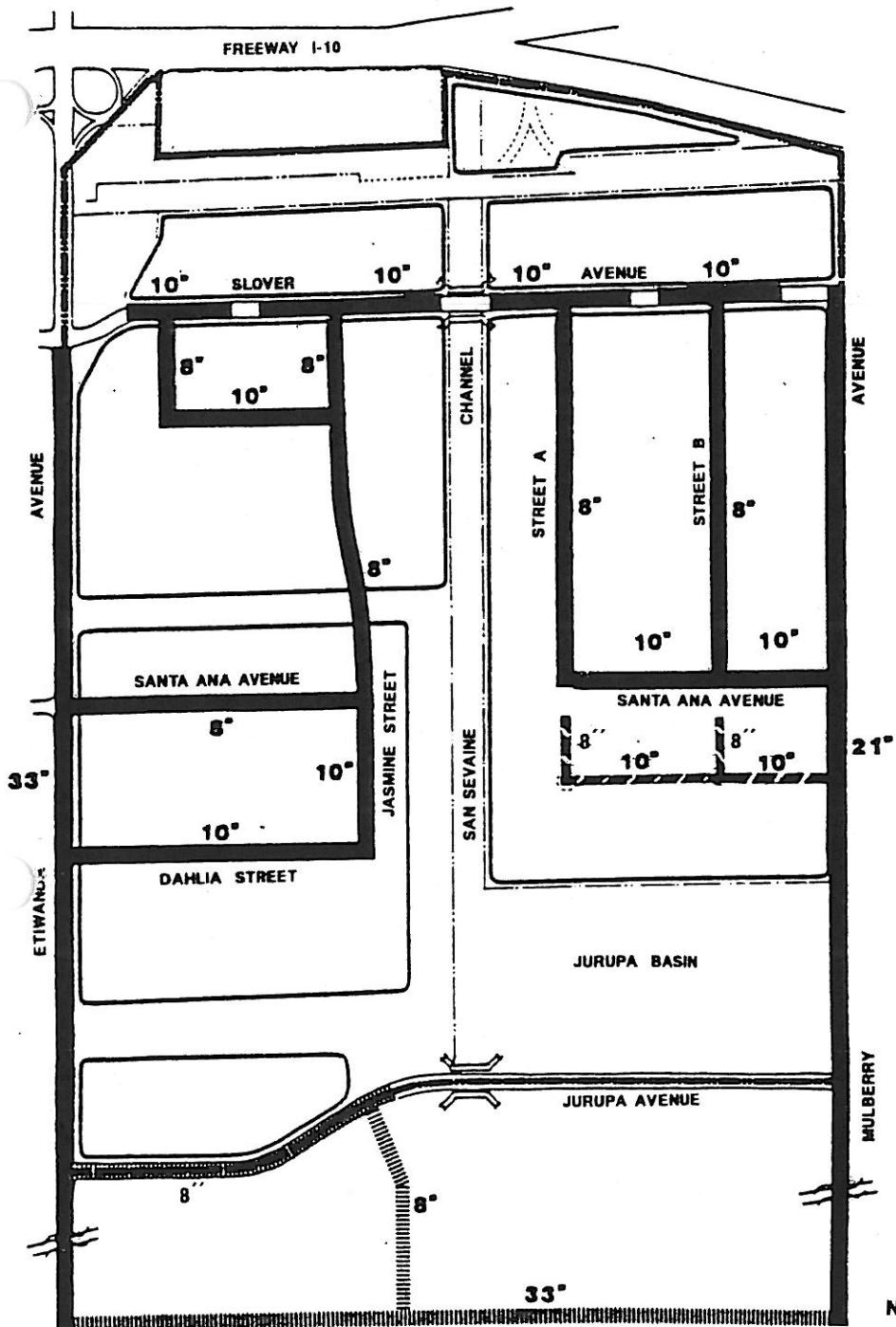
-  Phase I
-  Phase II
-  Phase III

## DEVELOPMENT PHASING PLAN

### CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

**NOTE:**  
THE LOCATION OF THESE STREETS AND  
ALL RELATED PUBLIC UTILITY FACILITIES  
ARE CONCEPTUAL ONLY AND WILL BE  
INSTALLED ONLY IN THE DISCRETION OF  
EACH AFFECTED PROPERTY OWNER.  
NO IMPLICATION OF PUBLIC UTILITY  
EASEMENT OR PUBLIC UTILITY FACILITY  
LOCATION IS TO BE DERIVED FROM THE  
LOCATION OF THESE CONCEPTUAL STREETS.



NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

LEGEND

-  EXISTING
-  PROPOSED
-  OPTIONAL

NO SCALE

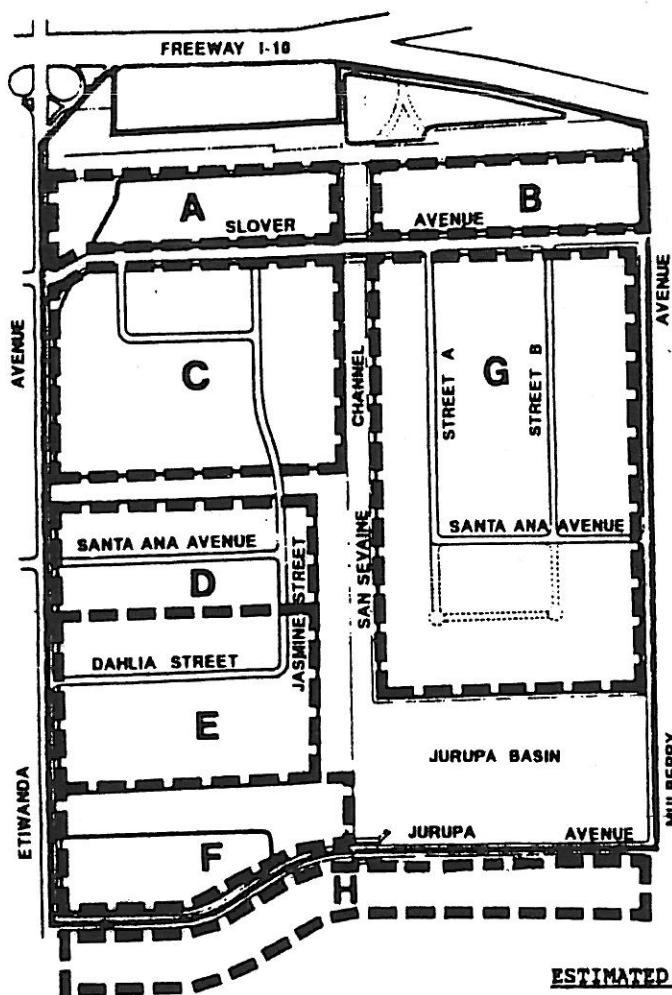
## SEWER SYSTEM

CITY OF FONTANA

# FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## SEWER QUANTITY GENERATION ANALYSIS



NOTE:  
 THE LOCATION OF THESE STREETS AND  
 ALL RELATED PUBLIC UTILITY FACILITIES  
 ARE CONCEPTUAL ONLY AND WILL BE  
 INSTALLED ONLY IN THE DISCRETION OF  
 EACH AFFECTED PROPERTY OWNER.  
 NO IMPLICATION OF PUBLIC UTILITY  
 EASEMENT OR PUBLIC UTILITY FACILITY  
 LOCATION IS TO BE DERIVED FROM THE  
 LOCATION OF THESE CONCEPTUAL STREETS.



NO SCALE

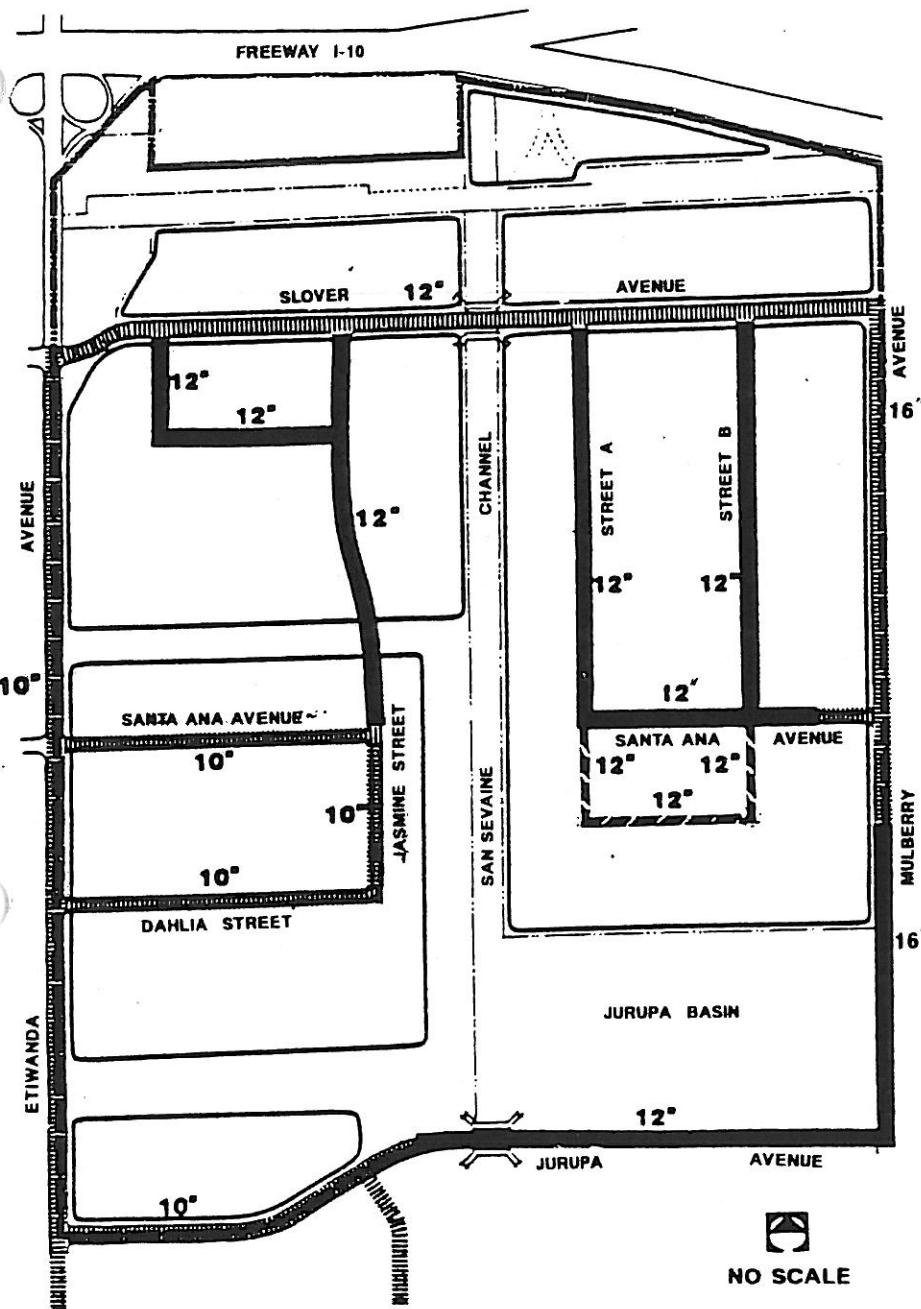
### ESTIMATED WASTEWATER GENERATION

AREA	ACREAGE	CUBIC FEET/SECOND cfs	GALLONS/DAY gpd
A	39	0.086	23,100
B	36	0.079	21,200
C	110	0.242	65,000
D	57	0.125	33,600
E	84	0.185	49,700
F	53	0.117	31,400
G	218	0.479	128,800
H	42	0.092	24,700
639 acres		*1.405 cfs	377,500 gpd

\* Average flow (peak flow equals 2.5 times the average flow)

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

LEGEND

- EXISTING
- PROPOSED
- OPTIONAL

THE WATER MAINS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA

# WATER SYSTEM

## CITY OF FONTANA

## FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

requirements would dictate a four hour flow duration at 4,000 gallons per minute.

#### 5.2.3 Proposed Water System

Figure 28 depicts the location and size of proposed water mains to meet service requirements for the Specific Plan area and fire flows should be met by proposed mains. However, existing tests show that the 10 inch lines in Santa Ana Avenue, Jasmine Street, Dahlia Street, Etiwanda Avenue and Jurupa Avenue in the southwest portion of the Specific Plan area, do not meet the 4,000 gallon per minute requirement and may require upgrading.

### 5.3 Drainage

#### 5.3.1 Existing Drainage System

Located within and adjacent to the Specific Plan area are two major water carrying channels. The San Sevaine Channel bisects the Specific Plan area in a north to south direction and has an ultimate design capacity of 18,850 cfs. The lower Etiwanda Channel is located westerly of Etiwanda Avenue within the City of Ontario and has an ultimate design capacity of 3,500 cfs.

There are two existing minor storm drains located within the Specific Plan area presently being used to drain isolated areas. The existing storm drain systems within the Specific Plan area are shown on Figure 30.

#### 5.3.2 Master Planned Drainage System

The County of San Bernardino required two major drainage studies within the drainage basin that cover the Specific Plan area.

- o Day, Etiwanda and San Sevaine Creek System Drainage Plan, Bill Mann & Associates, March 1983.
- o San Bernardino County Comprehensive Storm Drain Plan, Project No. 2, Moffatt and Nichol, March 1969.

These studies outline the improvements to the major drainage courses, San Sevaine Channel and Lower Etiwanda Creek, as well as the proposed backbone storm drain system. These improvements shall not be required as part of this Specific Plan.

Of major importance is the storm drain link between Etiwanda Avenue and the lower Etiwanda Channel required to aid in draining Etiwanda Avenue.

#### 5.3.3 Projected Drainage Flows

It is estimated that the Specific Plan area has a contributory drainage area of 629 acres and would generate at full development, 1,120 cubic feet per second based on a 100 year storm. This would be an increase in drainage flow generated by the Specific Plan area of 306 cubic feet per second above the existing conditions.

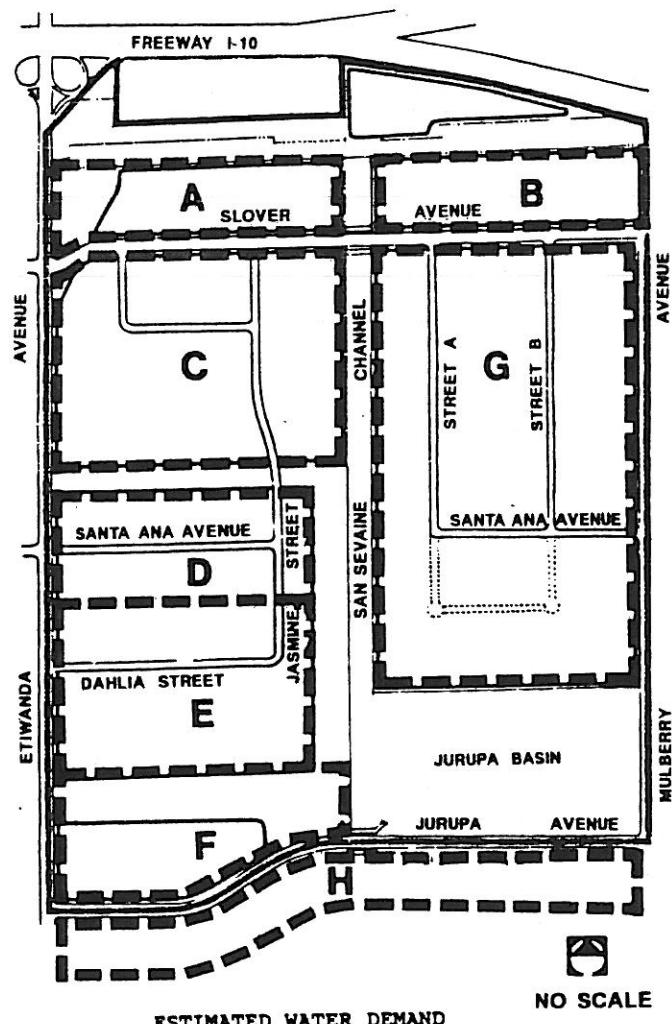
Figure 31 provides a summary of the subarea generation calculations, including a comparison of existing conditions to full development conditions.

#### 5.3.4 Proposed Drainage System

Utilizing the quantities obtained through the drainage subareas calculation and a review of the topography of the Specific Plan area, the overall storm drain system as shown on Figure 30, has been developed. This figure shows the main locations and their relative sizes for the ultimate build-out of the Specific Plan area.

Special attention should be given to the draining of Etiwanda Avenue, since this is contingent on the development of the Master Storm Drain Backbone System to the lower Etiwanda Channel.

## WATER QUANTITY GENERATION ANALYSIS

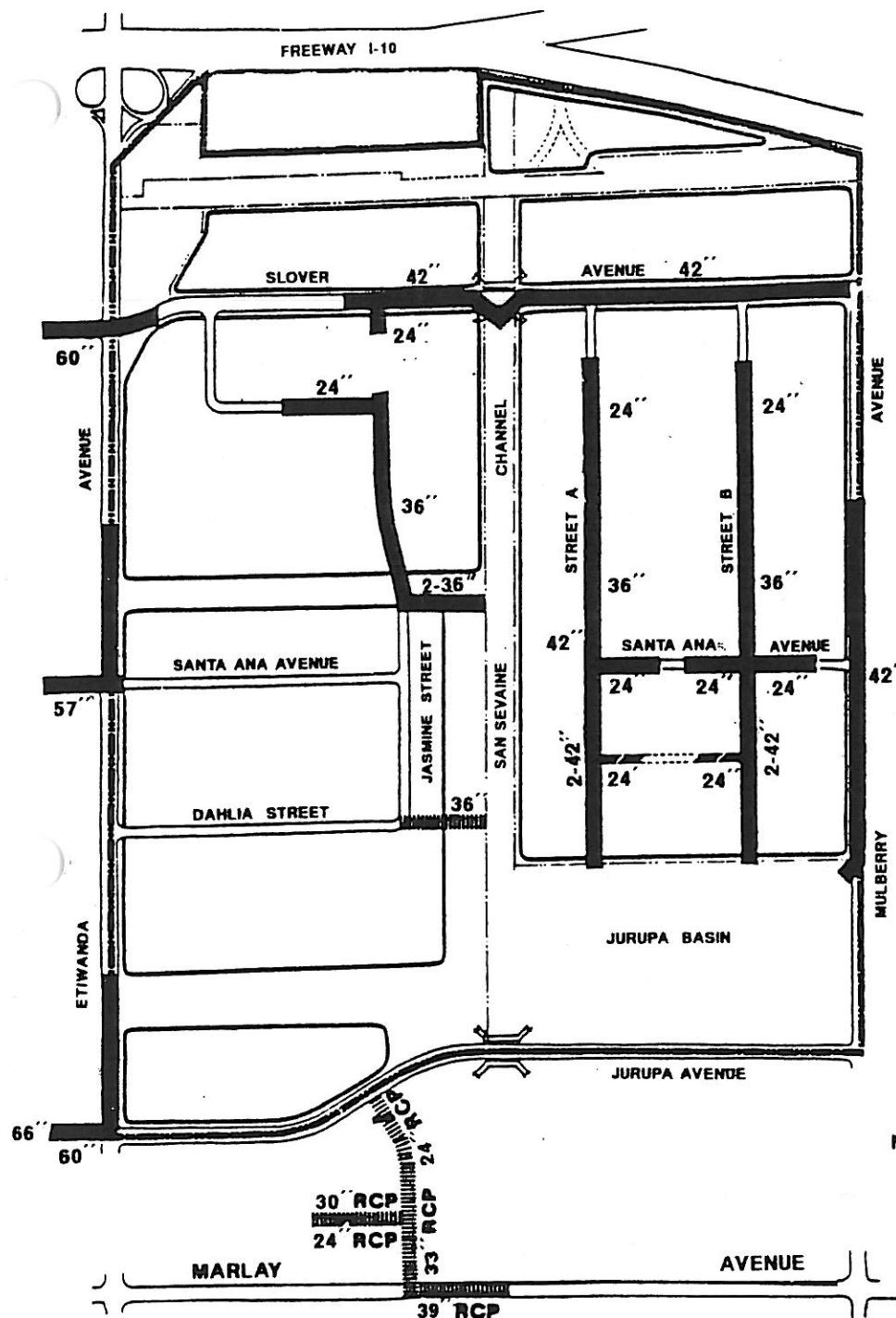


### ESTIMATED WATER DEMAND

<u>AREA</u>	<u>ACREAGE</u>	<u>GALLONS/MIN. (gpm)</u>	<u>GALLONS/DAY (gpd)</u>	<u>NOTE:</u>
A	39	49	29,250	THE LOCATION OF THESE STREETS AND
B	36	45	27,000	ALL RELATED PUBLIC UTILITY FACILITIES
C	110	137	82,500	ARE CONCEPTUAL ONLY AND WILL BE
D	57	71	42,750	INSTALLED ONLY IN THE DISCRETION OF
E	84	105	63,000	EACH AFFECTED PROPERTY OWNER.
F	53	66	39,750	NO IMPLICATION OF PUBLIC UTILITY
G	218	272	163,500	EASEMENT OR PUBLIC UTILITY FACILITY
H	42	52	31,500	LOCATION IS TO BE DERIVED FROM THE
<b>TOTAL</b>	<b>639 acres</b>	<b>797 gpm</b>	<b>479,250 gpd</b>	<b>LOCATION OF THESE CONCEPTUAL STREETS.</b>

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

LEGEND

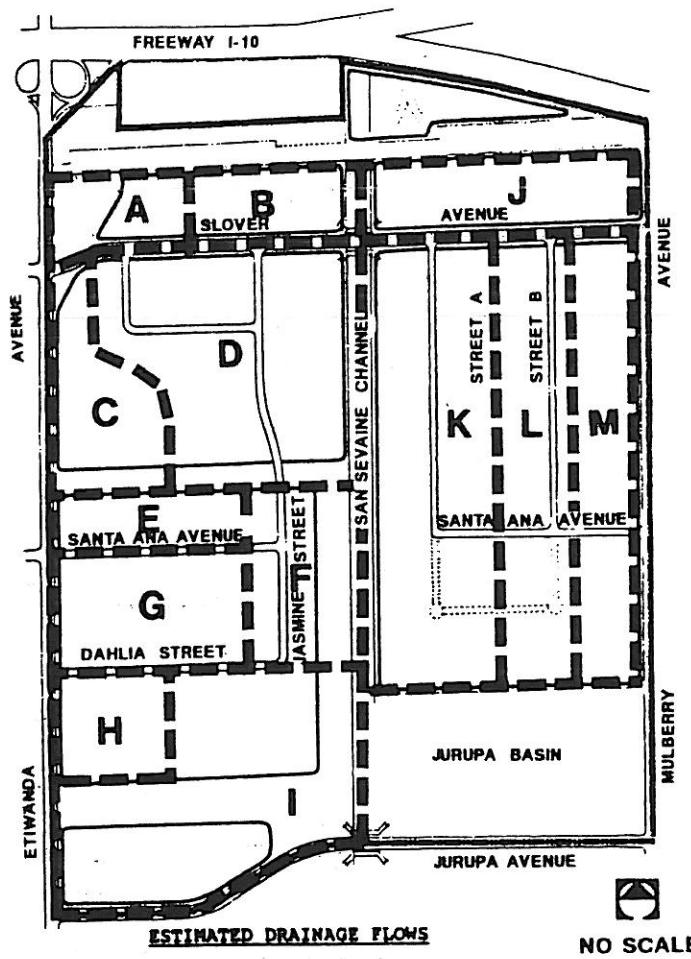
-  EXISTING
-  PROPOSED
-  OPTIONAL

STORM  
DRAIN  
SYSTEM

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## DRAINAGE AREA FLOW COMPARISON



ESTIMATED DRAINAGE FLOWS

NO SCALE

AREA	ACREAGE	ESTIMATED DRAINAGE FLOWS			NOTE:
		EXISTING CONDITIONS $Q_{100}$ (cfs)	FULL DEVELOPMENT $Q_{100}$ (cfs)	INCREASE $Q_{100}$ (cfs)	
A	20	26	36	10	
B	20	26	36	10	
C	40	52	72	20	THE LOCATION OF THESE STREETS AND
D	80	104	144	40	ALL RELATED PUBLIC UTILITY FACILITIES
E	25	33	44	11	ARE CONCEPTUAL ONLY AND WILL BE
F	30	39	53	14	INSTALLED ONLY IN THE DISCRETION OF
G	50	65	88	23	EACH AFFECTED PROPERTY OWNER .
H	30	39	53	14	NO IMPLICATION OF PUBLIC UTILITY
I	80	104	144	40	EASEMENT OR PUBLIC UTILITY FACILITY
J	36	46	64	18	LOCATION IS TO BE DERIVED FROM THE
K	87	112	154	42	LOCATION OF THESE CONCEPTUAL STREETS.
L	87	112	154	42	
M	44	52	78	26	
TOTAL	629 acres	814 cfs	1120 cfs	306 cfs	

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

6.0  
**DEVELOPMENT  
PHASING**

CITY OF FONTANA  
**FONTANA GATEWAY SPECIFIC PLAN**

---

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## **6.0 DEVELOPMENT PHASING**

The Market Analysis Report prepared for the Specific Plan identifies the market conditions and projections which provide the basis for the proposed land use mix and phasing of development for the Fontana Gateway Specific Plan Area.

It is anticipated that new development would occur on the 300 currently vacant acres within the Specific Plan Area, and that approximately 80 of the 253 acres which are currently in existing industrial uses would be redeveloped with more intensive and economic industrial uses later in the development period. Thus, a total of 380 acres are projected for development/redevelopment.

As discussed above, the projected timing of development of the Fontana Gateway Specific Plan Area is based on: (a) the above development/redevelopment projections; (b) a reasonable phasing schedule for the necessary improvements (Figure 36); and (c) fiscal and marketing considerations. Given these facts, a three-phase development program has been proposed for the study area. This phasing program is depicted graphically in Figure 36. Fiscal and marketing considerations dictate that Phase I be comprised of 2 subphases. These subphases reflect a difference in financing mechanisms and are not intended to reflect a material difference in the timing of the development of the relevant infrastructure.

The Fontana Redevelopment Agency (FRA) will be involved in the initiation and facilitation of development within the Fontana Gateway Specific Plan Area. The role of the FRA is expected to be limited principally to assisting in the provision of infrastructure improvements.

### **6.1 INFRASTRUCTURE PHASING**

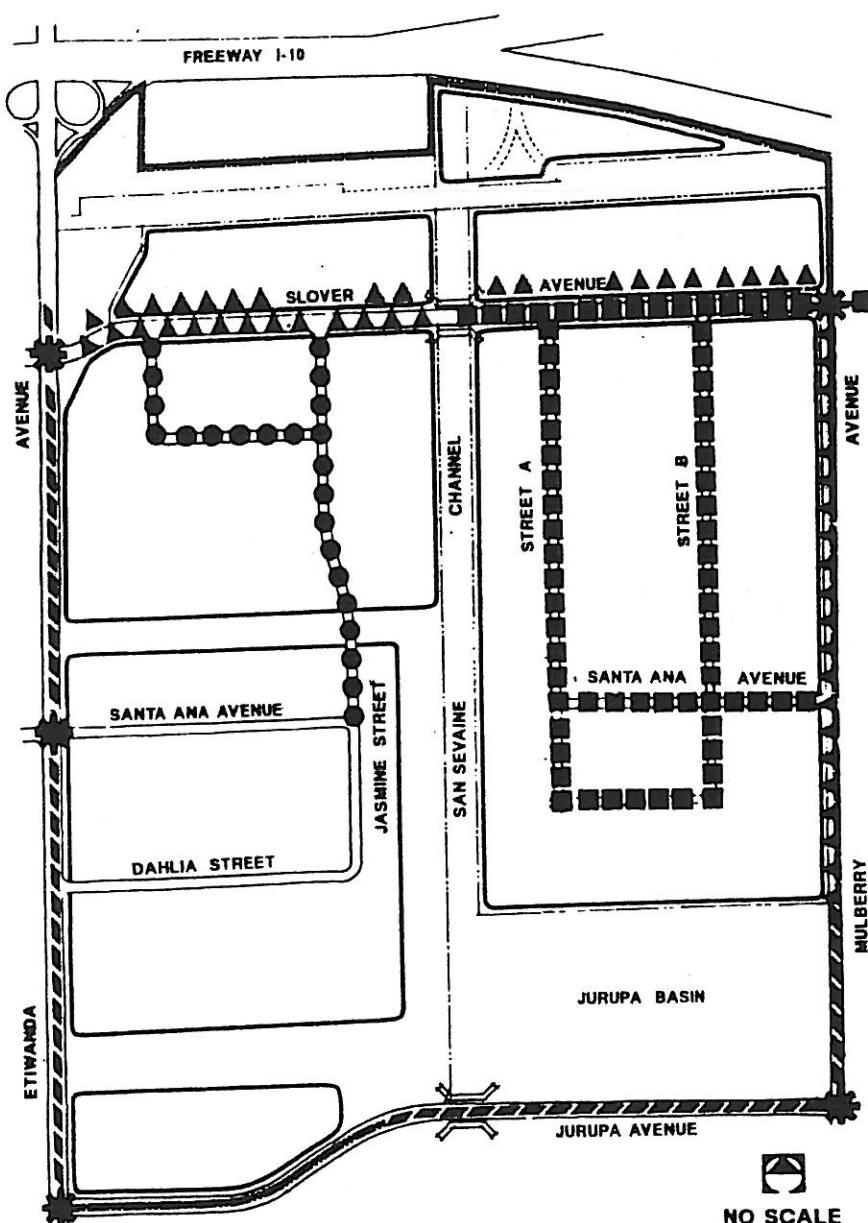
In conjunction with the development phasing of the Fontana Gateway Specific Plan Area, the infrastructure improvements will be phased to coincide with the three phase development program for the Specific Plan area. Figures 32, 33, 34 and 35 illustrate the infrastructure phasing for:

- o Street Improvements,
- o Sewer improvements,
- o Water improvements, and
- o Storm drain improvements.

Necessary infrastructure improvements will be constructed according to a schedule which will facilitate the orderly development of the private improvements.

### **6.2 INFRASTRUCTURE SIZING AND LOCATION**

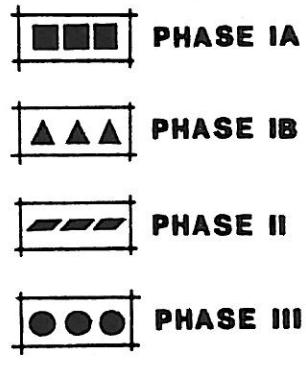
Infrastructure sizes are subject to adjustment upward or downward based upon future engineering recommendations or requirements. Location of sewer lines, water lines and storm drain improvements are conceptually depicted. Exact locations will be determined by final engineering design.



**NOTE:**

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

**LEGEND**

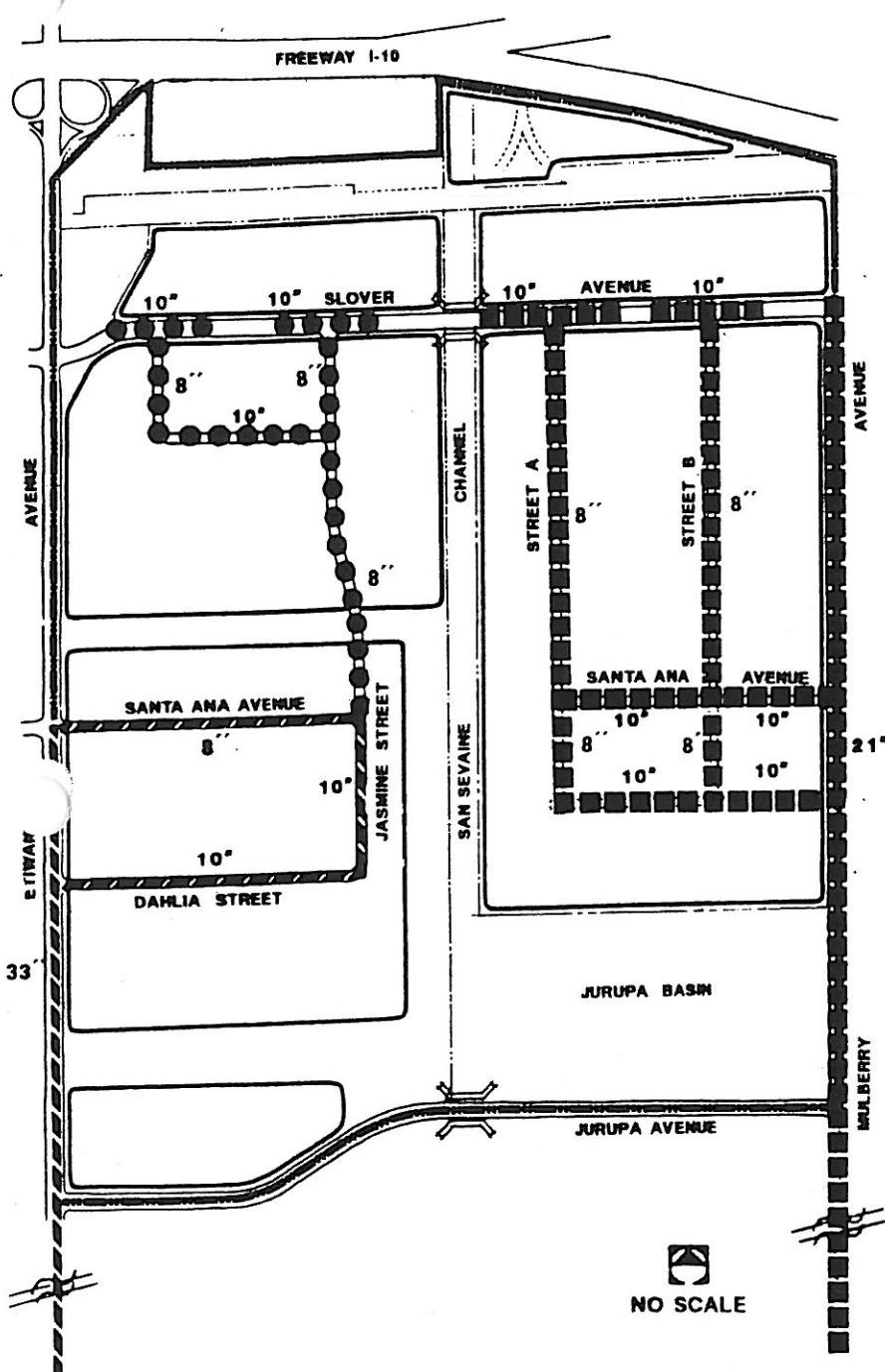


 NEW TRAFFIC SIGNAL LOCATION OR UPGRADE

# STREET IMPROVEMENT PHASING

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

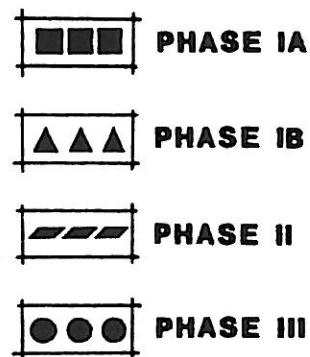
WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

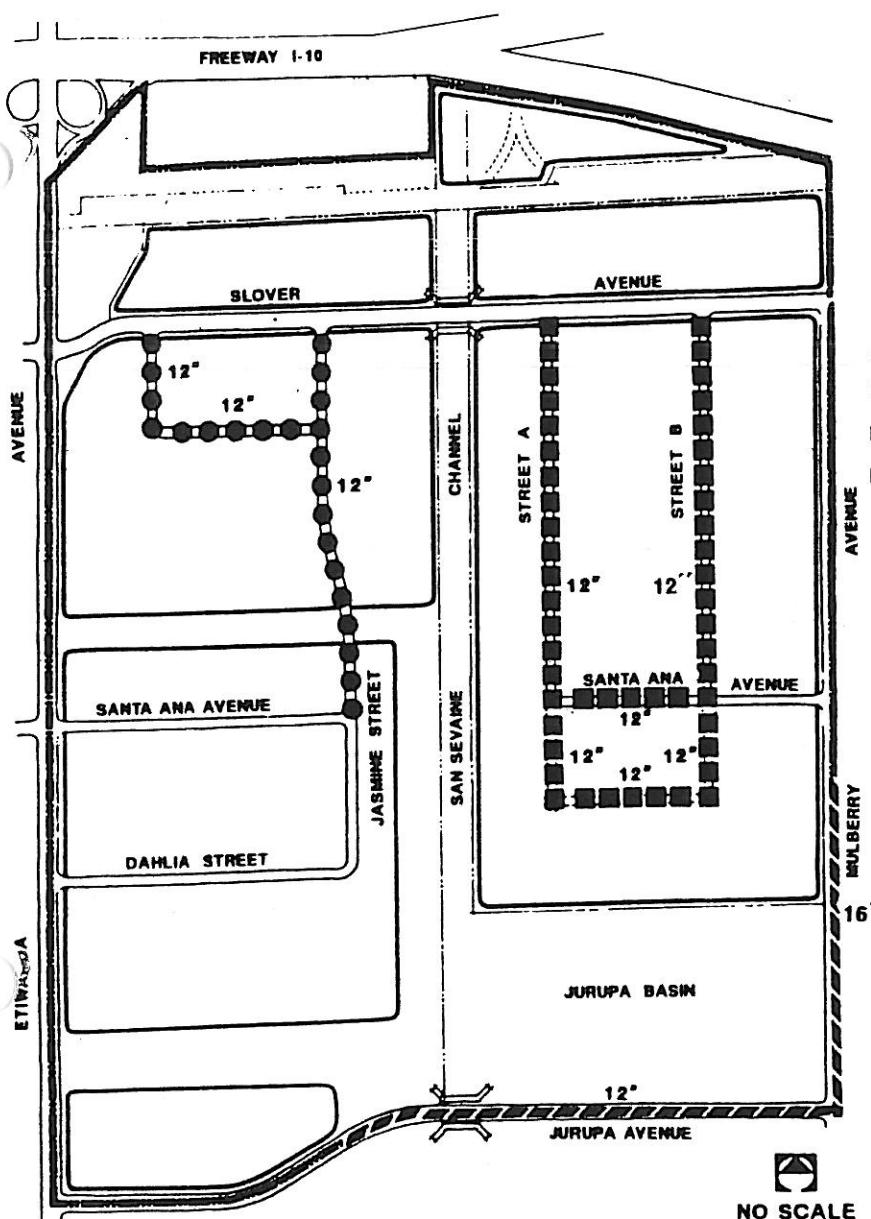
**LEGEND**



THE INTERIOR SANITARY SEWERS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

**SEWER SYSTEM PHASING**  
**CITY OF FONTANA**  
**FONTANA GATEWAY SPECIFIC PLAN**

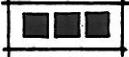
WILLIAMS • KUEBELBECK & Associates, Inc.  
 FORMA-PLANNING NETWORK



NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

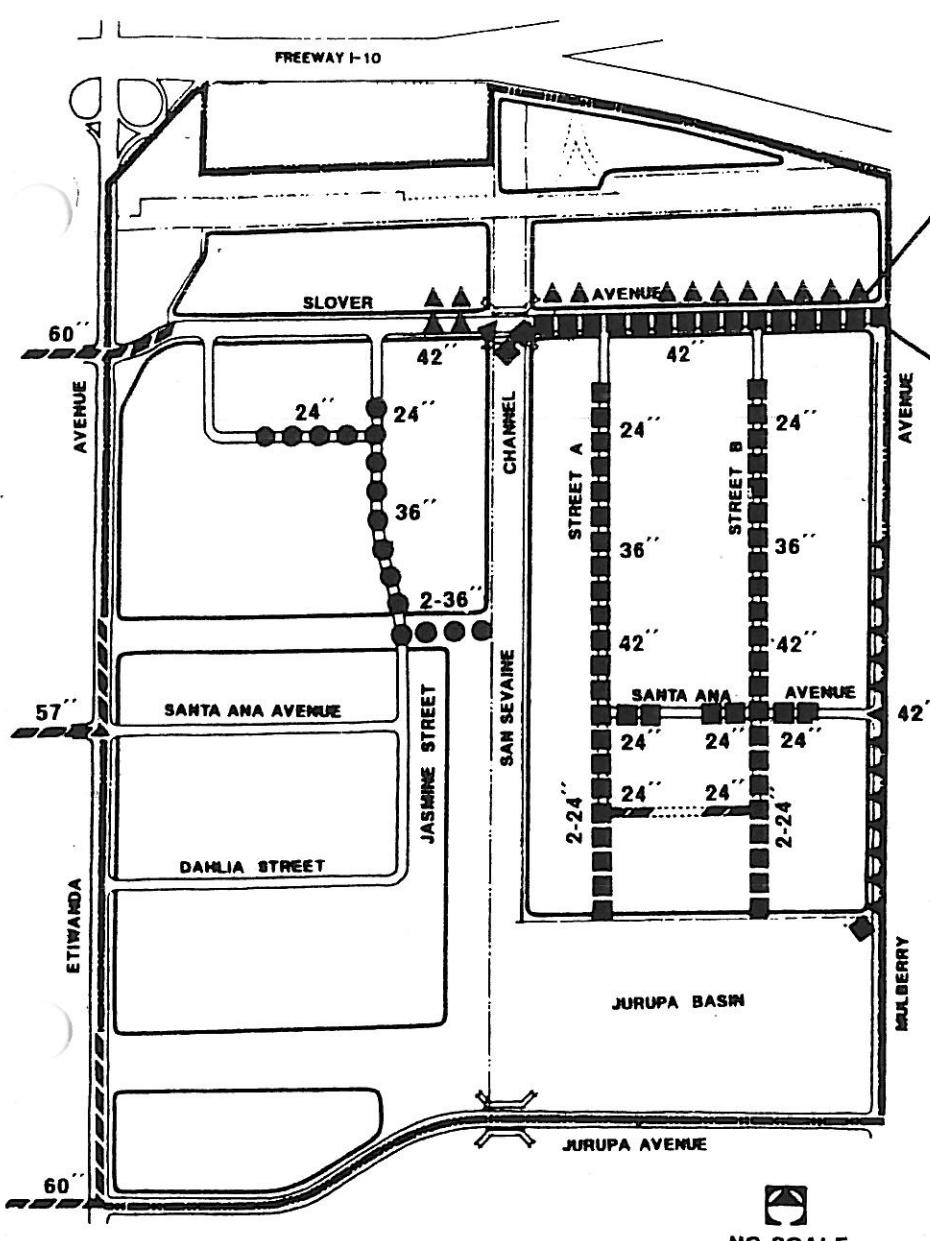
LEGEND

-  **PHASE 1A**
-  **PHASE 1B**
-  **PHASE II**
-  **PHASE III**

THE WATER MAINS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

**WATER  
SYSTEM  
PHASING**  
**CITY OF FONTANA**  
**FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



**NORTH SIDE PHASE IB**  
(North laterals and catch basin)

**SOUTH SIDE PHASE IA**  
(Mainline, south laterals and catch basins)

### LEGEND

**PHASE IA**

**PHASE IB**

**PHASE II**

**PHASE III**

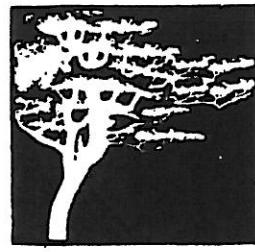
#### NOTE:

THE LOCATION OF THESE STREETS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL STREETS.

## STORM DRAIN PHASING

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



---

**CITY OF FONTANA  
FONTANA GATEWAY · SPECIFIC PLAN · EIR**

---

WILLIAMS · KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

ORDINANCE NO. 869

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF FONTANA  
ADOPTING THE FONTANA GATEWAY SPECIFIC PLAN (SPECIFIC  
PLAN NO. 13)

THE CITY COUNCIL OF THE CITY OF FONTANA DOES ORDAIN AS  
FOLLOWS:

SECTION 1: The Fontana Gateway Specific Plan (Specific Plan No. 13) in its entirety is hereby adopted by reference, a copy of which is on file in the City Clerk's office.

SECTION 2: This ordinance shall take effect thirty (30) days after the date of its adoption and prior to the expiration of fifteen (15) days from the passage thereof shall be published at least once in the Herald News, a newspaper of general circulation, published and circulated in the City of Fontana and thenceforth and thereafter the same shall be in full force and effect.

APPROVED AND ADOPTED this 21st day of April, 1987

/s/ Nathan A. Simon  
Mayor of the City of Fontana

ATTEST:

/s/ Patricia M. Murray  
City Clerk

I, Patricia M. Murray, City Clerk of the City of Fontana and Ex-Officio Clerk of the City Council, do hereby certify that the foregoing ordinance, which was introduced at a regular meeting of said City Council on the 7th day of April, 1987 was finally passed and adopted not less than five days thereafter on the 21st day of April, 1987, by the following vote, to-wit:

AYES: Mayor Simon, Councilmen Boyles, Day, Koehler, Kragness

NOES: None

ABSENT: None

/s/ Patricia M. Murray  
City Clerk

I further testify that said ordinance was thereupon signed by the Mayor of the City of Fontana.

/s/ Patricia M. Murray  
City Clerk of the City of Fontana

**FINAL ENVIRONMENTAL IMPACT REPORT**

**FONTANA GATEWAY SPECIFIC PLAN  
(Amendment #4 to the Southwest  
Industrial Park Redevelopment Project Area  
and  
General Plan Amendment 86-4)**

**(State Clearinghouse Number 86072803)**

**Prepared for:**

**CITY OF FONTANA**

**By:**

**PLANNING NETWORK  
1055 N. Euclid Avenue  
Ontario, CA 91762**

**JANUARY 1987**

**(IMPORTANT! TEXT CHANGES HAVE BEEN MADE: REFER TO ADDENDUM  
TO ENSURE CORRECTNESS OF INFORMATION)**

D.	<b>Biotic Resources</b>	IV-D-1
1.	Environmental Issue	IV-D-1
2.	Existing Setting	IV-D-1
3.	Project Impacts	IV-D-1
4.	Mitigation Measures	IV-D-2
5.	Analysis of Significance	IV-D-2
E.	<b>Noise</b>	IV-E-1
1.	Environmental Issue	IV-E-1
2.	Existing Setting	IV-E-1
3.	Project Impacts	IV-E-3
4.	Mitigation Measures	IV-E-5
5.	Analysis of Significance	IV-E-6
F.	<b>Safety/Nuisance</b>	IV-F-1
1.	Environmental Issue	IV-F-1
2.	Existing Setting	IV-F-1
3.	Project Impacts	IV-F-1
4.	Mitigation Measures	IV-F-2
5.	Analysis of Significance	IV-F-2
G.	<b>Circulation</b>	IV-G-1
1.	Environmental Issue	IV-G-1
2.	Existing Setting	IV-G-1
3.	Project Impacts	IV-G-5
4.	Mitigation Measures	IV-G-13
5.	Analysis of Significance	IV-G-13
H.	<b>Public Services</b>	IV-H-1
1.	Environmental Issue	IV-H-1
2.	Existing Setting	IV-H-1
3.	Project Impacts	IV-H-2
4.	Mitigation Measures	IV-H-4
5.	Analysis of Significance	IV-H-5
I.	<b>Energy Resources</b>	IV-I-1
1.	Environmental Issue	IV-I-1
2.	Existing Setting	IV-I-1
3.	Project Impacts	IV-I-1
4.	Mitigation Measures	IV-I-2
5.	Analysis of Significance	IV-I-3
J.	<b>Water Service</b>	IV-J-1
1.	Environmental Issue	IV-J-1
2.	Existing Setting	IV-J-1
3.	Project Impacts	IV-J-2
4.	Mitigation Measures	IV-J-5
5.	Analysis of Significance	IV-J-5

## CONTENTS

I.	Introduction	I-A-1
A.	Contact Persons	I-A-1
B.	CEQA Requirements	I-B-1
C.	Document Format	I-C-1
II.	Project Description	II-A-1
A.	Site Location, Orientation and Description	II-A-1
B.	Background and Objectives	II-B-1
1.	Background	II-B-1
2.	Objectives	II-B-2
C.	General Characteristics	II-C-1
D.	Intended Use of the EIR	II-D-1
III.	Executive Summary	III-A-1
	Project Description	III-A-1
	Environmental Impacts and Measures	III-A-1
IV.	Detailed Environmental Analysis	IV-A-1
A.	Soils	IV-A-1
1.	Environmental Issue	IV-A-1
2.	Existing Setting	IV-A-1
3.	Project Impacts	IV-A-3
4.	Mitigation Measures	IV-A-4
5.	Analysis of Significance	IV-A-5
B.	Air Resources	IV-B-1
1.	Environmental Issue	IV-B-1
2.	Existing Setting	IV-B-1
3.	Project Impacts	IV-B-6
4.	Mitigation Measures	IV-B-9
5.	Analysis of Significance	IV-B-9
C.	Drainage/Flooding	IV-C-1
1.	Environmental Issue	IV-C-1
2.	Existing Setting	IV-C-1
3.	Project Impacts	IV-C-2
4.	Mitigation Measures	IV-C-2
5.	Analysis of Significance	IV-C-3

K.	Sewer Service	IV-K-1
1.	Environmental Issue	IV-K-1
2.	Existing Setting	IV-K-1
3.	Project Impacts	IV-K-1
4.	Mitigation Measures	IV-K-3
5.	Analysis of Significance	IV-K-3
L.	Cultural Resources	IV-L-1
1.	Environmental Issue	IV-L-1
2.	Existing Setting	IV-L-1
3.	Project Impacts	IV-L-1
4.	Mitigation Measures	IV-L-1
5.	Analysis of Significance	IV-L-2
V.	Alternatives to the Proposed Project	V-A-1
A.	No Development (Maintain Existing Uses)	V-A-2
B.	No Project (Existing Land Use Designations)	V-B-1
C.	Land Use Alternative "One"	V-C-1
D.	Land Use Alternative "Two"	V-D-1
VI.	Relationship Between Local Short-Term Uses and the Maintenance and Enhancement of Long-Term Productivity	VI-A-1
VII.	Significant Irreversible Environmental Changes	VII-A-1
VIII.	Cumulative and Growth-Inducing Impacts	VIII-A-1
IX.	Document Preparation Resources	IX-A-1
A.	Preparation Staff	IX-A-1
B.	Persons and Organizations Consulted	IX-B-1
C.	References	IX-C-1

Appendices:

Appendix A: Environmental Description Form and Agency Responses

Appendix B: Analysis/Summary: Kaiser Specific Plan Infrastructure, Krueper Engineering and Associates, Inc., June 1986

Appendix C: Proposed Project Specific Plan

TABLES

II-C-A	Proposed Project: Statistical Summary	II-C-1
III-A	Summary Matrix	III-A-2
IV-A-A	Onsite Soil Characteristics	IV-A-1
IV-B-A	Local Air Quality, 1983	IV-B-2
IV-B-B	Existing Electrical Consumption Air Quality Impacts	IV-B-4
IV-B-C	Existing Natural Gas Consumption Air Quality Impacts	IV-B-5
IV-B-D	Existing Mobile Source Air Pollutant Emissions	IV-B-6
IV-B-E	Projected Electrical Consumption Air Quality Impacts	IV-B-7
IV-B-F	Projected Natural Gas Consumption Air Quality Impacts	IV-B-7
IV-B-G	Mobile Source Impacts	IV-B-8
IV-E-A	Distances to Existing Noise Contours	IV-E-2
IV-E-B	Distances to Projected Noise Contours	IV-E-4
IV-G-A	Existing Roadway Capacity and Utilization	IV-G-4
IV-G-B	Projected Trip Generation	IV-G-7
IV-G-C	Traffic Impact Analysis: Existing Roadways	IV-G-8
IV-G-D	Traffic Impact Analysis: Ultimate Roadways	IV-G-9
IV-G-E	Level of Service: Ultimate Roadway Configurations	IV-G-11
IV-I-A	Projected Daily Energy Consumption	IV-I-2

IV-J-A	Fire Flow Test Results	IV-J-2
IV-J-B	Projected Water Consumption	IV-J-3
IV-K-A	Projected Sewage Generation	IV-K-2
V-A	Project Alternatives Comparison Matrix	V-E-1
V-C-A	Land Use Alternative "One": Comparison to Proposed Project	V-C-1
V-D-A	Land Use Alternative "Two": Comparison to Proposed Project	V-D-1

## FIGURES

II-A-1.	Regional Location	II-A-2
II-A-2.	Site Location	II-A-3
II-C-1.	Land Use Plan	II-C-3
II-C-2.	Water Concept Plan	II-C-5
II-C-3.	Sewer Concept Plan	II-C-6
II-C-4.	Drainage Concept Plan	II-C-7
II-C-5.	Figure Deleted	II-C-8
II-C-6.	Circulation Plan	II-C-9
IV-A-1.	Soils	IV-A-2
IV-J-1.	Services Sub-Areas	IV-J-3

## INTRODUCTION

### I. INTRODUCTION

#### A. CONTACT PERSONS

The primary contact person for this report is Ernie Perea. He can be contacted at the City of Fontana Planning Department, 8353 Sierra Avenue, Fontana, CA 92335, or at (714) 350-7600. The alternate contact person is Lloyd Zola, AICP, preparer of this report. He can be contacted at FORMA-PLANNING NETWORK, 1055 North Euclid Avenue, Ontario, CA 91762, or at (714) 983-4144. The project applicant is the City of Fontana.

## INTRODUCTION

### B. CEQA REQUIREMENTS

The proposed specific plan requires discretionary action by the City of Fontana, which is the Lead Agency. According to the California Environmental Impact Report Guidelines, all discretionary projects must be reviewed to determine potential effects on the environment.

An initial study and environmental determination were prepared by the City of Fontana which indicated the need for an Environmental Impact Report to be prepared in accordance with the California Environmental Quality Act (CEQA). The purpose of this EIR is to inform the public and decision makers about the nature of the proposed project. Its purpose is also to inform the public about the extent and kinds of impacts which the project and its alternatives are expected to have on the environment if the project is carried out.

The City of Fontana's environmental determination stated that the following environmental factors could affect or be affected by the proposed project, and were therefore to be discussed in this document (the City's Initial Study is included in Appendix A). The City's environmental determination is paraphrased below.

**Soils** Development within the study area may result in increased erosion and siltation. Potential erosion/siltation impacts resulting from the proposed project will be examined.

**Air Quality** The proposed project may result in an increase in the emission of air pollutants within the study area.

**Drainage/Flooding** Development within the study area may result in increased storm flows, due to the replacement of existing pervious areas with impervious surfaces.

**Biotic Resources** The impacts of the introduction of new plant species into the study area, and the potential for deterioration of existing fish and wildlife habitat, if any, will be examined.

**Noise** The proposed project will result in increases in noise levels within the study area, and may result in an increase in the number of persons subjected to severe noise levels.

## INTRODUCTION

**Safety/Nuisance** Development of the proposed project may result in the production of objectionable odors, and may result in an increase in the potential for explosions and/or the release of hazardous substances within the study area.

**Circulation** The impacts of the proposed project on the local circulation system, including the need for roadway improvements, if any, will be examined.

**Public Services** The impacts of the proposed project on police, fire, and solid waste services will be examined.

**Energy Resources** The impacts of the proposed project on the consumption of electricity and natural gas will be examined.

**Water Service** The impacts of the proposed project on the demand for water service will be examined.

**Sewer Service** The impacts of the proposed project on the demand for sewer service will be examined.

**Cultural Resources** The proposed project may result in the alteration or destruction of existing cultural or historic resources within the study area.

## INTRODUCTION

### C. DOCUMENT FORMAT

This environmental impact report is arranged in the following manner:

Chapter II describes the proposed project, including the location of the project area and the background of the proposal.

Chapter III contains a summary of the detailed environmental analysis contained in Chapter IV.

Chapter IV is a detailed analysis of existing conditions, expected impacts, and proposed mitigation measures for the proposed project.

Chapter V provides a description of the alternative land use scenarios for the project area.

Chapters VI to VIII contain discussions of topical issues required by CEQA. Analyses of cumulative impacts, long-term impacts, irreversible changes, growth-inducing and significant unavoidable impacts are included.

Chapter IX includes information on the resources utilized to prepare this report, including: the names of the preparation team, the names of the organizations and individuals who were consulted, and a bibliography.

The final chapter contains appendices, including the Initial Study prepared by the County, the Notice of Preparation, and responses to the Notice of Preparation.

## PROJECT DESCRIPTION

### II. PROJECT DESCRIPTION

#### A. SITE LOCATION, ORIENTATION AND DESCRIPTION

The Fontana Gateway Specific Plan site is located on approximately 755 ± acres in the unincorporated portion of San Bernardino county within the Sphere of Influence of the City of Fontana (see Figure II-A-1, Regional Location). The site is generally bounded by the I-10 freeway on the north, Mulberry Avenue on the east, Jurupa Avenue on the south, and Etiwanda Avenue and the City of Ontario on the west (see Figure II-A-2).

The site is generally flat, sloping slightly to the south. The only major topographical features within the study area are the San Sevaine Channel, which generally bisects the site in a north-south direction, and the Jurupa Basin, a storm flow retention basin located in the southeast corner of the site. Both the channel and basin are part of the regional storm drain system which carries flows from the mountains and developed areas located north of the site to the Santa Ana River.

The study area has been partially developed in the past with industrial uses primarily related to the Kaiser steel plant to the north of the site. Due to the closing of that facility, a significant portion of the industrial uses within the study area have been closed or reduced in scale. A large portion of the land associated with the existing industrial uses is utilized for outdoor storage of materials.

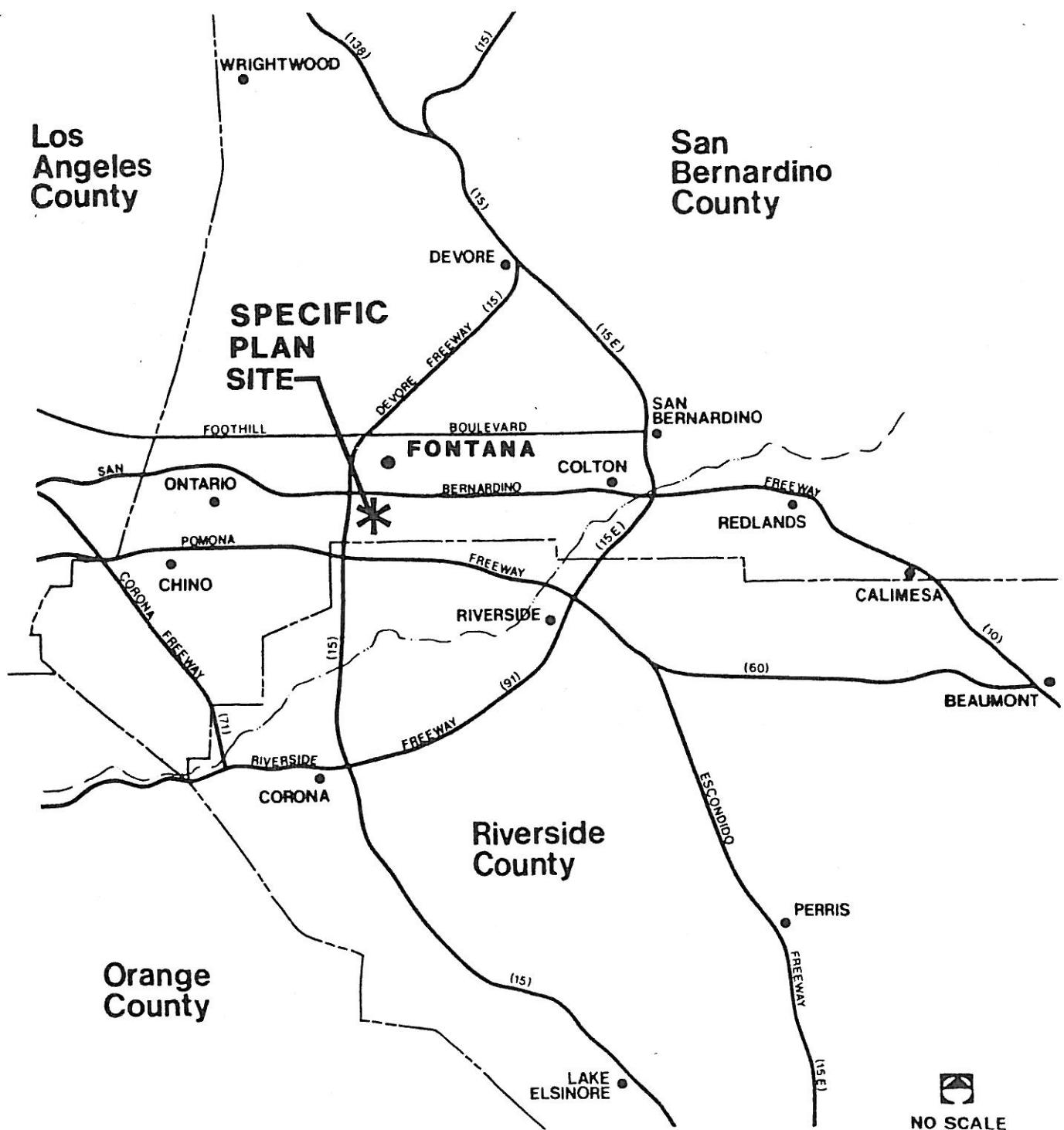
This environmental impact report is intended to encompass the following entitlements related to this project:

1. Annexation.
2. Specific Plan adoption.
3. Redevelopment area formation.
4. Assessment district formation.
5. Fiscal plan adoption.
6. Development Agreement(s) and/or Owner Participation Agreement(s).
7. Development plan approvals.
8. Master design plans.

PROJECT DESCRIPTION

9. Master Tentative Tract(s).
10. General Plan Amendment No. 86-4.
11. Any other discretionary approvals as may be required by the City of Fontana and other regulatory agencies.

This Environmental Impact Report shall also serve as a baseline assessment for related discretionary entitlements including, but not limited to, permits and/or approvals by such agencies as the South Coast Air Quality Management District, the Santa Ana Regional Water Quality Control Board, and others.

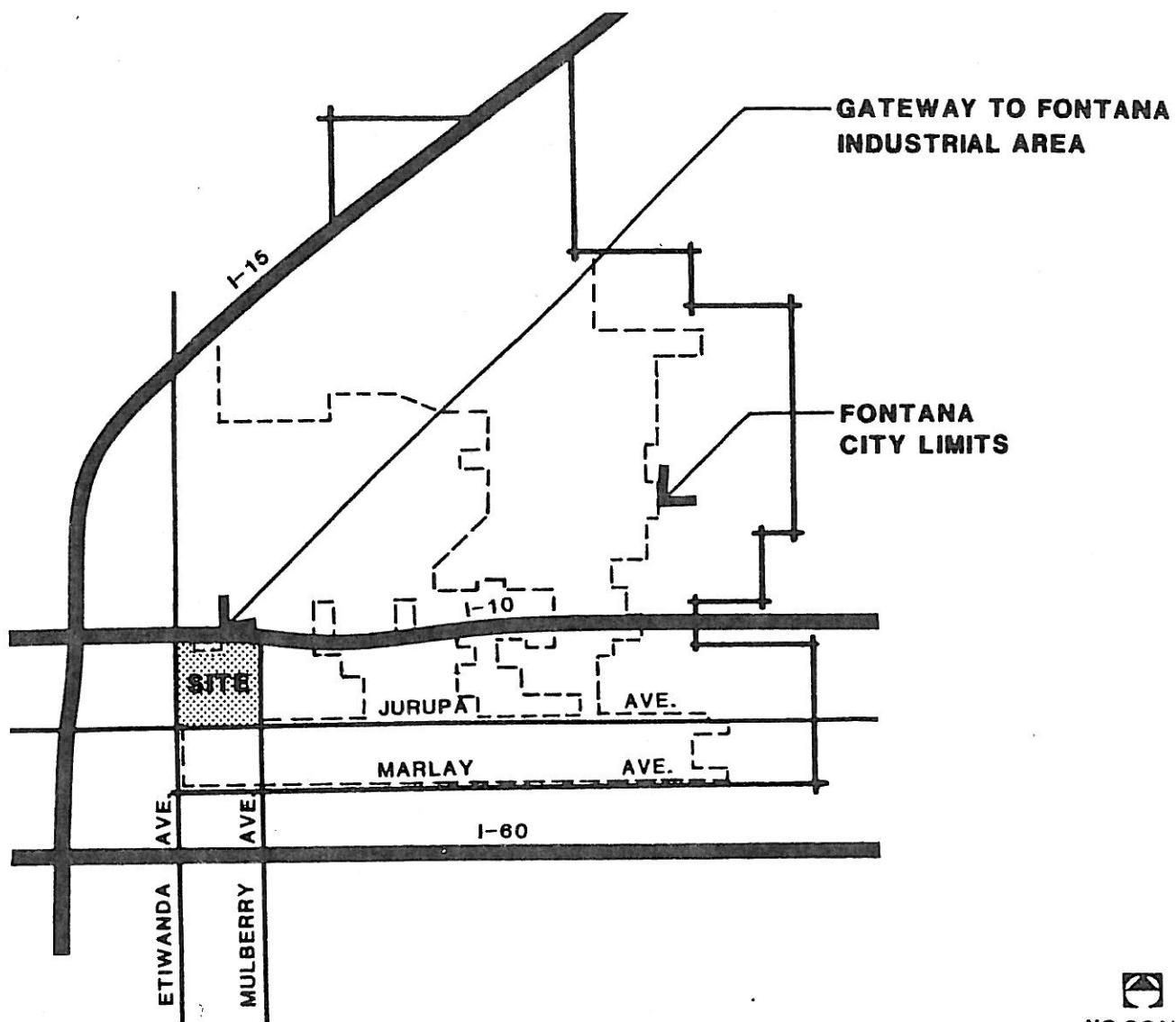


## VICINITY MAP

FIGURE II-A-1

# CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



NO SCALE

## LOCATION MAP

FIGURE II-A-2

# CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## PROJECT DESCRIPTION

### B. BACKGROUND AND OBJECTIVES

#### 1. Background

The study area, which is located in the unincorporated portion of San Bernardino County, is presently undergoing annexation into the City of Fontana. As part of the annexation process, the City of Fontana in 1985 initiated the specific plan study to aid in the development of a public-private industrial development plan for the study area. It is the specific plan resulting from that study which this document examines.

The methodology employed for the preparation of the Fontana Gateway Specific Plan was divided into three phases, with each phase building upon the findings of the previous phase. The initial work efforts included an assessment of local issues, problems and opportunities, City goals and objectives, physical and environmental characteristics of the study area, economic development and market potential, and the availability and limitations of area infrastructure.

The second phase involved the development of alternative land use concepts for the study area. These alternatives explored various ways that the objectives of both the City and local landowners could be translated into land uses and design concepts for the study area. This phase also included a determination of the level of marketing efforts which would be necessary to develop each of the alternative concepts. Each of the alternative concepts was evaluated in terms of its feasibility, infrastructure needs and costs, environmental consequences, level of risk, and potential financial returns to the City and local landowners, and a preferred alternative was selected.

The final phase of work involved the refinement of the preferred alternative concept through more detailed planning and the formulation of proposed development and design controls, as well as a phasing program. The purpose of the refined plan is to ensure that the proposed development of the Fontana Gateway Specific Plan study area, including the provision and financing of infrastructure is realistic, that it can be built, operated, and maintained within the capabilities of responsible public agencies, and that it furthers the goals and objectives of the community and local landowners.

## PROJECT DESCRIPTION

### 2. Objectives

The objective of the proposed project is to formulate a public-private industrial development program for the 755 ± acre study area which will be mutually beneficial to the landowners, as well as the City of Fontana. The specific objectives of the proposed project, as stated in the Fontana Gateway Specific Plan are to:

- \* Provide economic development opportunities;
- \* Ensure long-term fiscal and financial benefits to the City;
- \* Increase the taxable base of the City;
- \* Provide for the orderly and feasible industrial development of the study area without creating a detrimental effect on existing uses, even though they may be non-conforming uses; and
- \* Upgrade the gateway image of the City.

## PROJECT DESCRIPTION

### C. GENERAL CHARACTERISTICS

As discussed above, the objective of the proposed project is the approval of a specific plan for the industrial development of the study area. The proposed project is comprised of industrial uses such as mixed manufacturing, wholesale, and storage; commercial uses such as professional services, limited retail, and office uses; and public facilities (consisting primarily of drainage facilities). Included within the proposed industrial uses are "incubator" uses, which consist of small, start-up businesses contained within an enclave of multi-tenant facilities ranging in square footage from 2,000 to 10,000 square feet. The proposed service commercial and office land uses will consist of those types of uses which will support and be supported by the industrial land uses.

The specific plan area encompasses approximately 755+ acres. Of these, approximately 151 consist of roadways and public facilities. Of the remaining 639 acres, approximately 257 have been previously developed; thus, new development will take place on the approximately 380 + vacant acres within the site. A statistical summary of the proposed project is shown in Table II-C-A.

TABLE II-C-A  
PROPOSED PROJECT: STATISTICAL SUMMARY

Land Use	Acres	Percent of Site
Existing Development'	257	33%
Existing Vacant	382	48%
Public Facilities/ Roadways	151	19%
Total.....	755	100%

-----  
' all acreage references are approximate

The proposed specific plan consists of the following major components: Land Use Master Plan, Circulation Master Plan, Landscape/Streetscape Plan, Infrastructure Concept Plans, and a Phasing Concept Plan. These are discussed below.

## PROJECT DESCRIPTION

**Land Use Master Plan** This consists of the land use designations for the study area. As shown in Figure II-C-1, the Land Use Master Plan designates the entire site for industrial uses, which include "incubator" uses and service commercial/office development. These are described in the proposed specific plan, which is included in the appendices to this document.

**Circulation Master Plan** This consists of conceptual design standards for the development of roadways within the specific plan area, and includes the following elements:

- 1) Alignments for arterial highways and collector streets within the study area.
- 2) Rights-of-way and cross-sections for arterial highways, collector streets, and local streets within the study area.

The Circulation Master Plan was based upon a traffic study for the proposed project, which is included in the appendices to this document. The traffic study addressed the following issues regarding design of the circulation plan:

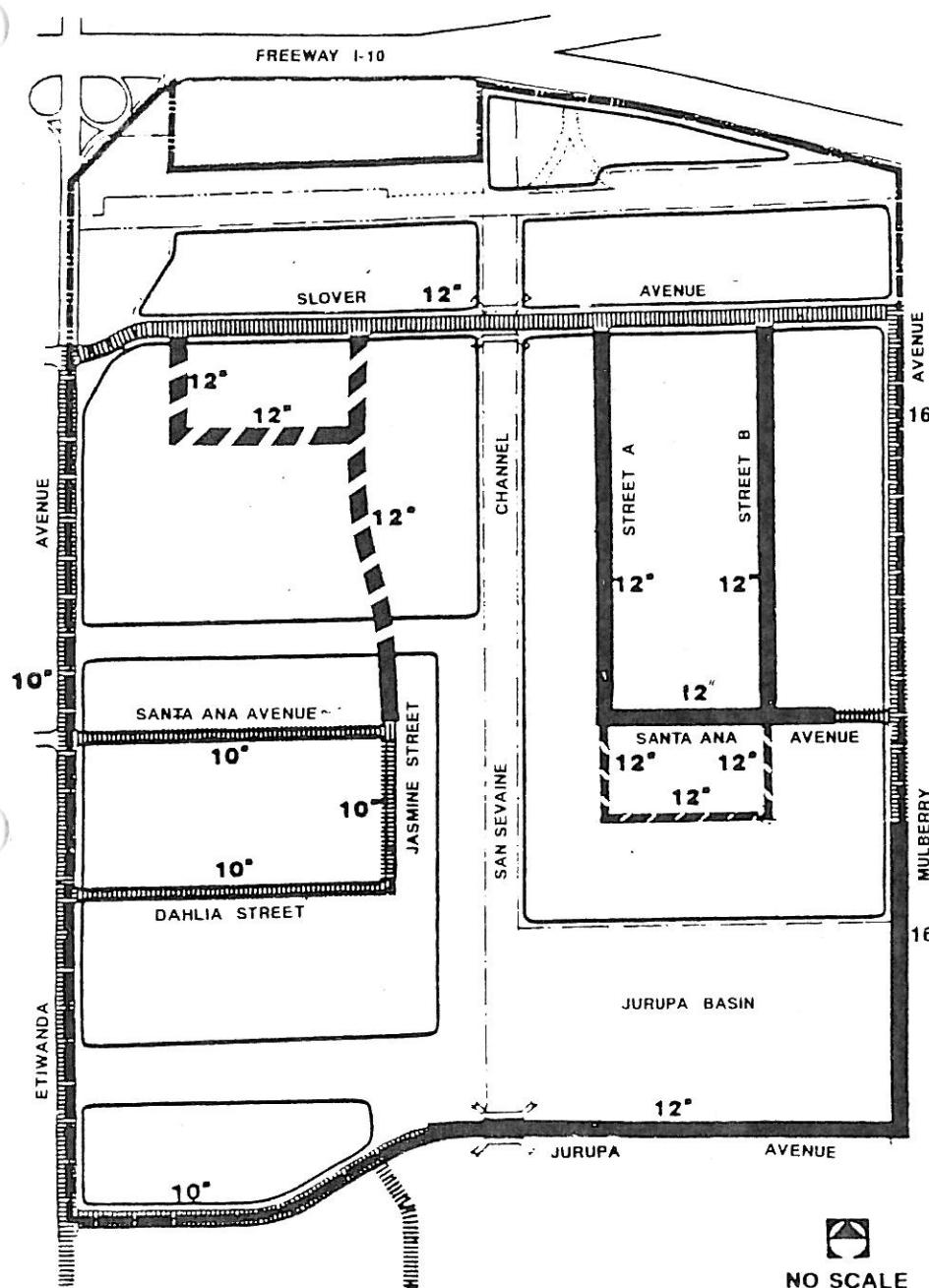
- Estimated trip generation and distribution on the surrounding street system.
- Estimated future traffic volumes on all study area roads.
- Recommended street classifications, rights-of-way, and cross-sections.
- Estimated future volume-to-capacity ratios and intersection utilization rates for street and intersections within the study area.

## PROJECT DESCRIPTION

**Landscape/Streetscape Plan** This is intended to provide design criteria related to landscape coverage, berthing, tree size, maintenance, and species of plants utilized in study area landscaping. Specific tree varieties are established in the Plan for major transportation corridors within the site (Etiwanda, Jurupa, Mulberry, and Slover avenues and the I-10 freeway) to provide streetscape continuity in these areas.

**Infrastructure Concept Plans** These consist of conceptual designs for onsite water and sewer systems, based on projected water consumption and fire flow requirements and upon resulting sewage generation (see Figures II-C-2 and II-C-3). Also included is a conceptual design for onsite flood protection (see Figure II-C-4), based upon incorporation of projected study area storm flows into the regional drainage system, of which two components -- San Sevaine Channel and Jurupa Basin -- are located within the site.

# WATER SYSTEM



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

- EXISTING:** Diagonal hatching
- PROPOSED:** Horizontal hatching
- OPTIONAL:** Vertical hatching

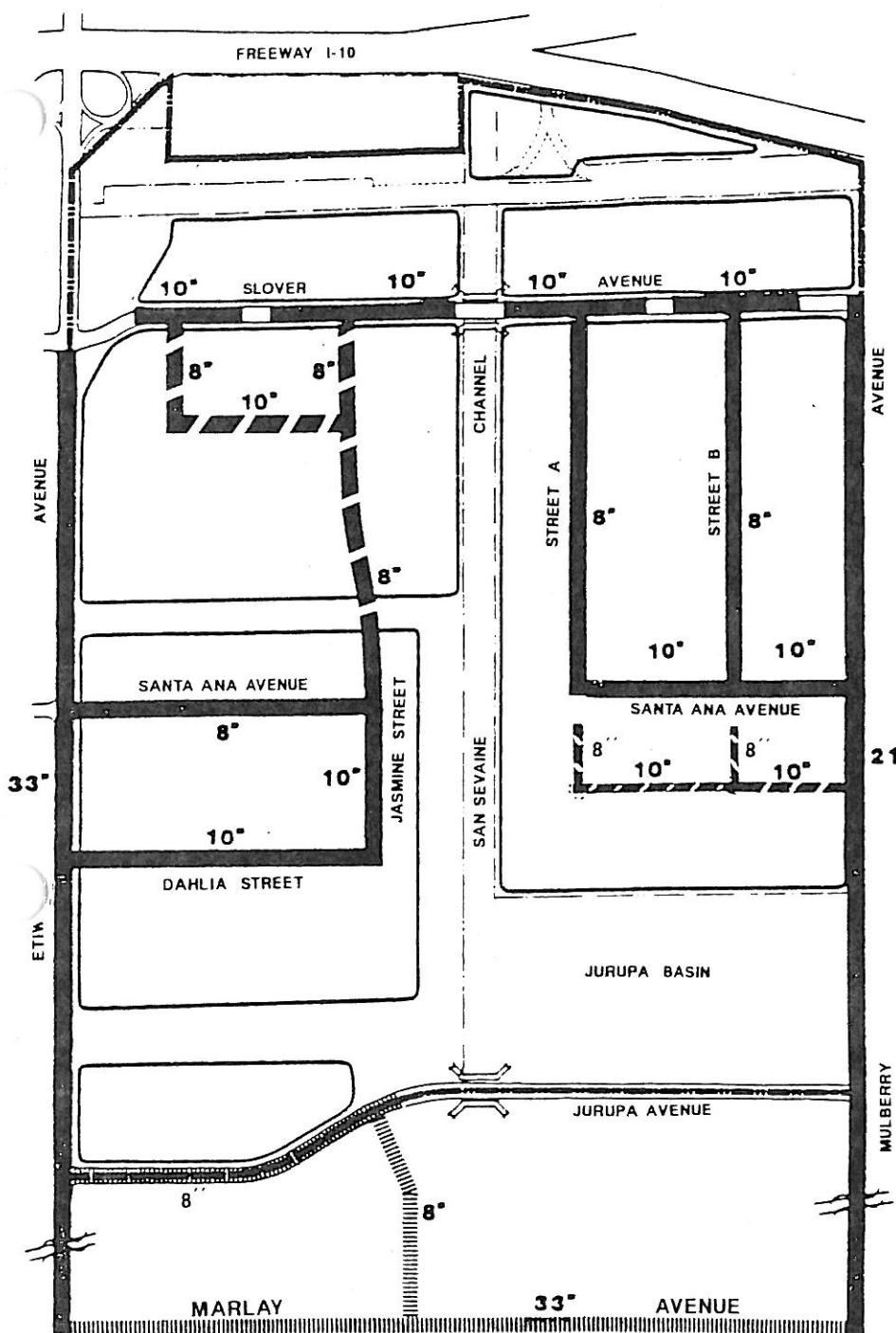
THE WATER MAINS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA

FIGURE II-C-2

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# SEWER SYSTEM



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

- EXISTING
- PROPOSED
- OPTIONAL

NO SCALE

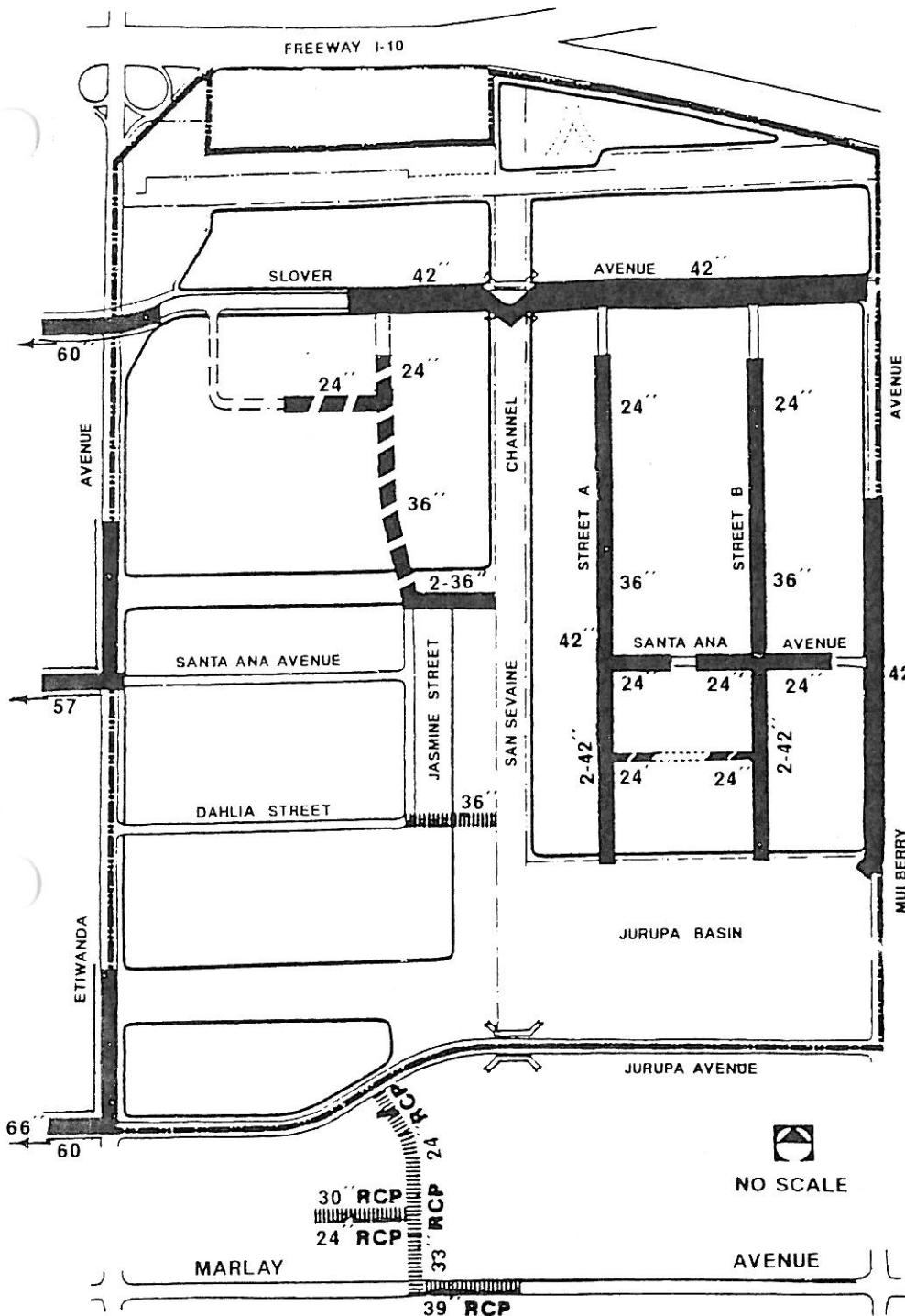
THE INTERIOR SANITARY SEWERS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA

FIGURE II-C-3

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc  
FORMA-PLANNING NETWORK

# STORM DRAIN SYSTEM



## NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

- EXISTING
- PROPOSED
- OPTIONAL
- BACKBONE STORM DRAIN SYSTEM TO LOWER ETIWANDA CHANNEL

THE STORM DRAIN EXTENSION ON DAHLIA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREET EXTENSION OF DAHLIA

FIGURE II-C-4

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## PROJECT DESCRIPTION

### D. INTENDED USE OF THE EIR

This Environmental Impact Report is intended to be used for the following discretionary actions by the Lead Agency, the City of Fontana:

- Specific Plan Amendment #4 (Southwest Industrial Park and Specific Plan)
- Changes of Zone (to be submitted)
- Individual Plot Plans and Use Permits (to be submitted)
- Individual Grading Plans (to be submitted)
- Annexation of Specific Plan Amendment #4 to the City of Fontana.
- Formation of Redevelopment Agency Plan Amendment #4.
- Assessment districts formation.
- Fiscal plan adoption.
- Development Agreement(s) and/or Owner Participation Agreement(s).
- Development plan approval(s).
- Master design plan(s).
- Master tentative tract(s).
- General Plan Amendment No. 86-4.
- Any other discretionary approval(s) as may be required by the City of Fontana and other regulatory agencies.

## SUMMARY

### III. SUMMARY

#### 1. Project Description

The objective of the proposed project is the approval of a specific plan for the industrial development of the study area. The proposed project is comprised of industrial uses such as mixed manufacturing, wholesale, and storage; commercial uses such as professional services, limited retail, and office uses; and public facilities (consisting primarily of drainage facilities). Included within the proposed industrial uses are "incubator" uses, which consist of small, start-up businesses contained within an enclave of multi-tenant facilities ranging in square footage from 2,000 to 10,000 square feet. The proposed service commercial and office land uses will consist of those types of uses which will support and be supported by the industrial land uses.

The specific plan area encompasses approximately 755+ acres. Of these, approximately 151 consist of roadways and public facilities. Of the remaining 639 acres, approximately 257 have been previously developed; thus, new development will take place on the approximately 380 + vacant acres within the site.

The proposed specific plan consists of the following major components: Land Use Master Plan, Circulation Master Plan, Landscape/Streetscape Plan, Infrastructure Concept Plans, and a Phasing Concept Plan. These are discussed in Section II of this document.

#### 2. Environmental Impacts and Measures

The following pages contain summaries of the various environmental impacts associated with the proposed project, as well as a listing of mitigation measures proposed to reduce levels of impacts to acceptable levels. Table III-A is a summary matrix of impacts associated with the proposed project.

## SUMMARY

**TABLE III-A: SUMMARY IMPACT MATRIX**

	Direct	Indirect	Short Term	Long Term	Cumula-tive	No Impact	Adverse/ Mitigable	Signifi-cant/Not Mitigable	No Signi-ficant Impact
Soils	X			X					X
Air									
Resources	X			X	X	X		X	
Drainage/ Flooding	X				X	X		X	
Biotic Resources									X
Noise	X			X	X	X			X
Public Health/ Safety/ Nuisance									
Circulation	X		X	X	X			X	
Public Services	X			X	X	X			X
Energy	X			X	X	X			X
Water Service	X			X	X	X			X
Sewer Service	X			X	X	X			X
Cultural Resources							X		

	Direct	Indirect	Short Term	Long Term	Cumula-tive	No Impact	Adverse/ Mitigable	Signifi-cant/Not Mitigable	No Signi-ficant Impact

## SUMMARY

1. **Soils.** Development of the study area will result in the generation of fugitive dust. However, since: 1) dust control measures will be implemented, 2) onsite soils are marked by slight erosion hazards and 3) erosion control plans will be implemented, project-related impacts will be insignificant.

### Mitigation Measures

The following measures are appropriate to mitigate impacts related to the generation of fugitive dust:

- \* Regular watering of cleared areas.
- \* Establishment of maximum speed limits within construction areas.
- \* Use of soil tackifiers, soil stabilization mulches, and/or oil emulsions, where feasible.

In order to mitigate water erosion, all grading plans submitted to the City of Fontana shall include an erosion control plan for approval by the City Engineer.

2. **Air Resources.** Development of the proposed project will create construction source impacts, and will result in an increase in point source and mobile source impacts, compared to present levels.

However, as discussed in the "Project Description" portion of this document, land uses which will result under the proposed project are not significantly different than those which would be expected under the "No Project" scenario. Land uses contained in the "No Project" scenario have been utilized in planning assumptions made by the City of Fontana, and are thus included in mitigation measures contained in the City of Fontana General Plan, as well as in the San Bernardino County General Plan and the South Coast Regional Air Quality Management Plan.

While there exists an uncertainty regarding point source impacts which might be generated by future mixed manufacturing uses, implementation of emissions control measures as required by the South Coast Air Quality Management District will reduce such impacts to a level of insignificance.

### Mitigation Measures

The following measures are proposed to mitigate air quality-related impacts resulting from the proposed project:

## SUMMARY

### Point Source Impacts

Mitigation of point source impacts is primarily related to reduction of the amount of electricity and natural gas consumed onsite. Measures to reduce onsite consumption of these resources are included in the "Energy Resources" section of this document.

In addition, the following mitigation measure is proposed:

- All uses which have the potential to generate air pollutant emissions shall be required to construct such control measures as may be required by the South Coast Air Quality Management District.

### Mobile Source Impacts

Mitigation of mobile source impacts is related to the reduction of vehicle trips and miles driven. Measures to reduce vehicle travel generated by the proposed project are contained in the "Circulation" section of this document.

### Point and Mobile Source Impacts Related to Construction

- Controlling fugitive dust by regular watering, pacing construction roads or other preventive measures in order to comply with District Rule 403.
- Maintaining engines in proper tune.
- Using water trucks or sprinkler systems to keep all areas of vehicle movement damp enough to prevent dust raised when leaving the site.
- Wetting down areas in the late morning and after work is completed for the day.
- Phasing and scheduling construction activities to level emission peaks.
- Discontinuing construction activities during first and second stage smog alerts.

3. Drainage/Flooding. Storm flows generated within the study area will increase upon development of the proposed project, due to the replacement of existing pervious areas with buildings, roadways, parking lots and other impervious surfaces. However, construction of storm drainage facilities as outlined in

## SUMMARY

the Project Engineer's report (included in the appendices to this report) will mitigate drainage-related impacts to a level of insignificance.

### Mitigation Measures

The following measures are proposed to mitigate impacts related to drainage:

- Concurrent with the development of individual phases of the proposed project (and prior to the issuance of the certificate of occupancy), a storm drainage system sufficient to serve the private improvements under development shall be installed or assured.
- The City of Fontana shall initiate negotiations with the City of Ontario for the construction of storm drainage connections to the lower Etiwanda Channel.

4. **Biotic Resources.** Since no significant biology presently exists within the study area, and since the proposed project includes landscaping of the site, no significant impacts to biology will result.

### Mitigation Measures

No mitigation measures for impacts to area biotic resources are proposed. However, as noted above, the proposed project includes a landscaping plan which will involve the planting of new grasses, shrubs and trees within the site.

5. **Noise.** Onsite and offsite noise levels will increase as the result of development of the proposed project. However, as noted above, development of the proposed project will not result in increases in noise levels above those which would be expected if the site were developed under the "No Project" scenario. In addition, the industrial uses proposed within the site are noise-tolerant, with the result that no onsite impacts will result. Thus, the proposed project will not result in significant impacts.

### Mitigation Measures

The following measures are proposed to mitigate noise-related impacts resulting from development of the proposed project:

## SUMMARY

### Construction-Related Noise

- To the extent possible, regular construction activity shall be limited to the weekday hours of 7:00 a.m. to 7:00 p.m.

### Long-Term Noise

- Interior noise levels within office portions of study area buildings shall be no higher than a CNEL of 50 dB within general offices and 45 dB within private offices. Where necessary, noise attenuation (such as insulation or distance) will be used to shield office areas from adjacent noise sources.
- Interior noise levels within retail structures shall be no higher than 60 dB CNEL.
- A combination of walls, berms, and building placement shall be utilized to ensure that noise generated by onsite industrial uses does not impact offsite residential areas.

6. **Public Health/Safety/Huimance.** Development of the study area may increase the potential for exposure of persons to hazardous materials or to accidents involving such materials. However, storage and use of such materials within the study area will be subject to applicable safety standards and to review by the Central Valley Fire District, thus reducing any expected impacts to a level of insignificance. In addition, it is possible that industrial processes which will be located within the study area will produce objectionable odors. However, the implementation of the SCAQMD permitting process will assure that such potential impacts are reduced to a level of insignificance.

### Mitigation Measures

The following measures are proposed to mitigate impacts related to storage and use of hazardous materials:

- All individual uses within the study area which utilize hazardous materials shall be required to install all storage and transport systems in accordance with applicable safety standards, and to file records of the type, quantity, storage method, and utilization of all hazardous materials with the Central Valley Fire District.

## SUMMARY

- All onsite uses which utilize hazardous materials shall provide a "Knox vault" or similar device in which shall be stored information regarding the types, locations, quantities, and uses of hazardous materials within the site.

7. **Circulation.** Traffic on site vicinity roadways will increase upon development of the proposed project. However, since the proposed project will generally result in impacts equivalent to those which would be expected under the "No Project" scenario, for which mitigation measures have been provided (in the form of master-planned roadway improvements), and since the proposed project includes the construction of an internal roadway system sufficient to carry projected traffic volumes, no significant impacts are expected to result.

It should be noted that significant impacts would result even without development of the proposed project if vicinity roadways are not improved to their ultimate widths. Thus, significant impacts may result if the construction of roadway improvements lags behind need.

Because of the negligible amount of additional rail traffic which will result from the expansion of rail service to the proposed project, no significant impacts are expected to result from the expansion of rail service as contemplated in the Specific Plan.

### Mitigation Measures

The following mitigation measures are proposed to mitigate traffic-related impacts resulting from the proposed project:

- Concurrent with the development of individual phases of the proposed project (and prior to the issuance of the certificate of occupancy), an internal roadway system sufficient to serve the private improvements under development shall be installed or assured.
- Signage and landscaping for project area roadways shall be installed as discussed in the proposed specific plan.

8. **Public Services.** Demand for police and fire protection services will increase upon development of the proposed project. However, ultimate levels of demand will not be greater than those which would be expected under the "No Project" scenario. Since 1) revenues from the proposed project will be sufficient to offset the cost of providing additional service, and 2) mitiga-

## SUMMARY

tion measures have been proposed to reduce demand for police and fire protection, no significant impact are expected to result. Although demands for solid waste services will increase, ultimate waste generation will not exceed presently projected levels for the "No Project" scenario; thus, no significant impacts will result.

### Mitigation Measures

The following measures are proposed to mitigate public service-related impacts of the proposed project:

#### **Police Protection**

- As project area development occurs, Police Department manpower and equipment should be expended as necessary to serve the proposed project and surrounding development.
- To reduce the need for police protection during construction, onsite security measures (see below) shall be provided for individual projects as development proceeds. The police department, along with the fire department, shall be provided with keys to all construction site gates.
- Adequate emergency access shall be provided to all study area structures, including the installation of "Knox Box"-type key receptacles for use by police and fire department personnel.
- The following are appropriate onsite security measures during construction:
  - \*\* Fencing of all outdoor storage areas.
  - \*\* Provision of nighttime security lighting for all study area structures and storage areas.

#### **Fire Protection**

- Since Station #4 provides adequate response times for study area development, and since new equipment will be added to this station's inventory, no additional fire facilities are necessary. However, development of significant numbers of structures in excess of 35 feet in height may require addition of ladder service to Station #4 or the relocation of existing ladder equipment to provide optimum response times.

## SUMMARY

- Concurrent with the development of individual phases of the proposed project (and prior to issuance of the certificate of occupancy), a water system sufficient to serve the private improvements under development shall be installed or assured.
- Site plans within individual developments shall be designed to have a minimum of two points of ingress and egress at all times, and shall be designed to accommodate access by emergency vehicles, including adequate turn-around areas.
- If required by City Ordinance, all study area structures shall be provided with automatic fire suppression systems, as required by City ordinance (if enacted) or the CVFD.

### Solid Waste

No mitigation is necessary to mitigate solid waste impacts.

9. Energy Resources. Consumption of energy resources within the study area will increase upon development of the proposed project. However, projected consumption of energy resources will not differ significantly from presently projected levels, due to the inclusion of similar land uses in the proposed project and "No Project" scenarios. Thus, projected energy consumption is within levels included in facilities planning and programming by the Southern California Edison and Southern California Gas companies, and has therefore been previously mitigated to a level of insignificance. However, as also noted, demand for service may occur sooner than would be expected under the "No Project" scenario, due to the expedited construction schedule included which would result from the proposed project.

### Mitigation Measures

The following measures are proposed to mitigate energy-related impacts resulting from development of the proposed project:

- All structures shall be required to comply with applicable energy conservation measures required by the California Administrative Code.
- To the extent possible, and consistent with other mitigation measures, onsite buildings and infrastructure shall be designed to:

## SUMMARY

- \*\* maintain internal roadways at the minimum widths possible, consistent with maintaining safe circulation, to reduce the amount of heat reflected from roadways onto structures.
- \*\* landscaping should be provided to reduce the amount of energy reflected towards buildings.
- \*\* provide for reflective coatings on roofs, wherever possible, to reduce heat absorption.
- \*\* provide for the use of tinted or reflective glass to help regulate interior temperatures.
- \*\* provide for the use of energy-efficient lighting in both indoor and outdoor applications.
- \*\* provide for the insulation of water heaters and hot water transmission lines to reduce heat loss.
- \*\* incorporate, wherever possible, the utilization of the most energy-efficient technology available for manufacturing or other energy-intensive uses.
- \*\* encourage the installation of cogeneration facilities to utilize spent heat to generate electricity and reduce power plant demand.

10. **Water Service.** Water consumption within the project area will increase upon buildout of the proposed project, compared to existing levels. However, ultimate water consumption will not be significantly different than would be expected under existing land use designations, although the demand for increased water service may occur sooner than would be anticipated under the "No Project" scenario.

Since adequate water service will be available to each phase of the proposed project prior to the need for such facilities, and since water demand within the study area upon buildout will be essentially the same as was considered in long-range planning assumptions utilized by the Fontana Water Company, no significant impacts related to water consumption will result.

### Mitigation Measures

The following mitigation measures are proposed to mitigate water service impacts resulting from the proposed project:

- Prior to the recordation of final maps for any phase of the proposed project, a backbone water system capable

## SUMMARY

of providing sufficient fire flows to the portion of the study area shall be installed or assured.

11. **Sewer Service.** Sewage generation will increase upon development of the proposed project, compared to existing levels. However, ultimate sewage generation will not differ significantly from levels which would be expected under the "No Project" scenario, although ultimate generation levels may be achieved sooner than would be expected under the "No Project" buildout schedule. Since required sewer connection fees will be paid and ultimate generation levels will be similar, and since generation levels expected under the "No Project" scenario have been utilized in the planning assumptions made by the Chino Basin Municipal Water District in the long-range sizing and planning of its facilities, no facilities will be required in addition to those already planned, although these facilities may be needed ahead of presently planned schedules.

Since adequate sewage service will be available from existing and presently planned facilities, and since sewage lines will be constructed ahead of need for each individual phase of the proposed project, no significant impact will result.

### Mitigation Measures

The following mitigation measures are proposed to mitigate sewage-related impacts:

- Concurrent with the development of individual phases of the proposed project (and prior to the issuance of the certificate of occupancy), a sewer system sufficient to serve the private improvements under development shall be installed or assured.

12. **Cultural Resources.** Since no identified historical or cultural resources are located within the study area, no impacts to these resources are expected to result from the proposed project.

### Mitigation Measures

The following measure is proposed to mitigate cultural resource impacts of the proposed project:

- If cultural resources are uncovered during grading and construction activities, all work in the area of the find shall cease until a qualified archaeologist is consulted to assess its significance and research

**SUMMARY**

potential. The recommendations of the archaeologist regarding disposition of the find shall be followed.

## SOILS

### A. SOILS

#### i. Environmental Issue

Development within the study area may result in increased erosion and siltation. Potential erosion/siltation impacts resulting from the proposed project will be examined.

#### 2. Existing Setting

According to the Soil Survey of San Bernardino County, South western Part prepared by the United States Department of Agriculture, two soil types are found within the study area. These are Tujunga loamy sand (TuB) and Tujunga gravelly loamy sand (TvC). Figure IV-A-1 illustrates the areas covered by these soil types.

The Soil Survey lists the following characteristics for onsite soil types:

TABLE IV-A-A  
ONSITE SOILS CHARACTERISTICS

Soil Type	Runoff	Erosion Potential
Tujunga loamy sand (TuB)	Slow to very slow.	Hazard of water erosion slight; soil will blow if unprotected; hazard of soil blowing moderate to high on bare soil.
Tujunga gravelly loamy sand (TvC)	Very slow to slow.	Hazard of erosion slight due to gravelly surface layer.

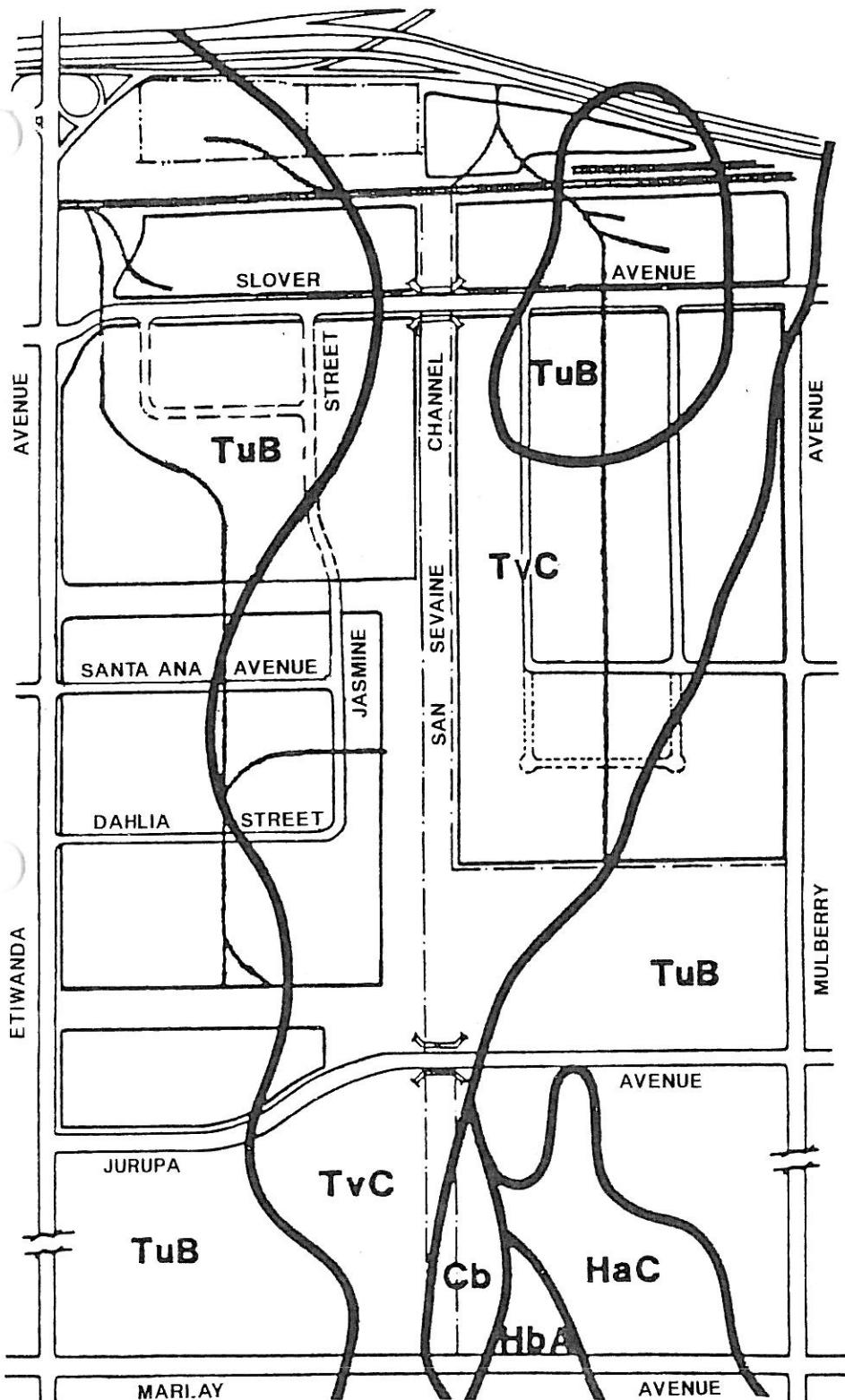
## DETAILED ENVIRONMENTAL ANALYSIS

### IV. DETAILED ENVIRONMENTAL ANALYSIS

This section evaluates the environmental impacts which are expected to result from approval of the proposed project. This analysis discusses those areas of potential impact identified by the Lead Agency, the City of Fontana. Each issue is addressed in the following format:

1. **Environmental Issue:** This portion of each section identifies the issue(s) to be addressed.
2. **Existing Setting:** This is a discussion of the existing environment, physical conditions, and services, as appropriate.
3. **Project Impacts:** This is an evaluation of the impacts of the proposed project in qualitative and quantitative terms as regards the particular environmental factor being addressed.
4. **Mitigation Measures:** This is an identification of the measures incorporated into the proposed project and measures recommended by the environmental consultant to minimize identified environmental effects.
5. **Analysis of Significance:** The final portion of each section is an evaluation of the environmental impacts and the effectiveness of mitigation measures in reducing the degree of environmental impacts.

# SOILS



## NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

FIGURE IV-A-1

CITY OF FONTANA

## FONTANA GATEWAY SPECIFIC PLAN

 NO SCALE

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## SOILS

### 3. Project Impacts

a. **Fugitive Dust Generation.** According to the U.S. Environmental Protection Agency (EPA), clearing, grading, and travel on unpaved portions of a development project are estimated to generate approximately 1.2 tons of fugitive dust per acre of disturbed land for each month of construction activity. This figure translates to a daily dust generation figure of 80 pounds per day per acre. As described in the proposed specific plan, approximately 386 acres -- comprising 306 vacant acres and 80 acres suitable for redevelopment with more intensive uses -- will be subject to construction activities. Based on this figure, the study area would generate approximately 30,880 pounds of dust per day if the entire area to be developed were cleared and graded simultaneously, and if no dust control measures were implemented.

However, actual average dust generation within the study area will be less than the figure noted above, since: 1) the proposed project will be phased, and 2) dust control measures will be implemented during construction of all phases to limit the generation of fugitive dust. These measures are discussed below.

Development of the proposed project will occur in separate phases. Actual daily dust generation will thus be reduced compared to levels which would be expected if the entire site were developed. In addition, each phase will be built incrementally, limiting daily fugitive dust generation but increasing the time period over which dust is generated.

Dust control measures will also be implemented. These include a variety of techniques, such as watering of cleared areas, limiting of amount of cleared area, etc. Significant reductions in the amount of dust generated can be achieved through these methods. According to the EPA (Technical Bulletin AP-42), twice-daily watering of cleared areas can reduce dust generation by approximately 50 percent; chemical stabilization of inactive areas can reduce generation of dust in these areas by approximately 80 percent.

It should be noted, however, that the above discussion is based on average wind conditions. At various times during the year, western San Bernardino County is subjected to high winds from the north to northeast. During such "Santa Ana" wind conditions, actual fugitive dust generation would be significantly higher than normal.

## SOILS

b. Water Erosion. As noted above, onsite soils are characterized by slight erosion hazards (with the exception of Tujunga loamy sand, which has a moderate to high potential for wind erosion if left unprotected). In addition, due to the relatively flat topography of the site, storm flow velocities are expected to be low, further reducing the potential for water erosion. Some dust control measures included in the proposed project, such as planting of cleared areas, will also aid in the prevention of soil erosion.

Thus, erosion of soils within the study area is not expected to be significant. Development of the proposed project may result in a decrease in onsite erosion, through the replacement of existing unimproved drainage swales with buildings, paving, or improved drainage systems.

### 4. Mitigation Measures

The following measures are appropriate to mitigate impacts related to the generation of fugitive dust:

- \* Regular watering of cleared areas.
- \* Establishment of maximum speed limits within construction areas.
- \* Use of soil tackifiers, soil stabilization mulches, and/or oil emulsions, where feasible.

In order to mitigate water erosion, all grading plans submitted to the City of Fontana shall include an erosion control plan for approval by the City Engineer.

### 5. Analysis of Significance

Development of the study area will result in the generation of fugitive dust. However, since dust control measures will be implemented, and since onsite soils are marked by slight erosion hazards and since erosion control plans will be implemented, project-related impacts will be insignificant.

## AIR QUALITY

### B. AIR QUALITY

#### 1. Environmental Issue

The proposed project may result in an increase in the emission of air pollutants within the study area. The potential for air pollutant emissions from construction, mobile, and stationary sources will be analyzed.

#### 2. Existing Setting

Within the South Coast Air Basin, in which the specific plan area is located, topography and meteorology combine to create serious air pollution problems. Temperature inversions are a common occurrence, limiting the vertical dispersion of air pollutants. Restrictive mountain ranges and generally low winds limit the horizontal dispersion of air pollutants. Because the inversion lid will not allow for the dilution of the progressively polluted shallow layer of air, ozone levels in the vicinity of the study area are among the highest in the South Coast Air Basin.

High ozone levels are primarily a result of the chemical reaction of hydrocarbons, nitrogen oxides, and sunlight. The principal sources of these hydrocarbons and nitrogen oxides are far upwind of the study area, allowing sufficient time for photochemical reactions to occur. Thus, the high ozone levels within the study area and vicinity are a regional, rather than purely local, problem.

The primary local air pollutant source in the study area is automobile and truck traffic, although industrial activities also have the potential to impact local air quality through the production of various types of air pollutant emissions. Local air quality levels for Fontana area are shown in Table IV-B-A, below.

# AIR QUALITY

TABLE IV-B-A

LOCAL AIR QUALITY, 1983  
(Source: South Coast Air  
Quality Management District)

Fontana Monitoring Station

Pollutant	Standard	Days Exceeding <sup>1,2</sup>	Maximum Concentration
	Federal > 9.3 PPM 8 Hours	0	10
Carbon Monoxide	Federal > 35 PPM 1 Hour	0	
	State > 9.1 PPM 8 Hours	0	
	State > 20 PPM 1 Hour	0	
Ozone	Federal > .12 PPM 1 Hour	127	0.32
		152	
	State > .10 PPM 1 Hour		
Nitrogen Dioxide	State >= .25 PPM 1 Hour	0	0.16
Sulfur Dioxide	Federal > .14 PPM 24 Hours	0	0.06
	State > .05 PPM 24 Hours	0	

<sup>1</sup> For Suspended Particulate and Sulfate, data indicates number of samples which exceeded standard.  
<sup>2</sup> For Lead, data indicates number of occasions on which standard was exceeded.

## AIR QUALITY

TABLE IV-B-A  
(Continued)LOCAL AIR QUALITY, 1984  
(Source: South Coast Air  
Quality Management District)

Pollutant	Standard	<u>Fontana Monitoring Station</u>	
		Days Exceeding <sup>1,2</sup>	Maximum Concentration
Suspended Particulates (Hi-Vol.)	Federal > 260 ug/m <sup>3</sup>	1	273
	Federal > 150 ug/m <sup>3</sup>	17	
	State > 100 ug/m <sup>3</sup>	33	
Lead (Hi-Vol.)	Federal 1.5 ug/m <sup>3</sup> Quarterly Ave.	0	0.57
	State 1.5 ug/m <sup>3</sup> Monthly Ave.	0	
Sulfate (Hi-Vol.)	State >= 25 ug/m <sup>3</sup> 24 Hours	1	33.1

As noted above, the primary sources of air pollutant emissions generated within the study area result from 1) vehicle trips generated by study area uses, and 2) point source emissions (resulting from onsite usage of electricity and natural gas). Existing air quality impacts resulting from these sources are discussed below.

-----

- <sup>1</sup> For Suspended Particulate and Sulfate, data indicates number of samples which exceeded standard.
- <sup>2</sup> For Lead, data indicates number of occasions on which standard was exceeded.

## AIR QUALITY

a. Existing Point Source Impacts. As noted above, existing point source impacts are primarily related to the onsite consumption of electricity and natural gas. Although a number of industrial uses presently operate onsite, no significant air quality impacts are known to be generated, due to the light manufacturing nature of these uses.

Existing stationary source impacts from study area development are primarily related to the approximately 30,452 kilowatt hours (Kwh) of electricity and 165,465 cubic feet (cf) of natural gas presently consumed within the site (see Section IV-I, Energy Resources, for further discussion of existing energy consumption). Based on emissions factors contained in the Air Quality Handbook for Environmental Impact Reports published by the South Coast Air Quality Management District (SCAQMD), existing air quality impacts resulting from electrical consumption are as follows:

TABLE IV-B-B

EXISTING ELECTRICAL CONSUMPTION  
AIR QUALITY IMPACTS: POUNDS PER DAY  
(Source: SCAQMD Handbook)

Kwh Used	Emissions				
	CO	NO <sub>x</sub>	SO <sub>x</sub>	Part	ROG
30,452	6.4	64	42.6	5.48	3.96

-----  
CO --- Carbon Monoxide  
NO<sub>x</sub> -- Nitrogen Oxides  
SO<sub>x</sub> -- Sulfur Oxides  
Part - Particulates  
ROG -- Reactive Organic Gases  
-----

It should be noted that the emissions factors utilized in Table IV-B-B reflect 1979 emissions inventories. Assuming that control techniques have been upgraded, 1986 emissions can be expected to be lower, due to utilization of emissions controls technology not available in 1979.

Utilizing emissions factors contained in the SCAQMD Handbook, the existing emissions from natural gas consumption were estimated, and are shown in Table IV-B-C.

## AIR QUALITY

TABLE IV-B-C

EXISTING NATURAL GAS CONSUMPTION  
AIR QUALITY IMPACTS: POUNDS PER DAY  
(Source: SCAQMD Handbook)

Consumption	Emissions				
	CO	NO <sub>x</sub>	SO <sub>x</sub>	Part	HC
165,465 cf	3.15	18.89	Negl.	0.0236	1.26

cf -- Cubic Feet

CO -- Carbon Monoxide

NO<sub>x</sub> - Nitrogen Oxides

SO<sub>x</sub> - Sulfur Oxides

Part - Particulate

HC -- Hydrocarbons (CH<sub>4</sub>)

b. Existing Mobile Source Emissions. As stated above, existing mobile source emissions are primarily related to the approximately 14,392 daily trips generated by study area uses.

To determine air quality impacts resulting from existing trips, an average trip length of 9.6 miles (as indicated in LARTS information) was utilized; emissions were calculated for an average vehicle speed of 30 mph with emissions factors determined by the Caltrans EMFAC6D computer model. Resulting existing mobile source-related emissions are shown in Table IV-B-D, below.

## AIR QUALITY

TABLE IV-B-D

EXISTING MOBILE SOURCE AIR POLLUTANT EMISSIONS  
(POUNDS PER DAY)  
(Source: SCAQMD Handbook)

Daily Miles	Emissions				
	CO	THC	NO <sub>x</sub>	SO <sub>x</sub>	Part
138,163	646	65	88	15	20

CO --- Carbon Monoxide  
THC -- Total Hydrocarbons  
NO<sub>x</sub> -- Nitrogen Oxides  
SO<sub>x</sub> -- Sulfur Oxides  
Part - Particulate

### 3. Project Impacts

Air quality impacts resulting from the proposed specific plan will be generated in three major categories: 1) construction impacts, 2) mobile sources emissions (vehicular traffic) and 3) stationary source emissions (use of natural gas and generation of electricity consumed within the site). The impacts associated with these sources are discussed below.

a. **Construction-Related Impacts.** In addition to the generation of fugitive dust (discussed in the preceding Soils section), air pollutant emissions will result from the use of construction equipment -- including vehicles and stationary equipment such as cement mixers -- during development and re-development of the specific plan area. Due to expected variations in the level of construction activity, emissions resulting from these sources are difficult to quantify. However, these impacts can be expected to consist primarily of low levels of carbon monoxide (CO), nitrogen oxides (NO<sub>x</sub>), and hydrocarbons.

## AIR QUALITY

b. Point Source Impacts. Point source impacts resulting from the proposed project were determined utilizing electricity and natural gas consumption projections (see Section IV-G) and emissions factors contained in the SCAQMD Handbook. Resulting emissions projections are presented below.

TABLE IV-B-E

PROJECTED ELECTRICAL CONSUMPTION  
AIR QUALITY IMPACTS: POUNDS PER DAY  
(Source: SCAQMD Handbook)

Consumption	Emissions				
	CO	NO <sub>x</sub>	SO <sub>x</sub>	Part	ROG
75,717 Kwh	15.9	159	106	13.6	9.8

CO --- Carbon Monoxide  
NO<sub>x</sub> -- Nitrogen Oxides  
SO<sub>x</sub> -- Sulfur Oxides  
Part - Particulates  
ROG -- Reactive Organic Gases

TABLE IV-B-F

PROJECTED NATURAL GAS CONSUMPTION  
AIR QUALITY IMPACTS: POUNDS PER DAY  
(Source: SCAQMD Handbook)

Consumption	Emissions				
	CO	NO <sub>x</sub>	SO <sub>x</sub>	Part	HC
411,410 cf	8.23	49.3	Negl.	0.06	3.29

cf -- Cubic Feet  
CO -- Carbon Monoxide  
NO<sub>x</sub> - Nitrogen Oxides  
SO<sub>x</sub> - Sulfur Oxides  
Part - Particulate  
HC -- Hydrocarbons (CH<sub>4</sub>)

## AIR QUALITY

As shown in Tables IV-B-E and IV-B-F, air quality impacts will increase as the result of development of the proposed project.

It should be noted that, as discussed in the Project Description, the proposed project will contain "medium manufacturing" uses, which may include, but not be limited to manufacturing, compounding of materials, processing, assembly, packaging, treatment or fabrication of materials and products. These uses can be expected to generate air pollutant emissions in addition to those listed above. However, the number of medium manufacturing uses, and the exact types of uses in this category, is unavailable, and cannot be reasonably estimated at this time, making quantification of emissions from these land uses impossible. However, all future uses which generate air pollutant emissions will be subject to individual review by the SCAQMD, and will be subject to emissions limitation which may be required by that agency.

c. Mobile Source Impacts. Mobile source impacts resulting from the proposed project are related to the 61,100 daily trips expected to be generated by onsite uses (see Section IV-G, Circulation, for further discussion of traffic impacts). Applying the 9.6-mile average trip distance noted above, and utilizing emissions factors contained in the SCAQMD Handbook, the following emissions impact projections were developed.

TABLE IV-B-8

MOBILE SOURCE IMPACTS: POUNDS PER DAY  
(Source: SCAQMD Handbook)

Average Daily Miles	Emissions				
	CO	THC	NO <sub>x</sub>	SO <sub>x</sub>	Part
586,560	2,742	276	375	64	85

CO --- Carbon Monoxide  
THC -- Total Hydrocarbons  
NO<sub>x</sub> -- Nitrogen Oxides  
SO<sub>x</sub> -- Sulfur Oxides  
Part - Particulate

## AIR QUALITY

As shown in Table IV-B-G, mobile source impacts will increase compared to existing levels. However, as noted in the Project Description portion of this document, the proposed project will not differ significantly from the "No Project" scenario, for which mitigation measures have been included in the City of Fontana General Plan (although, as noted, ultimate trip generation may be achieved sooner than anticipated in the General Plan). Therefore, no significant impacts related to mobile source impacts are expected.

### 4. Mitigation Measures

The following measures are proposed to mitigate air quality-related impacts resulting from the proposed project:

#### Point Source Impacts

Mitigation of point source impacts is primarily related to reduction of the amount of electricity and natural gas consumed onsite. Measures to reduce onsite consumption of these resources are included in the "Energy Resources" section of this document.

In addition, the following mitigation measure is proposed:

- All uses which have the potential to generate air pollutant emissions shall be required to construct such control measures as may be required by the South Coast Air Quality Management District.

#### Mobile Source Impacts

Mitigation of mobile source impacts is related to the reduction of vehicle trips and miles driven. Measures to reduce vehicle travel generated by the proposed project are contained in the "Circulation" section of this document.

#### Point and Mobile Source Impacts Related to Construction

- Controlling fugitive dust by regular watering, paving construction roads or other dust preventive measures in order to comply with District Rule 403.
- Maintaining equipment engines in proper tune.

## AIR QUALITY

- Using water trucks or sprinkler systems to keep all areas of vehicle movement damp enough to prevent dust raised when leaving the site.
- Wetting down areas in the late morning and after work is completed for the day.
- Phasing and scheduling construction activities to level emission peaks.
- Discontinuing construction activities during first and second stage smog alerts.

### 5. Analysis of Significance

Development of the proposed project will create construction source impacts, and will result in an increase in point source and mobile source impacts, compared to present levels. However, as discussed in the "Project Description" portion of this document, land uses which will result under the proposed project are not significantly different than those which would be expected under the "No Project" scenario. Land uses contained in the "No Project" scenario have been utilized in planning assumptions made by the City of Fontana, and are thus included in mitigation measures contained in the City of Fontana General Plan, as well as in the San Bernardino County General Plan and the South Coast Regional Air Quality Management Plan. While there exists an uncertainty regarding point source impacts which might be generated by future medium manufacturing uses, implementation of emissions control measures as required by the SCAQMD will reduce such impacts to a level of insignificance.

## DRAINAGE/FLOODING

### C. DRAINAGE/FLOODING

#### 1. Environmental Issue

Development within the study area may result in increased storm flows, due to the replacement of existing pervious areas with impervious surfaces. By increasing storm flows within the project site, downstream properties could be impacted. This section analyzes the amount of water runoff which can be expected to result from project implementation, and identifies the facilities required to safely accommodate project site runoff.

#### 2. Existing Setting

The study area is traversed in a north-south direction by the San Sevaine Channel, a rip-rap reinforced open channel. In addition to the San Sevaine Channel, major onsite storm drainage facilities include the Jurupa Basin, an interim storm flow storage facility with a capacity of 1,300 acre-feet. North of the Jurupa Basin, the San Sevaine Channel has an ultimate design capacity of 18,850 cubic feet per second (cfs). South of the basin, the channel has an ultimate capacity of 12,100 cfs. The basin is designed as a bypass system, with a proposed over-side spillway designed to remove peak flows from the San Sevaine Channel. The basin is planned to be provided with a spillway to direct excess flows back to the channel.

The San Sevaine Channel is crossed by the Santa Fe railway and by Slover Avenue. Slover Avenue is presently provided with a low-flow crossing which can become inundated by normal rainfall occurring over a period of several days.

Minor onsite storm drainage facilities consist of several sections of reinforced concrete pipe culverts utilized to deviate portions of the western half of the study area. The locations of these culverts are shown in the hydrology report on the study area prepared by the Project Engineer, which is included in the appendices to this report.

In addition to onsite storm drainage facilities is the lower Etiwanda Channel, located west of Etiwanda Avenue within the City of Ontario. This facility presently has an ultimate design capacity of 3,500 cfs.

According to the Project Engineer, approximately 629 acres of the study area contribute storm flows to the regional storm

## DRAINAGE/FLOODING

drain system; this area is projected to generate storm flows of 814 cfs during a 100-year storm (each storm season has a one percent chance of containing a storm of a "100-year" severity).

The City of Fontana designates a small portion of the site near the northern entry of the San Sevaine Channel as a "Flood Hazard Area".

### 3. Project Impacts

As part of the report prepared by the Project Engineer, storm flow projections for the study area at development were prepared. Based on this analysis, it was determined that storm flows during a 100-year storm would increase by 306 cfs to an ultimate flow of 1,120 cfs.

Based on projected storm flows, and taking into account onsite topography, an onsite storm drainage system was developed by the Project Engineer; the resulting system is illustrated in the Project Engineer's report, which is included in the appendices to this report. As noted in the Project Engineer's report, one component of the ultimate storm drainage system -- new lines which will be utilized to devate Etivanda Avenue -- will require negotiations between the cities of Fontana and Ontario, since these lines will connect to the lower Etivanda Channel.

### 4. Mitigation Measures

The following measures are proposed to mitigate impacts related to drainage:

- Concurrent with the development of individual phases of the proposed project (and prior to issuance of the certificate of occupancy), a storm drainage system sufficient to serve the private improvements under development shall be installed or assured.
- The City of Fontana shall initiate negotiations with the City of Ontario for the construction of storm drainage connections to the lower Etivanda Channel.

## DRAINAGE/FLOODING

### 5. Analysis of Significance

Storm flows generated within the study area will increase upon development of the proposed project. However, construction of storm drainage facilities as outlined in the Project Engineer's report (included in the appendices to this report) will mitigate drainage-related impacts to a level of insignificance.

## BIOTIC RESOURCES

### D. BIOTIC RESOURCES

#### 1. Environmental Issue

The impacts of the introduction of new plant species into the study area, and the potential for deterioration of existing fish and wildlife habitat, if any, will be examined.

#### 2. Existing Setting

Although several significant biotic associations are found within the city's planning area, no rare or endangered species are located with the city or its sphere of influence, according to the City of Fontana General Plan.

Since the study area has been primarily developed for industrial uses, existing biology within the site can be expected to consist entirely of introduced landscaping and naturalized species, such as weeds and forbs. However, landscaping introduced as part of area industrial development has been limited. In addition, a small number of eucalyptus trees are located along a minor drainage swale in the northeastern corner of the site.

The vegetative species within the study area would support several rodent species; a small number of birds common to the area would be supported within the eucalyptus trees. A 1983 survey of the San Sevaine Channel Jurupa Basin, and adjacent areas, conducted as part of flood control studies for the area confirms these conclusions.

#### 3. Project Impacts

Development of the proposed project will include the replacement of existing introduced species with other introduced species, as part of the proposed landscaping of the site. Although a number of existing open areas will be cleared and replaced with buildings and paved areas, no significant impacts will occur, due to the marginal nature of existing onsite biology.

Although the trees located in the northeastern corner of the site will be removed as development of the study area occurs, new trees will be planted as replacements as part of the proposed

## BIOTIC RESOURCES

landscaping of the study area. The result will be an overall increase in the number of trees within the study area.

### 4. Mitigation Measures

No mitigation measures for impacts to area biotic resources are proposed. However, as noted above, the proposed project includes a landscaping plan which will involve the planting of new grasses, shrubs and trees within the site.

### 5. Analysis of Significance

Since no significant biology presently exists within the study area, and since the proposed project includes landscaping of the site, no significant impacts to biology are expected to result.

## NOISE

### E. NOISE

#### 1. Environmental Issue

The proposed project will result in increases in the noise levels within the study area, and may result in an increase in the number of persons subjected to severe noise levels. Potential noise levels within the study area will be evaluated.

#### 2. Existing Setting

The study area is subject to noise from three major sources -- onsite industrial activities, railway traffic, and traffic on vicinity roadways. Due to the relatively limited industrial activity within the site, noise generated by this source is presently relatively minor.

Railway noise generated along the Southern Pacific Railway line, located adjacent to the northern study area boundary, generates CNEL (Community Equivalent Noise Level) levels of up to approximately 80 dB, according to the City of Fontana General Plan.

To determine existing noise generated by traffic on vicinity roadways, noise calculations contained in California Noise Insulation Standards, October 1974, by J.J. Van Houten and Associates, were utilized. Resulting distances to existing CNEL (Community Equivalent Noise Level) contours are shown in Table IV-E-A, below.

## NOISE

TABLE IV-E-A

**DISTANCES TO EXISTING NOISE CONTOURS<sup>1</sup>**  
 (Source: California Noise Insulation Standards)

Roadway Link	Distance to 55 CNEL Contour	Distance to 60 CNEL Contour	Distance to 65 CNEL Contour
Slover Avenue west of San Sevaine Channel	150 feet	80 feet	48 feet
Slover Avenue east of San Sevaine Channel	120 feet	65 feet	35 feet
Etiwanda Avenue south of Slover Avenue	280 feet	145 feet	80 feet
Etiwanda Avenue south of Slover Avenue	280 feet	145 feet	80 feet
Jurupa Avenue east of Etiwanda Avenue	W/R	W/R	W/R
Jurupa Avenue west of Mulberry Avenue <sup>2</sup>	---	---	---
Mulberry Avenue south of Santa Ana Avenue	120 feet	64 feet	36 feet
Mulberry Avenue north of Santa Ana Avenue	125 feet	70 feet	40 feet

<sup>1</sup> Distances to noise contours based on existing (1986) traffic levels, as contained in Project Engineer's report (see appendices to this document). Figures assume average vehicle speed of 30 mph and truck-to-auto mix of three percent. Distance measured from near lane centerline.

<sup>2</sup> Roadway not yet built; no traffic

W/R -- Indicates contours fall within roadway width

## NOISE

### 3. Project Impacts

Onsite noise will increase as the result of development of regional and onsite traffic-generating uses. To determine ultimate noise conditions, the calculations utilized in Table IV-E-A were applied to ultimate (1997) traffic conditions; results are shown below in Table IV-E-B. It should be noted that project-related traffic will comprise a relatively small portion of traffic on perimeter roadways -- generally between 13 and 35 percent -- and will thus contribute a similarly small portion of ultimate traffic-generated noise.

In addition to long-term noise sources, construction activities related to onsite development -- including equipment operation and construction-related vehicle traffic -- will also generate noise, although these impacts can be expected to be limited to the study area.

## NOISE

TABLE IV-E-B

**DISTANCES TO PROJECTED NOISE CONTOURS<sup>1</sup>**  
 (Source: California Noise Insulation Standards)

Roadway Link	Distance to 55 CNEL Contour	Distance to 60 CNEL Contour	Distance to 65 CNEL Contour
Slover Avenue west of San Sevaine Channel	600 feet	320 feet	170 feet
Slover Avenue east of San Sevaine Channel	400 feet	220 feet	115 feet
Etivanda Avenue south of Slover Avenue	760 feet	410 feet	215 feet
Etivanda Avenue south of Santa Ana Avenue	700 feet	375 feet	200 feet
Jurupa Avenue east of Etivanda Avenue	490 feet	270 feet	140 feet
Jurupa Avenue west of Mulberry Avenue	410 feet	220 feet	120 feet
Mulberry Avenue south of Santa Ana Avenue	400 feet	215 feet	115 feet
Mulberry Avenue north of Santa Ana Avenue	370 feet	200 feet	108 feet

<sup>1</sup> Distances to noise contours based on projected (1986) traffic levels, as contained in Project Engineer's report (see appendices to this document). Figures assume average vehicle speed of 30 mph and truck-to-auto mix of three percent. Distances measured from near lane centerline.

## NOISE

As shown in Table IV-E-B, significant portions of the study area will be subject to exterior CNEL noise levels in excess of 65 dB at ultimate traffic conditions. However, due to the proposed industrial use of the study area, no significant impacts are expected to result, although residential areas to the southeast of the site may be subjected to increased noise levels. It should be noted that these residential areas were designed with ultimate traffic volumes in mind, and have been provided with noise attenuation (in the form of sound walls) sufficient to provide acceptable exterior CNEL levels. Development of the proposed project will not result in significant changes in traffic levels (see Circulation section of this document) and will thus not increase projected noise levels or require installation of additional noise attenuation measures for adjacent residential areas.

Noise produced by industrial uses within the study area will also increase, due to the increased intensity of use. However, since uses along the easterly project boundary will be limited to light industrial development, noise generated by these sources is not expected to impact offsite areas; thus, no impact will result.

Noise generated by traffic on the Southern Pacific Railway line, and any expansion of the same, is not expected to increase as the result of development of the proposed project. Development of more intensive uses in the specific plan area adjacent to the railway line will result in attenuation of railway noise through the introduction of additional buildings in this area.

### 4. Mitigation Measures

The following measures are proposed to mitigate noise-related impacts resulting from development of the proposed project:

#### Construction-Related Noise

- To the extent possible, regular construction activity shall be limited to the weekday hours of 7:00 a.m. to 7:00 p.m. or pursuant to Section 17-3(i) of the Fontana Municipal Code.

#### Long-Term Noise

- Interior noise levels within office portions of study area buildings shall be no higher than a CNEL of 50 dB

## NOISE

within general offices and 45 dB within private offices. Where necessary, noise attenuation (such as insulation or distance) will be used to shield office areas from adjacent noise sources.

- Interior noise levels within retail structures shall be no higher than 60 dB CNEL.
- A combination of walls, berms, and building placement shall be utilized to ensure that noise generated by onsite industrial uses does not impact offsite residential areas.

### 5. Analysis of Significance

Onsite and offsite noise levels will increase as the result of development of the proposed project. However, as noted above, development of the proposed project will not result in increases in noise levels above those which would be expected if the site were developed under the "No Project" scenario. In addition, the industrial uses proposed within the site are not noise-sensitive, and no onsite impacts are expected to result. Thus, the proposed project will not result in significant impacts.

## SAFETY/NUISANCE

### F. SAFETY/NUISANCE

#### 1. Environmental Issue

Development of the proposed project may result in the production of objectionable odors, and may result in an increase in the potential for the release of hazardous substances within the study area.

#### 2. Existing Setting

The City of Fontana General Plan does not identify the study area as a specific location in which hazardous materials are used or stored. However, due to the presence of industrial uses which may utilize hazardous materials, it can be expected that such substances may be present.

According to the City of Fontana General Plan, no approved hauling routes or stopping sites for vehicles carrying hazardous or explosive materials are located within the study area. The San Bernardino Freeway, located north of the study area, is an approved haul route for vehicles carrying hazardous or explosive materials.

Local regulation of the storage and use of hazardous materials within the study area is the responsibility of the Central Valley Fire District (CVFD), which maintains records of hazardous materials used within its service area (i.e. type of quantity of material, location, type of process, etc.). Any use or storage of hazardous materials within the study area will be subject to review and regulation by the CVFD.

#### 3. Project Impacts

Development of the proposed project will result in an increase in the number of industrial uses within the study area, including uses which may utilize hazardous materials as part of manufacturing or assembly processes. By increasing the number of potential users of hazardous materials within the project site, the potential for an accident involved the transport, use, or storage of such materials will also be increased. It is expected, however, that the level of risk which will be associated with study area development will be similar to that which is typical of modern industrial development.

## SAFETY/HUISANCE

In addition, petroleum products storage -- including the bulk storage, sale, and distribution of gasoline, liquefied petroleum gas, and other petroleum products -- will be permitted within the study area as a conditional use. Storage, manufacture, distribution or other use of explosive materials within the study area (apart from such petroleum materials as may be considered explosive due to their combustion characteristics) is not proposed.

Finally, it is possible that future industrial uses could produce objectionable odors. Because the study area is primarily surrounded by industrial uses, it is not expected that significant impacts would result. However, as with point air pollutant emissions, industrial processes which have the potential for creating objectionable odors will be reviewed by the South Coast Air Quality Management District, and appropriate measures will be enforced at that time.

### 4. Mitigation Measures

The following measure is proposed to mitigate impacts related to storage and use of hazardous materials:

- All individual uses within the study area which utilize hazardous materials shall be required to install all storage and transport systems in accordance with applicable safety standards, and to file records of the type, quantity, storage method, and utilization of all hazardous materials with the Central Valley Fire District. All individual uses shall also meet all other requirements of the City of Fontana which may be required as part of a conditional use permit.

### 5. Analysis of Significance

Development of the study area may increase the potential for exposure of persons to hazardous materials or to accidents involving such materials. However, storage and use of such materials within the study area will be subject to applicable safety standards and to review by the Central Valley Fire District, thus reducing any expected impacts to a level of insignificance. In addition, it is possible that industrial processes which will be located within the study area will produce objectionable odors. However, the implementation of the SCAQMD permitting process will assure that such potential impacts are reduced to a level of insignificance.

## CIRCULATION

### G. CIRCULATION

#### 1. Environmental Issue

Implementation of the proposed project will increase traffic volumes within the study area and on surrounding roadways. The impacts of the proposed project on the local circulation system, including the need for roadway improvements, if any, will be examined.

#### 2. Existing Setting

As part of the preparation of this report, a traffic study for the proposed project was prepared by the Project Engineer, Krueper Engineering and Associates, Inc. The complete text of that report is included in the appendices to this document.

a. Existing Roadway System. The study area is presently served by the following major roadways:

**San Bernardino Freeway (Interstate 10)** This is an eight-lane facility which serves as a major east-west transportation corridor between the Los Angeles area and the "Inland Empire", including San Bernardino County. Access to the I-10 freeway, which is located approximately 0.3 mile north of the study area, is provided via a full interchange at Etiwanda Avenue. Existing (1986) average daily volume on the I-10 freeway is approximately 90,000 - 100,000 vehicles.<sup>1</sup>

**Devore 15 Freeway (Interstate 15)** This major, eight-lane facility is located approximately 1.5 miles west of the study area. Upon completion of improvements presently under construction, this facility will provide access both to the north (the desert areas of Barstow, Las Vegas, and beyond) and the south (San Diego). Access to the I-15 freeway is presently available from the study area via the I-10 freeway. An interchange has

---

<sup>1</sup> Existing (1985) traffic volumes were obtained from 1985 State of California Traffic Volumes, San Bernardino County Transportation Department, City of Fontana, City of Ontario, and June 1983 traffic study prepared by Krueper Engineering. Where necessary, a six percent straight line projection was made to determine 1986 traffic volumes.

## CIRCULATION

been constructed at Jurupa Avenue which will ultimately provide more direct access to the I-15 freeway; however, utilization of this route will require construction of Jurupa Avenue between the study area and the interchange, including a crossing over the San Sevaine Channel. Existing (1985) daily traffic volume on the I-15 freeway is approximately 50,000 - 55,000 vehicles.

**Pomona Freeway (State Route 60)** This is a major six-lane facility located approximately 2.0 miles south of the study area. Access to this facility is available via the Riverside Avenue/Etivanda Avenue" interchange. Existing (1985) daily traffic volume on the Pomona freeway is approximately 46,000 - 49,500 vehicles.

**Etivanda Avenue** This roadway presently exists in a variety of cross-sections between the I-10 freeway and Jurupa Avenue, ranging between 40 and 65 feet in width. This roadway is ultimately planned as a major highway, with a right-of-way width of 120 feet. Existing (1986) daily traffic levels on this roadway consist of approximately 9,900 vehicles north of Slover Avenue, 8,400 vehicles north of Jurupa Avenue, and 6,700 vehicles south of Jurupa Avenue.

**Slover Avenue** This roadway presently exists in a variety of widths between 26 and 46 feet, providing one travel lane in each direction. Ultimate planned right-of-way width for this roadway is 100 feet. Existing (1986) daily traffic volumes on this roadway range between 3,800 vehicles near Etivanda Avenue and 3,300 vehicles near Mulberry Avenue.

**Mulberry Avenue** This roadway presently exists in a variety of configurations: south of Slover Avenue, Mulberry Avenue consists of a two-lane roadway with an average pavement width of 26 feet, although some minor improvements have taken place between Slover and Jurupa avenues; south of Jurupa Avenue, Mulberry exists as a three-lane roadway (two northbound and one southbound travel lanes) with an average pavement width of 48 feet. Mulberry Avenue is planned for an ultimate right-of-way width of 100 feet. Existing (1986) daily traffic volumes on this roadway range between 3,000 and 4,100 vehicles.

**Jurupa Avenue** This roadway presently exists only in parts of its ultimate route: between Etivanda Avenue and a point approximately 400 feet west of the San Sevaine channel, this roadway has been fully improved to an 80-foot

## CIRCULATION

pavement width; from the point 400 feet west of the San Sevaine Channel to Mulberry Avenue, this roadway does not exist; east of Mulberry Avenue, Jurupa Avenue has been partially constructed to a pavement width of 46 feet. Master plans for the entire length of Jurupa Avenue indicate a right-of-way width of 100 feet, and include an all-weather crossing of the San Sevaine Channel. Existing (1986) daily traffic on Jurupa Avenue consists of approximately 200 vehicles in the western portion and 1,900 vehicles in the eastern portion.

In addition to the roadways described above, a number of internal roadways have been constructed within the study area; these have all been constructed to their ultimate planned widths (see the Project Engineer's report for discussion of interior street widths). Several additions will be made to the interior roadway system as part of development of the proposed project; ultimate roadway widths and alignments are contained in the Project Engineer's report.

b. Existing Roadway Utilization. As part of the Project Engineer's report, existing roadway capacity and utilization for study area vicinity roadways was determined. As shown in Table IV-G-A, all study area vicinity roadways are presently operating well within their capacities at present widths and traffic levels. With the exception of Etiwanda Avenue south of Slover Avenue, all roadways are presently operating at Level of Service (LOS) A, indicating optimum traffic conditions.

## CIRCULATION

TABLE IV-G-A

**EXISTING ROADWAY CAPACITY AND UTILIZATION**  
(Source: Krueper Engineering)

Roadway	Existing Capacity <sup>1</sup>	Existing Traffic (ADT)	Utilization/ <sup>2</sup> Level of Service
<b><u>Etiwanda Avenue</u></b>			
North of Slover	45,000	9,900	0.22/A
South of Slover	18,000	8,400	0.47/B
South of Santa Ana	18,000	8,400	0.47/B
South of Jurupa	18,000	8,400	0.47/B
<b><u>Slover Avenue</u></b>			
East of Etiwanda	18,000	3,800	0.21/A
West of Mulberry	18,000	3,300	0.18/A
East of Mulberry	18,000	2,500	0.14/A
<b><u>Mulberry Avenue</u></b>			
South of Slover	18,000	3,300	0.18/A
South of Santa Ana	18,000	3,000	0.17/A
South of Jurupa	18,000	4,100	0.23/A
<b><u>Jurupa Avenue</u></b>			
West of Etiwanda	N/B	N/B	N/B
East of Etiwanda	38,000	200	0.01/A
West of Mulberry	N/B	N/B	N/B
East of Mulberry	30,000	1,900	0.06/A

<sup>1</sup> "Existing capacity" = Ultimate design capacity.

<sup>2</sup> Utilization = Existing ADT/Capacity.

N/B -- Indicates roadway section not built.

## CIRCULATION

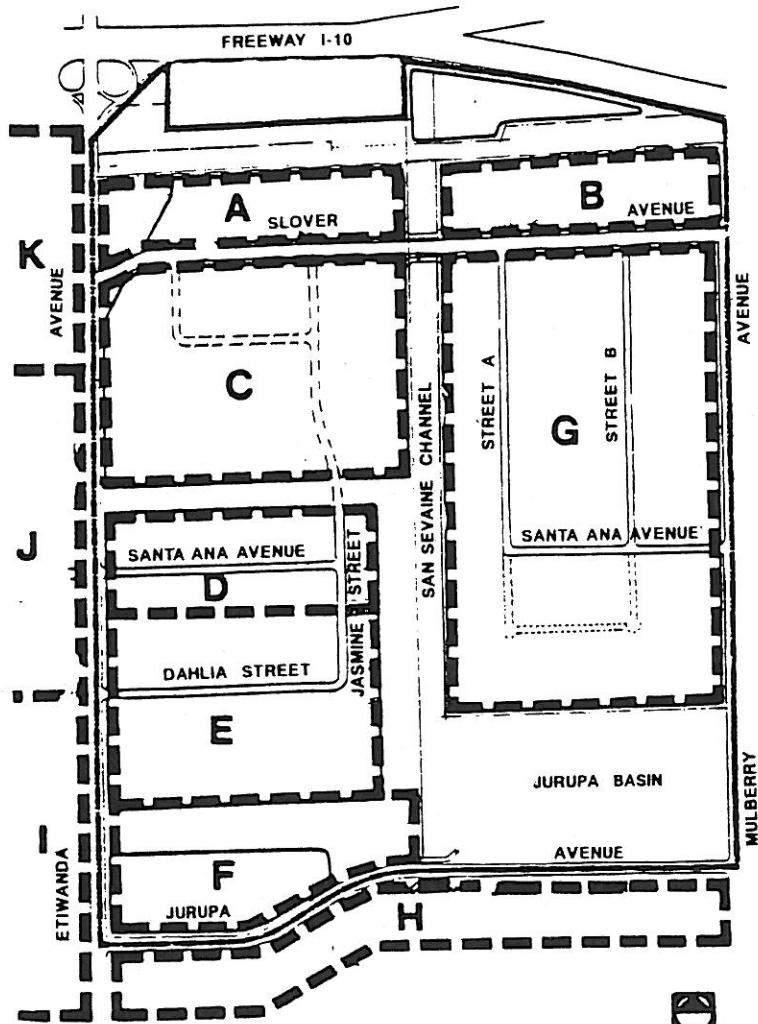
### 3. Project Impacts

Traffic on local roadways will increase as the result of development of the proposed project, resulting in impacts to the local roadway system. Impacts of the proposed project, as determined by the Project Engineer, are discussed below.

a. **Trip Generation.** To determine project trip generation for the proposed project, a trip generation rate of 56 vehicle trips per day per acre was utilized, as contained in Trip Generation, An Informational Report, Third Edition, published by the Institute of Transportation Engineers. This figure was then applied to 11 subareas within the project site, as shown in Figure IVG1, on the following page. Resulting projected trip generation for each subarea, based on this generation rate, is shown in Table IV-G-B.

Because of the negligible amount of additional rail traffic which will result from the expansion of rail service to the proposed project, no significant impacts are expected to result from the expansion of rail service as contemplated in the Specific Plan

## SPECIFIC PLAN AREA



AREA	ACREAGE	24-HOUR VOLUME	PEAK HOUR
A	39	2200	220
B	36	2000	200
C	110	6200	620
D	57	3200	320
E	84	4700	470
F	53	3000	300
G	218	12,200	1,220
H	42	2400	240
I	150	8400	840
J	150	8400	840
K	150	8400	840

NO SCALE

NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

FIGURE IV-G-1

# CITY OF FONTANA FONTANA GATEWAY · SPECIFIC PLAN

WILLIAMS · KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## CIRCULATION

TABLE IV-G-B

**PROJECTED TRIP GENERATION**  
(Source: Krueper Engineering)

Sub-Area <sup>1</sup>	Acres	Average Daily Trips	Peak Hour Traffic <sup>2</sup>
A	39	2,200	220
B	36	2,000	200
C	110	6,200	620
D	57	3,200	320
E	84	4,700	470
F	53	3,000	300
G	218	12,200	1,200
H	42	2,400	240
I	150	8,400	840
J	150	8,400	840
K	150	8,400	840
<hr/>			
Total... 61,100			

b. **Roadway Impacts.** To determine the impacts of the proposed project, based on the average daily trip generation indicated in Table IV-G-B, analyses of roadway capacity vs. ultimate traffic were prepared for existing and planned roadway widths. The results of these analyses are indicated in Tables IV-G-C and IV-G-D, below.

<sup>1</sup> For identification of Sub-Areas, see Figure IVG1.

<sup>2</sup> Peak hour traffic based on ten percent of total projected ADT.

## CIRCULATION

**TABLE IV-G-C**  
**TRAFFIC IMPACT ANALYSIS: EXISTING ROADWAYS**  
 (Data Source: Krueper Engineering)

Roadway	Design Capacity	W/O PROJECT <sup>1</sup>		WITH PROJECT	
		Daily Traffic (ADT)	Projected Traffic Vs. Capacity	Daily Traffic (ADT)	Projected Traffic Vs. Capacity
<b><u>Etiwanda Avenue</u></b>					
North of Slover	45,000	24,600	0.55	43,400	0.96
South of Slover	18,000	25,100	1.39	36,100	2.00
South of Santa Ana	18,000	23,900	1.33	31,800	1.77
South of Jurupa	18,000	22,900	1.27	28,600	1.59
<b><u>Slover Avenue</u></b>					
East of Etiwanda	18,000	10,800	0.60	25,500	1.42
West of Mulberry	18,000	9,400	0.52	14,400	0.80
East of Mulberry	18,000	10,000	0.55	13,600	0.75
<b><u>Mulberry Avenue</u></b>					
South of Slover	18,000	9,400	0.52	12,900	0.72
South of Santa Ana	18,000	9,600	0.53	14,500	0.81
South of Jurupa	18,000	14,600	0.81	16,700	0.93
<b><u>Jurupa Avenue</u></b>					
West of Etiwanda	N/B	18,000 <sup>2</sup>	N/A	24,100 <sup>2</sup>	N/A
East of Etiwanda	38,000	10,000	0.26	19,500	0.51
West of Mulberry	N/B	9,500 <sup>2</sup>	N/A	14,700 <sup>2</sup>	N/A
East of Mulberry	30,000	16,900	0.56	18,100	0.60

<sup>1</sup> "Without Project" traffic includes future non-project traffic generation.  
<sup>1</sup> Traffic assumes construction of this roadway link; due to unbuilt status, these trips would be reoriented to other roadway links.

## CIRCULATION

**TABLE IV-G-D**  
**TRAFFIC IMPACT ANALYSIS: ULTIMATE ROADWAYS**  
**(Source: Krueper Engineering)**

Roadway	W/O PROJECT <sup>1</sup>			WITH PROJECT	
	Ultimate Design Capacity	Daily Traffic (ADT)	Projected Traffic Vs. Capacity	Daily Traffic (ADT)	Projected Traffic Vs. Capacity
<b><u>Etiwanda Avenue</u></b>					
North of Slover	45,000	24,600	0.55	43,400	0.96
South of Slover	75,000	25,100	0.33	36,100	0.48
South of Santa Ana	75,000	23,900	0.32	31,800	0.43
South of Jurupa	75,000	22,900	0.31	28,600	0.38
<b><u>Slover Avenue</u></b>					
East of Etiwanda	38,000	10,800	0.28	25,500	0.67
West of Mulberry	38,000	9,400	0.25	14,400	0.38
East of Mulberry	38,000	10,000	0.26	13,600	0.36
<b><u>Mulberry Avenue</u></b>					
South of Slover	30,000	9,400	0.31	12,900	0.43
South of Santa Ana	30,000	9,600	0.32	14,500	0.48
South of Jurupa	30,000	14,600	0.49	16,700	0.56
<b><u>Jurupa Avenue</u></b>					
West of Etiwanda	75,000	18,000	0.24	24,100	0.32
East of Etiwanda	38,000	10,000	0.26	19,500	0.51
West of Mulberry	38,000	9,500	0.25	14,700	0.39
East of Mulberry	38,000	16,900	0.44	18,100	0.48

<sup>1</sup> "Without Project" traffic includes future non-project traffic generation.

## CIRCULATION

As shown in Table IV-G-C, capacity will be exceeded on portions of Etiwanda Avenue -- south of Slover Avenue, south of Santa Ana, and south of Jurupa -- at existing roadway widths without development of the proposed project. Addition of project-related traffic to the existing local roadway system would result in capacity being exceeded on only one additional roadway link -- Slover Avenue east of Etiwanda. (Note: As shown, portions of Jurupa Avenue are not yet built; traffic on other vicinity roadways would likely increase over levels shown in Table IV-G-C as the result of drivers seeking alternate travel routes).

As shown in Table IV-G-D, all study area vicinity roadways will operate within capacity at ultimate widths, although Etiwanda Avenue north of Slover Avenue will operate near capacity -- 96 percent of available capacity will be utilized assuming project and area buildout.

c. **Level of Service.** As part of the traffic analysis prepared by the Project Engineer, Level of Service' (LOS) on study area vicinity roadways was determined for the "With Project" and "Without Project" scenarios. Resulting LOS for study area vicinity roadways is shown in Table IV-G-E, below.

-----  
Level of Service (LOS) is a relative measure of driver satisfaction, and is described in values ranging from "A" (free flow) to F (generally known to the layman as "gridlock"). Qualitative indicators used to determine LOS include traffic interruptions, freedom to maneuver, driver comfort, convenience, and safety. Although a LOS designation is primarily qualitative, some quantitative guidelines, such as length of time spent waiting for traffic signals to change and distance between vehicles, are also used to determine a roadway's LOS.

## CIRCULATION

TABLE IV-G-E

**LEVEL OF SERVICE: ULTIMATE ROADWAY CONFIGURATIONS**  
(Data Source: Krueper Engineering)

Roadway	W/O PROJECT	WITH PROJECT
	Level of Service	Level of Service
North of Slover	B	E
South of Slover	A	B
South of Santa Ana	A	B
South of Jurupa	B	B
 <u>Etiwanda Avenue</u>		
East of Etiwanda	A	B
West of Mulberry	A	A
East of Mulberry	A	A
 <u>Slover Avenue</u>		
South of Slover	A	B
South of Santa Ana	A	B
South of Jurupa	B	B
 <u>Mulberry Avenue</u>		
West of Etiwanda	A	A
East of Etiwanda	A	B
West of Mulberry	A	B
East of Mulberry	A	B
 <u>Jurupa Avenue</u>		
West of Etiwanda	A	A
East of Etiwanda	A	B
West of Mulberry	A	B
East of Mulberry	A	B

As shown in Table IV-G-E, all study area vicinity roadways will operate at acceptable levels of service at project area buildout and ultimate roadway widths, with the exception of Etiwanda Avenue north of Slover Avenue, which will operate at LOS E (although LOS E would likely not be achieved in practice as drivers, experiencing traffic congestion on this route, sought alternate routes).

According to the Project Engineer, this roadway link will experience LOS E due to the configuration of the dual bridge

## CIRCULATION

system located within this link. The railroad overpass in this area is limited in width to allow only four travel lanes, and the I-10 freeway overpass and on/offramp combination serve to further restrict operating efficiency. Further study will be required to determine whether rechannelization/striping of the roadway or reconstruction of the overpass will be required to increase LOS on this roadway link to acceptable levels.

d. Required Roadway Improvements. As discussed above, construction of all study area vicinity roadways to ultimate design configurations will be required to mitigate impacts generated as the result of future area buildout (not including construction of the proposed project). As also noted, rechannelization/striping or reconstruction of the railroad overpass on Etiwanda Avenue may also be required to mitigate impacts to the link of that roadway north of Slover Avenue. It should be noted that, as noted in Table IV-G-C, significant impacts would result even without development of the proposed project if vicinity roadways are not improved to their ultimate widths. Thus, significant impacts may result if the construction of roadway improvements lags behind need.

In addition, it was determined by the Project Engineer that, based on areawide and project buildout projections, traffic signals will be required at the following intersections:

- Slover Avenue - Mulberry Avenue (New signal)
- Slover Avenue - Jurupa Avenue (New signal)
- Etiwanda Avenue - Santa Ana Avenue (New signal)
- Etiwanda Avenue - Airport Drive/Slover Avenue (Modify existing signal)
- Etiwanda Avenue - Jurupa Avenue (New signal)

e. Internal Circulation System. As part of the proposed specific plan, internal roadway system improvements sufficient to carry projected traffic volumes have been designed by the Project Engineer. Ultimate configurations for study area internal roadways are shown in the Project Engineer's report, which is included in the appendices to this document.

## CIRCULATION

### 4. Mitigation Measures

The following mitigation measures are proposed to mitigate traffic-related impacts resulting from the proposed project:

- Concurrent with the development of individual phases of the proposed project (and prior to the issuance of the certificate of occupancy), an internal roadway system sufficient to serve the private improvements under development shall be installed or assured.
- Signage and landscaping for project area roadways shall be installed as discussed in the proposed specific plan (ref: Section 3.3.2 of the Specific Plan).

### 5. Analysis of Significance

Traffic on site vicinity roadways will increase upon development of the proposed project. However, since the proposed project will generally result in impacts equivalent to those which would be expected under the "No Project" scenario, for which mitigation measures have been provided (in the form of master-planned roadway improvements), and since the proposed project includes the construction of an internal roadway system sufficient to carry projected traffic volumes, no significant impacts are expected to result. It should be noted that significant impacts would result even without development of the proposed project if vicinity roadways are not improved to their ultimate widths. Thus, significant impacts may result if the construction of roadway improvements lags behind need.

## **PUBLIC SERVICES**

### **H. PUBLIC SERVICES**

#### **1. Environmental Issue**

The impacts of the proposed project on police, fire, and solid waste services will be examined.

#### **2. Existing Setting**

a. **Police Protection.** The study area is presently provided with police protection services by the San Bernardino County Sheriff's Department. Due to the relatively low intensity of use within the site, existing demand for police services is minimal, and is primarily limited to vandalism complaints and response to traffic accidents. Service to the site is provided from the station at 17780 Arrow Route in Fontana; response times to the study area range up to four minutes, depending upon the location of the patrol unit at the time calls are received.

b. **Fire Protection.** The study area is provided with fire protection services by the Central Valley Fire District. The primary responding station for the study area is Station #4, which is presently located in temporary quarters at Industry Avenue and Marley Avenue. Station #4 is provided with an engine and water tanker, and is staffed by three full-time firefighters. Response times to the study area from Station #4 average less than five minutes. If ladder service is necessary, service is provided from Station #2, located at 15380 San Bernardino Avenue. Response times from this station average between ten and 15 minutes, compared to optimum ladder response times of seven to nine minutes.

As discussed in the "Water Resources" section of this document, the existing water supply system within the project area is not presently sufficient to provide required 4,000 gpm, four-hour fire flow.

c. **Solid Water Services.** Solid waste services to the study area are provided by Fontana Rubbish Collectors, Inc., which is franchised by the City of Fontana to provide collection

## PUBLIC SERVICES

services throughout the City planning area. Solid wastes from the study area may be taken to the Fontana or Milliken landfills, although the Fontana site is presently scheduled to be closed in 1988-89. Assuming average employment of 24 persons per acre<sup>1</sup>, and utilizing a waste generation factor of 22.5 pounds of waste per employee per day<sup>2</sup>, the 257-acre developed portion of the site presently generates 138,780 pounds of waste per day.

### 3. Project Impacts

a. **Police Protection.** Demand for police protection service will increase upon development of the proposed project, due to the increased intensity of use within the study area. Based on discussion with the Sheriff's Department, the type of uses proposed for the study area will not generate significant numbers of calls for service, although introduction of increased numbers of office personnel may result in an increase in the number of calls related to burglaries from parked vehicles.

Upon annexation, the study area will be provided with police protection services by the City of Fontana Police Department.

Demand for police protection services can be reduced through a variety of strategies, including installation of fencing, lighting of storage areas, and design and siting of structures to reduce crime opportunities.

b. **Fire Protection.** Demand for fire protection services will increase upon development of the proposed project. As noted above, the study area is presently provided with service by Station #4 from a temporary location at Industry and Marlay. This station will ultimately be moved to a permanent location at Live Oak and Jurupa avenues, at which time additional equipment will be added to provide for two engines at this station.<sup>3</sup>

As discussed above, ladder service is presently provided from Station #2, located approximately ten to 15 minutes from the study area. Thus, development of the proposed project may require additional ladder service or relocation of existing ladder equipment to provide adequate service to the site.

<sup>1</sup> City of Ontario Final EIR 85-3

<sup>2</sup> County of San Bernardino, Solid Waste Management Master Plan

<sup>3</sup> Telecon with Capt. Gary Boyles.

## PUBLIC SERVICES

However, according to the Central Valley Fire District, ladder height -- the height which can be reached by standard engine service is required only for structures which exceed 35 feet in companies. Thus, ladder service -- which provides firefighting capabilities for structures of up to 100 feet in height -- will be required only if study area structures in excess of 35 feet in height are constructed.

Fire Station #4 is to be located at the southwest corner of Jurupa and Mulberry Avenues in the Southwest Industrial Park Specific Plan. This station will have adequate staff and equipment to serve the Southridge Village, Southwest Industrial Park and Fontana Gateway Specific Plans.

Development of the proposed project may also increase increase the number of uses within the site which utilize or store hazardous materials (see the Safety/Nuisance section of this document). Usage or storage of these materials is required to be coordinated with the CVFD, which maintains a listing of materials utilized at individual firms to aid in assessing equipment needs and strategies at fires involving these chemicals. The County of San Bernardino also maintains a Hazardous Materials response team which aids local agencies in the handling of hazardous materials; this team would be available to aid in incidents within the site.

Demand for fire protection services within the study area can be reduced through a number of strategies, including installation of automatic fire suppression systems and maintenance of safety standards in the storage and use of hazardous and flammable materials.

As discussed in the "Water Service" section of this document, installation of a backbone water supply system as described in the Project Engineer's report (see appendices to this document) will provide sufficient water flow to maintain a 4,000 gpm, four hour fire flow requirement.

c. Solid Waste. Development of the proposed project will increase the amount of solid waste produced within the site, compared to existing levels. Based on the employment and waste generation factors discussed above, the study area at buildout will generate 345,060 pounds of solid waste per day (based on 639 acres of developed area).

However, while waste generation will increase compared to existing levels, ultimate generation will not be significantly different than that which would be expected under the "No Project" scenario, which was utilized in the formation of projected lifetimes of County landfill facilities. Thus, the lifetimes of

## PUBLIC SERVICES

the Fontana and Milliken landfill will not be impacted to a greater extent than would otherwise occur, although the proposed project will incrementally reduce the lifetimes of these facilities.

Solid waste removal services by Fontana Rubbish will be expanded as demand increases, although costs for these services may increase as the result of increased haul distances, equipment maintenance, etc.

### 4. Mitigation Measures

The following measures are proposed to mitigate public service-related impacts of the proposed project:

#### Police Protection

- As project area development occurs, Police Department manpower and equipment should be expended as necessary to serve the proposed project and surrounding development.
- To reduce the need for police protection during construction, onsite security shall be provided for individual projects as development proceeds. The police department, along with the fire department, shall be provided with keys to all construction site gates.
- Adequate emergency access shall be provided to all study area structures, including the installation of "Knox Box"-type key receptacles for use by police and fire department personnel.
- The following are appropriate onsite security measures during construction:
  - \*\* Fencing of all outdoor storage areas.
  - \*\* Provision of nighttime security lighting for all study area structures and storage areas.

#### Fire Protection

- Since Station #4 provides adequate response times for study area development, and since new equipment will be added to this station's inventory, no additional fire facilities are necessary. However, development of

## PUBLIC SERVICES

significant numbers of structures in excess of 35 feet in height may require addition of ladder service to Station #4 or the relocation of existing ladder equipment to provide optimum response times.

- Concurrent with the the development of individual phases of the proposed project (and prior to the issuance of the certificate of occupancy), a water system sufficient to serve the private improvements under development shall be installed or assured.
- Site plans within individual developments shall be designed to have a minimum of two points of ingress and egress at all times, and shall be designed to accommodate access by emergency vehicles, including adequate turn-around areas.
- If required by City Ordinance, all study area structures shall be provided with automatic fire suppression systems, as required by City ordinance (if enacted) or the CVFD.

### Solid Waste

No mitigation is necessary to mitigate solid waste impacts.

### 5. Analysis of Significance

Demand for police and fire protection services will increase upon development of the proposed project. However, ultimate levels of demand will not be greater than those which would be expected under the "No Project" scenario. Since 1) revenues from the proposed project will be sufficient to offset the cost of providing additional service, and 2) mitigation measures have been proposed to reduce demand for police and fire protection, no significant impact are expected to result. Although demands for solid waste services will increase, ultimate waste generation will not exceed presently projected levels for the "No Project" scenario; thus, no significant impacts will result.

## ENERGY RESOURCES

### I. ENERGY RESOURCES

#### 1. Environmental Issue

The impacts of the proposed project on the consumption of electricity and natural gas will be examined.

#### 2. Existing Setting

The study area is provided with electrical service by the Southern California Edison Company, which maintains an extensive service network within the City of Fontana planning area. Natural gas service is provided by the Southern California, which also maintains an extensive service network within the City of Fontana planning area. Natural gas facilities within the vicinity of the study area include a ten-inch semi-high-pressure line within Mulberry Avenue and a two-inch main in Jurupa Avenue east of Mulberry Avenue.

Existing electricity and natural gas consumption within the study area were determined using the following consumption factors:<sup>1</sup>

Electrical Consumption: 118.5 kilowatt hours (Kwh)/day/acre

Natural Gas Consumption: 643.8 cubic feet (cf)/day/acre

Based on these factors, it is estimated that the 257-acre developed portion of the study area presently consumes approximately 30,452 Kwh of electricity per day, and 165,465 c.f. of natural gas per day.

#### 3. Project Impacts

Consumption of electricity and natural gas within the study area will increase, compared to existing levels, as a result of development of the proposed project. To determine onsite electricity and natural gas consumption, the consumption factors

---

<sup>1</sup> Consumption factors obtained from City of Ontario Final Environmental Impact Report 85-3, August 1985, and based on SCAQMD Handbook.

## ENERGY RESOURCES

noted above were utilized for the 639 net acre portion of the site subject to development. Resulting consumption projections are shown in Table IV-I-A, below.

TABLE IV-I-A

**PROJECTED DAILY ENERGY CONSUMPTION**  
(Based on Development of 639 Net Acres)

	Electrical Consumption Rate: 118.5 kilowatt hours (Kwh)/day/acre	Natural Gas Consumption Rate: 643.8 cubic feet (cf)/day/acre
Projected Consumption	75,717 kwh/day	411,410 cf/day

It should be noted that the figures shown in Table IV-I-A represent average consumption; actual consumption of energy resources will be subject to a number of factors, including the specific type of manufacturing uses (and utilization of energy resources) which ultimately move onto the site, and relative cost/availability of either energy source. For instance, installation of energy-intensive uses, such as electronics component manufacturing, may result in higher energy utilization than reflected in Table IV-I-A, while installation of low-energy-intensive uses, such as warehousing or storage, would result in lower energy consumption.

Although energy consumption will increase upon development of the proposed project, ultimate energy consumption is not expected to differ significantly from that which would result from buildup under existing industrial land use designations. Demand for facilities may occur sooner with implementation of the proposed project than would be expected under existing conditions.

### 4. Mitigation Measures

The following measures are proposed to mitigate energy-related impacts resulting from development of the proposed project:

## ENERGY RESOURCES

- All structures shall be required to comply with applicable energy conservation measures required by the California Administrative Code.
- To the extent possible, and consistent with other mitigation measures, onsite buildings and infrastructure shall be designed to:
  - \*\* maintain internal roadways at the minimum widths possible, consistent with maintaining safe circulation, to reduce the amount of heat reflected from roadways onto structures.
  - \*\* Landscaping should be provided to reduce the amount of energy reflected towards buildings.
  - \*\* provide for reflective coatings on roofs, wherever possible, to reduce heat absorption.
  - \*\* provide for the use of tinted or reflective glass to help regulate interior temperatures.
  - \*\* provide for the use of energy-efficient lighting in both indoor and outdoor applications.
  - \*\* provide for the insulation of water heaters and hot water transmission lines to reduce heat loss.
  - \*\* incorporate, wherever possible, the utilization of the most energy-efficient technology available for manufacturing or other energy-intensive uses.
  - \*\* encourage the installation of cogeneration facilities to utilize spent heat to generate electricity and reduce power plant demand.

### 5. Analysis of Significance

Consumption of energy resources within the study area will increase upon development of the proposed project. However, as noted, projected consumption of energy resources will not differ significantly from presently projected levels, due to the inclusion of similar land uses in the proposed project and "No Project" scenarios. Thus, projected energy consumption is within levels included in facilities planning and programming by the Southern California Edison and Southern California Gas companies, and has therefore been previously mitigated to a level of insignificance. However, as also noted, demand for service may

## ENERGY RESOURCES

occur sooner than would be expected under the "No Project" scenario, due to the expedited construction schedule included which would result from the proposed project.

## WATER SERVICE

### J. WATER SERVICE

#### 1. Environmental Issue

The impacts of the proposed project on the demand for water service will be examined.

#### 2. Existing Setting

The study area is provided with water service by the Fontana Water Company, which is a division of the San Gabriel Water Company. The Fontana Water Company presently maintains a service network within the study area which consists of ten-inch lines in Etivanda, Santa Ana, and a portion of Jurupa avenues, and Dahlia and Jasmine Streets; a 12-inch line in Slover Street, and a 16-inch line in Mulberry Avenue extending southward from Slover Street to a point roughly parallel to the extension of Dahlia Street. The existing water system is illustrated in the report on the proposed project prepared by the Project Engineer, Krueper Engineering and Associates, Inc., which is included in the appendices to this report.

To determine existing flow within the study area, a series of fire flow tests were recently completed by the Fontana Water company; results of these tests are shown in Table IV-J-A, below.

## WATER SERVICE

TABLE IV-J-A

FIRE FLOW TEST RESULTS  
(Source: Krueper Engineering)

Test Location	Flow (Gallons Per Minute) <sup>1</sup>
Terminus of 16-inch line in Mulberry Avenue	3,892
Intersection of Slover and Etiwanda avenues	3,088
Intersection of Etiwanda Avenue and Dahlia Street	2,828
Jurupa Avenue east of Etiwanda Avenue	1,642
Jurupa Avenue east of Etiwanda Avenue	1,664

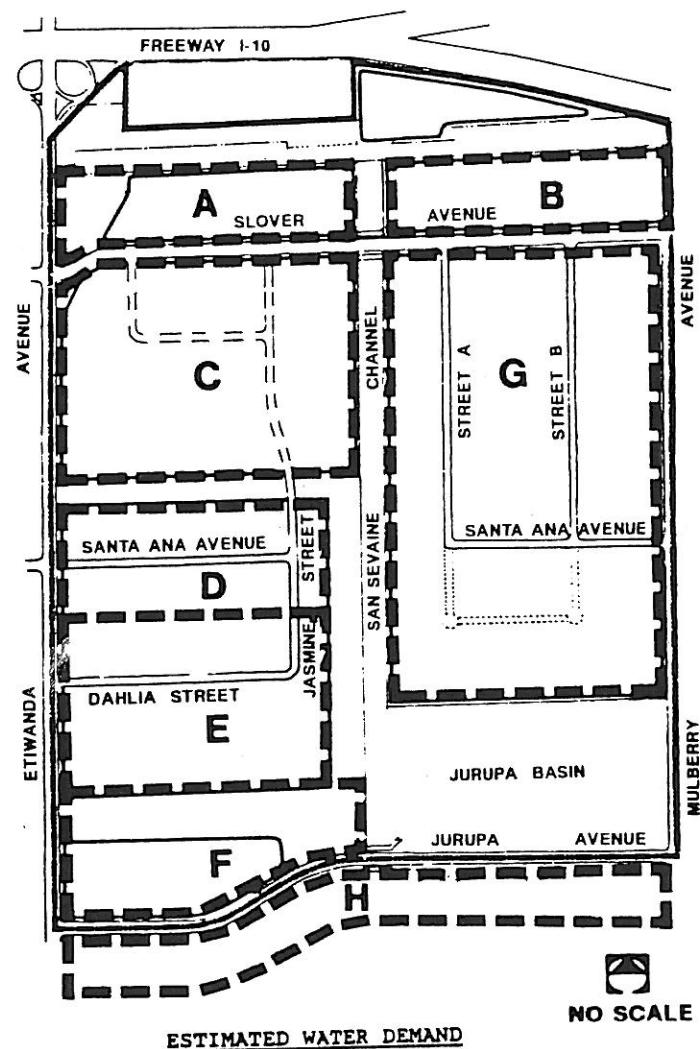
<sup>1</sup> Fire flow measurements taken at 20 PSI.

### 3. Project Impacts

Consumption of water within the study area will increase upon development of the proposed project. Based upon discussion by the Project Engineer with the Fontana Water Company, an average consumption rate of 750 gallons per day per acre was determined. Based on this rate, water consumption for sub-areas within the study area at buildout was determined; resulting consumption is shown in Table IV-J-B, below. Figure IV-J-1 illustrates the sub-areas used to determine water consumption.

It should be noted that, since land uses within the study area will not be significantly different than would be expected under the "No Project" scenario, which was utilized in planning assumptions made by the Fontana Water Company in the long-term planning of its facilities, no significant changes in long-term water demand will occur. Thus, no changes in long-term facili-

# WATER QUANTITY GENERATION ANALYSIS



### ESTIMATED WATER DEMAND

<u>AREA</u>	<u>ACREAGE</u>	<u>GALLONS/MIN.</u>	<u>GALLONS/DAY</u>
		<u>(gpm)</u>	<u>(gpd)</u>
A	39	49	29,250
B	36	45	27,000
C	110	137	82,500
D	57	71	42,750
E	84	105	63,000
F	53	66	39,750
G	218	272	163,500
H	42	52	31,500
<b>TOTAL</b>	<b>639 acres</b>	<b>797 gpm</b>	<b>479,250 gpd</b>

**NOTE:**  
THE DEPICTED OPTIONAL ROADWAYS AND  
ALL RELATED PUBLIC UTILITY FACILITIES  
ARE CONCEPTUAL ONLY AND WILL BE  
INSTALLED ONLY IN THE DISCRETION OF  
EACH AFFECTED PROPERTY OWNER. NO  
IMPLICATION OF PUBLIC UTILITY EASEMENT  
OR PUBLIC UTILITY FACILITY LOCATION IS  
TO BE DERIVED FROM THE LOCATION OF  
THESE CONCEPTUAL ROADWAYS.

**FIGURE IV-J-1**

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## WATER SERVICE

ties construction will be required, although some facilities may need to be installed sooner than originally planned, due to the increased buildout schedule anticipated under the proposed project.

TABLE IV-J-B

PROJECTED WATER CONSUMPTION  
(Source: Krueper Engineering)

Sub-Area	Acres <sup>1</sup>	Gallons Per Day
A	39	29, 250
B	36	27, 000
C	110	82, 500
D	57	42, 750
E	84	63, 000
F	53	39, 750
G	218	163, 500
H	42	31, 500
Total:	639	479, 250

<sup>1</sup> Figures reflect net acreage, and do not include streets.

Although water consumption will increase upon development of the site, compared to existing levels, it was determined by the Project Engineer, based upon discussion with the Fontana Water Company, that fire flow requirements for the study area would require larger water lines than would be sufficient to meet normal water service needs. Thus, the sizing of lines serving the study area will be determined by fire flow requirements, rather than service needs. The existing water main system will not be inadequate to provide sufficient service to the study area, based on a 4,000 gallon per minute, four hour fire flow requirement (see Table IV-J-A).

## WATER SERVICE

As part of the report prepared by the Project Engineer, sizes of water lines necessary to serve the proposed project were calculated; the resulting backbone water system is illustrated in the Project Engineer's report, which is included in the appendices of this document.

It is anticipated that a part or all of the water system would be installed as necessary to serve private improvements being developed in each of the three phases proposed for the specific plan (see Section II of this document for discussion of phasing).

### 4. Mitigation Measures

The following mitigation measures are proposed to mitigate water service impacts resulting from the proposed project:

- Prior to the recordation of final maps for any phase of the proposed project, a backbone water system capable of providing sufficient fire flows to the portion of the study area shall be installed or assured.

### 5. Analysis of Significance

Water consumption within the project area will increase upon buildup of the proposed project, compared to existing levels. However, ultimate water consumption will not be significantly different than would be expected under existing land use designations, although the demand for increased water service may occur sooner than would be anticipated under the "No Project" scenario.

Since adequate water service will be available to each phase of the proposed project prior to the need for such facilities, and since water demand within the study area upon buildup will be essentially the same as was considered in long-range planning assumptions utilized by the Fontana Water Company, no significant impacts related to water consumption will result.

## SEWER SERVICE

### K. SEWER SERVICE

#### 1. Environmental Issue

The impacts of the proposed project on the demand for sewer service will be examined.

#### 2. Existing Setting

The study area is provided with sewer service for industrial wastes by the City of Fontana, which presently maintains an eight-inch sewer main in Jurupa Avenue. This line connects to an existing Chino basin Municipal Water District (CBMWD) 33-inch trunk line in Marlay Avenue, which carries sewage to CBMWD's Regional Plant #1 (RP-1), located in the City of Ontario in the southwest quadrant of the Pomona Freeway (Route 60) and Archibald Avenue. It should be noted that the specific plan area is not presently connected to the area sewage system.

The study area is not presently provided with industrial waste removal via a "brine line" or similar system, although a sewer line for industrial waste is located south of the study area.

#### 3. Project Impacts

As part of the preparation of this report, an analysis of projected sewage generation was prepared by the Project Engineer, Krueper Engineering & Associates, Inc. The complete text of the Project Engineer's report is included in the appendices to this report.

As part of the analysis of projected sewage generation, the study area was subdivided into eight sub-areas, designated A-H (see Figure IV-J-1 for illustration of sub-areas). Projected sewage generation was prepared by applying a 70-80 percent return rate to the water consumption figures shown in Section IV-J of this report. Projected sewage generation by sub-area is shown in Table IV-K-A, below. As noted in Section IV-J of this document, projected sewage flows at buildout of the proposed project will not be significantly different than would be expected under the "No Project" scenario, although ultimate flows may be achieved sooner than would be expected.

## SEWER SERVICE

Since sewage flows for the "No Project" scenario were utilized in planning assumptions by the Chino Basin Municipal Water District, no additional facilities will be needed to serve the proposed project, although the demand for facilities may occur sooner than expected, due to the expedited construction schedule included in the proposed project.

TABLE IV-K-A

PROJECTED SEWAGE GENERATION<sup>1</sup>  
(Source: Krueper Engineering)

Sub-Area	Acres	Gallons Per Day
A	39	23,100
B	36	21,200
C	110	65,000
D	57	33,600
E	84	49,700
F	53	31,400
G	218	128,800
H	42	24,700
<b>Total:</b>	<b>639</b>	<b>377,500</b>

<sup>1</sup> See Figure II-4 in the Wastewater section of Appendix B for location of sub-areas.

As part of the sewage analysis prepared by the project engineer, a backbone sewer system for the study area was designed. As discussed in the description of project phasing contained in this document, the onsite sewage system improvements will be constructed in phases as development of the study area proceeds. Detailed sewage phasing information is found in the Project Engineer's report, which is included in the appendices to this document.

## SEWER SERVICE

### 4. Mitigation Measures

The following mitigation measures are proposed to mitigate sewage-related impacts:

- Concurrent with the development of individual phases of the proposed project (and prior to the issuance of the certificate of occupancy), a sewer system sufficient to serve the private improvements under construction shall be installed or assured.

### 5. Analysis of Significance

Sewage generation will increase upon development of the proposed project, compared to existing levels. However, ultimate sewage generation will not differ significantly from levels which would be expected under the "No Project" scenario, although ultimate generation levels may be achieved sooner than would be expected under the "No Project" buildout schedule. Since required sewer connection fees will be paid and ultimate generation levels will be similar, and since generation levels expected under the "No Project" scenario have been utilized in the planning assumptions made by the Chino Basin Municipal Water District in the long-range sizing and planning of its facilities, no facilities will be required in addition to those already planned, although these facilities may be needed ahead of presently planned schedules.

Since adequate sewage service will be available from existing and presently planned facilities, and since sewage lines will be constructed ahead of need for each individual phase of the proposed project, no significant impact will result.

## CULTURAL RESOURCES

### L. CULTURAL RESOURCES

#### 1. Environmental Issue

The proposed project may result in the alteration or destruction of existing cultural or historic resources, if any, within the study area.

#### 2. Existing Setting

According to the City of Fontana General Plan, no identified historical or cultural resources are located within the study area. Although resources may have been located within the site, the past use of the site for agricultural and, more recently, industrial uses can be assumed to have destroyed any surface evidence of artifacts.

A recent cultural resources survey conducted for proposed improvements along San Sevaine Creek concluded that the likelihood for finding cultural resources within the vicinity of the Kaiser Specific Plan was very low. However, since the site is located within the floodplain of San Sevaine Creek, there is the potential for uncovering of subsurface artifacts during grading activities.

#### 3. Project Impacts

Since historical or cultural resources are not expected to be located within the study area, no impacts are expected to result. As noted above, the potential exists for uncovering subsurface artifacts during grading operations within the site.

#### 4. Mitigation Measures

The following measure is proposed to mitigate cultural resource impacts of the proposed project:

- If cultural resources are uncovered during grading and construction activities, all work in the area of the find shall cease until a qualified archaeologist is consulted to assess its significance and research

## CULTURAL RESOURCES

potential. The recommendations of the archaeologist regarding disposition of the find shall be followed.

### 5. Analysis of Significance

Since no identified historical or cultural resources are located within the study area, no impacts to these resources are expected to result from the proposed project.

## PROJECT ALTERNATIVES

### V. ALTERNATIVES TO THE PROPOSED PROJECT

This environmental impact report evaluates the implications of several different land use scenarios for the project area. The proposed project was described in Chapter II, Project Description.

This section examines alternatives to the proposed development concept presented in Chapter II. The alternatives examined consist of:

- No Development (Maintain Existing Uses);
- No Project (Full Development Without Specific Plan) and;
- Two Land Use Alternatives.

In addition to the discussions of the alternatives to the proposed project, an Impact Matrix (Table V-A) which compares the environmental impacts of the various alternatives is provided following the discussion of the project alternatives.

## PROJECT ALTERNATIVE: "NO DEVELOPMENT"

### A. "NO DEVELOPMENT" ALTERNATIVE

Under this alternative, existing uses within the study area would be maintained; no additional areas would be developed. The study area would not be annexed into the City of Fontana, and no redevelopment assistance from the City of Fontana would be available for development within the site.

Since this alternative would maintain existing conditions within the study area, it provides a baseline for measurement of impacts resulting from the proposed project and the three other alternatives examined in this document.

It should be noted that the City of Fontana, through the initiation annexation proceedings and the development of a specific plan for the development/redevelopment of the study area, rejected this alternative as undesirable.

Since the intensity of use within the site would remain at existing levels, this alternative would generally result in reduced impacts compared to the proposed project. However, since existing unimproved drainage areas would remain in place, the potential for erosion of study area soils would be higher than would be expected under the proposed project.

Environmental impacts which would result from this alternative are discussed below in comparison to those which would be expected from the proposed project.

**Soils** Impacts to soils would not result. However, the existing unimproved drainage course in the northeastern portion of the site would remain in place, resulting in the continued long-term water erosion of soils in this area.

**Air Resources** Impacts to air resources would be reduced, due to the reduction in the consumption of energy resources and generation of traffic which would occur under this alternative.

**Drainage/Flooding** Storm runoff would be reduced, since existing pervious areas would remain in place. However, existing drainage system inadequacies, including the unimproved drainage course in the northeastern portion of the site, would remain in place.

PROJECT ALTERNATIVE: "NO DEVELOPMENT"

**Biotic Resources** Impacts to biotic resources would not result, due to the maintenance of existing undeveloped areas in their present condition.

**Noise** Impacts would be reduced, due to the generation of lower traffic levels and continuation of less intensive uses than would occur under the proposed project.

**Safety/Nuisance** Impacts would be reduced, due to the maintenance of less intensive uses than would occur under the proposed project.

**Circulation** Impacts to the local circulation system would be reduced, due to the generation of lower levels of traffic than would result under the proposed project.

**Public Services** Demand for public services would be generally reduced, due to the inclusion of less intensive uses than would occur under the proposed project. However, existing fire flow inadequacies would result, due to the lack of a master-planned backbone water system, potentially resulting in increased fire service impacts.

**Energy Resources** Consumption of energy resources would be reduced compared to the proposed project, due to the inclusion of less intensive uses.

**Water Service** Consumption of water within the study area would be reduced. However, fire flows sufficient to fight study area fires would not be available, since this alternative does not include construction of an expanded backbone water system.

**Sewer Service** Production of sewage would be reduced, due to the reduced consumption of water which would result from this alternative.

**Cultural Resource** Cultural resources would not be impacted. In addition, the potential for uncovering subsurface artifacts would not exist.

## "NO PROJECT" ALTERNATIVE

### B. "NO PROJECT" ALTERNATIVE

Under this alternative, the study area would not be annexed by the City of Fontana, and would be developed by individual property owners under existing industrial land use designations of San Bernardino County. In addition, the study area would not be included in the City's redevelopment area, resulting in the potential for decreased availability and increased cost of funding for infrastructure and capital improvements within the site.

Since development of the site would take place on an individual basis without master-planning of facilities, and since funding for infrastructure improvements necessary to support increased intensity of use with the site would be more difficult and expensive to obtain, it is expected that this alternative would be built out over a longer period of time than would be expected under the proposed project.

Since development would not be master-planned and phased, the potential exists that individual users would be required to pay higher initial costs to provide adequate infrastructure improvements to serve sites separated from the presently developed portions of the site. In addition, since redevelopment assistance for infrastructure development would not be forthcoming, it is possible that desired development within the study area would be prohibitively expensive, thus preventing implementation of the City's General Plan.

Ultimate types and intensity of use would remain essentially the same as those which would be expected under the proposed project; thus, with the exception of timing and lack of master-planning, this alternative would result in essentially the same impacts as the proposed project.

Impacts which would result under this alternative are discussed below in comparison to the proposed project.

**Soils** Impacts to onsite soils would be essentially the same as those expected under the proposed project, although onsite erosion of soils within the unimproved drainage course could potentially occur over a longer period of time.

**Air Resources** Impacts to air resources would be essentially the same as those which would be expected under the proposed project, although generation of ultimate emissions levels would most likely occur at a later date.

## "NO PROJECT" ALTERNATIVE

**Drainage** Impacts to onsite drainage would be essentially the same as those expected under the proposed project, although existing drainage system inadequacies would likely remain in place for a longer period of time, due to the lack of redevelopment assistance to install infrastructure improvements.

**Biotic Resources** Impacts to biotic resources would be essentially the same as those expected under the proposed project, although this alternative would likely not include the level of landscaping included in the proposed project.

**Noise** Noise impacts of this alternative would be essentially the same as those which would be expected under the proposed project, although the attainment of ultimate noise levels would occur over a longer period of time.

**Safety/Nuisance** Impacts related to safety/nuisance factors would be essentially the same as those which would be expected under the proposed project.

**Circulation** Impacts to the local circulation system would be essentially the same as those expected under the proposed project, although attainment of ultimate traffic levels would occur at a later date. However, internal traffic system impacts would likely be increased, due to the lack of redevelopment assistance to install required circulation system improvements.

**Public Services** Impacts to public services would be essentially the same, although lack of water system improvements could result in increased fire protection impacts due to lack of sufficient fire flows to provide adequate fire flows.

**Energy Resources** Onsite consumption of energy resources would be essentially the same as those expected under the proposed project.

**Water Service** Onsite consumption of water would be essentially the same as that which would be expected under the proposed project.

**Sewer Service** Onsite generation of sewage would be essentially the same as that which would be expected under the proposed project.

**"NO PROJECT" ALTERNATIVE**

**Cultural Resources** Since no cultural resources have been identified onsite, no impacts would result. The potential for uncovering subsurface impacts would continue.

## LAND USE ALTERNATIVE "ONE"

### C. LAND USE ALTERNATIVE "ONE"

This alternative examines a slightly modified land use scenario for the study area. Under this alternative, the study area would be annexed into the City of Fontana's redevelopment area, and all infrastructure improvements included in the proposed project would be built. This alternative would also be built under the same phasing schedule included in the proposed project.

As noted, this alternative includes a slightly modified land use scenario. Under this alternative, land uses within the study area would be limited to industrial uses; the commercial and incubator industrial uses included in the proposed project would not occur. This land use alternative envisioned a more basic type industrial development than is presently being proposed. For example, image-creating features aimed at defining an overall project area identity for the specific plan area would be kept to a minimum, and individual properties within the project area would be marketed on an individual basis.

A comparison of this alternative to the proposed project is shown in Table V-C-A, below.

TABLE V-D-A

#### LAND USE "ONE" COMPARISON TO PROPOSED PROJECT

Land Use	Proposed Project	Land Use Alternative "Two"
Industrial	88%	100%
Incubator Industrial	11%	---
Commercial	1%	---

It should be noted that this alternative was reviewed by the City of Fontana during the preparation of the specific plan, and was ultimately rejected in favor of the present proposal. The projected absorption of this alternative was relatively slow, and it was felt that on at least a limited basis that the incubator industrial uses would enhance the overall absorption of the

## LAND USE ALTERNATIVE "ONE"

specific plan area since they would expand the range of available industrial opportunities. In addition, it was felt the the inclusion of a limited amount of support commercial development would enhance the desirability of the project area for industrial development.

Since this alternative would result in essentially similar land uses and would be built under the same phasing schedule as the proposed project, and since infrastructure improvements would be available on an equivalent time schedule, impacts associated with this alternative would be the same as those expected from the proposed project, as shown in Table V-A.

## LAND USE ALTERNATIVE "TWO"

### D. LAND USE ALTERNATIVE "TWO"

As with Land Use Alternative One, this alternative is similar in most respects to the proposed project. Under this alternative, the study area would be annexed into the City of Fontana's redevelopment area, and all infrastructure improvements included in the proposed project would be built. This alternative would also be built under the same phasing schedule included in the proposed project.

Under this alternative, a higher proportion of incubator industrial and commercial land uses would be developed within the study area, as shown in Table V-D-A, below. Significant effort would be placed into creating a positive image for the the City gateway and for project site as a desirable industrial development site.

TABLE V-D-A

#### LAND USE "TWO" COMPARISON TO PROPOSED PROJECT

Land Use	Proposed Project	Land Use Alternative "Two"
<hr/>		
Industrial	88%	79%
Incubator Industrial	11%	17%
Commercial	1%	4%
<hr/>		

It should be noted that this alternative was examined by the City of Fontana and rejected as infeasible. Increasing the amount of incubator industrial and support commercial development would require a much more significant marketing program than would be necessary under the present proposal, and the expected results of the significantly higher costs for image creation and marketing would be marginal as compared to the present proposal.

Since this alternative would include essentially similar land uses, environmental impacts would also be essentially similar, as shown in Table V-A.

# LAND USE ALTERNATIVE "TWO"

**TABLE V-A**  
**PROJECT ALTERNATIVES IMPACT COMPARISON MATRIX**  
 (All impacts discussed in comparison to proposed project)

ENVIRONMENTAL FACTOR	PROJECT ALTERNATIVE			
	LAND USE ALTERNATIVE		LAND USE ALTERNATIVE	
	NO DEVELOPMENT	NO PROJECT	"1"	"2"
Soils	No direct impact. Existing unimproved drainage areas will remain, resulting in continued erosion potential.	No change in identified impacts, although opportunity for coordination of grading and erosion activities will be reduced.	No change in identified impacts.	No change in identified impacts.
Air Resources	Impacts reduced due to decreased intensity of use.	No change in identified impacts.	Potential for increased impacts due to inclusion of larger number of "heavy" industrial uses.	Potential for decreased impacts, due to inclusion of larger proportion of office and commercial uses.
Drainage/ Flooding	Decreased storm flows, due to decrease in impervious area.	No change in identified impacts.	No change in identified impacts.	No change in identified impacts.
Biotic Resources	No impact.	No change in identified impact; reduced opportunity for coordination of project area landscaping.	No change in identified impact.	No change in identified impact.
Noise	Reduced impact, due to reduced traffic levels.	No change in identified impacts.	No change in identified impacts.	No change in identified impacts.

# LAND USE ALTERNATIVE "TWO"

TABLE V-A  
(Continued)

**PROJECT ALTERNATIVES IMPACT COMPARISON MATRIX**  
(All impacts discussed in comparison to proposed project)

ENVIRONMENTAL FACTOR	PROJECT ALTERNATIVE			
			LAND USE ALTERNATIVE	LAND USE ALTERNATIVE
	NO DEVELOPMENT	NO PROJECT	"1"	"2"
Safety/ Nuisance	Reduced impact due to reduced intensity of use.	No change in identified impacts.	No change in identified impacts.	No change in identified impacts.
Circulation	Reduced impact due to reduced intensity of use.	No change in identified impacts.	No change in identified impacts.	No change in identified impacts.
Public Services	Reduced impact due to reduced intensity of use.	No change in identified impacts. Poten- tial for in- creased impact due to higher proportion of heavier indus- trial uses and lack of ade- quate fire flow.	No change in identified impacts.	No change in identified impacts.
Energy Resources	Reduced impacts due to reduced in- tensity of use.	No change in identified impacts.	No change in identified impacts.	No change in identified impacts.
Water Service	Reduced impacts due to reduced intensity of use.	Increased im- pact due to lack of master-plan- ned water system improvements.	No change in identified impacts.	No change in identified impacts.
Sewer Service	Reduced impacts due to reduced intensity of use.	Increased im- pact due to lack of master-plan- ned sewer system improvements.	No change in identified impacts.	No change in identified impacts.

LAND USE ALTERNATIVE "TWO"

TABLE V-A  
(Continued)

PROJECT ALTERNATIVES IMPACT COMPARISON MATRIX  
(All impacts discussed in comparison to proposed project)

ENVIRONMENTAL FACTOR	PROJECT ALTERNATIVE			
	LAND USE ALTERNATIVE		LAND USE ALTERNATIVE	
	NO DEVELOPMENT	NO PROJECT	"1"	"2"
Cultural Resources	No impact.	No impact.	No impact.	No impact.

## SHORT TERM-LONG TERM RELATIONSHIPS

### VI. RELATIONSHIP BETWEEN LOCAL SHORT-TERM USES AND THE MAINTENANCE AND ENHANCEMENT OF LONG-TERM PRODUCTIVITY

Implementation of the proposed project will result in a number of minor short-term impacts associated with construction, including noise, dust, and air pollution generation. Road improvements and installation of expanded water, sewer, and drainage systems will require that some work occur within the rights-of-way of roadways adjacent to the project area, temporarily affecting traffic flow.

Long-term impacts generated by the project will result primarily from the increased intensity of use within the site, compared to existing levels (although, as noted in the Project Description, the proposed use will not result in a higher intensity of use than would occur under existing land use designations). The impacts described in this document, with the exception of those associated with construction, are long-term, and are within the limits of the impacts described in the City of Fontana General Plan.

## SIGNIFICANT IRREVERSIBLE IMPACTS

### VII. SIGNIFICANT IRREVERSIBLE ENVIRONMENTAL CHANGES

In general, irreversible environmental changes which will result from approval of the proposed specific plan can be classified as one-time use of energy and construction materials required for development of the site and the consumption of natural resources by future industrial users.

The consumption and use of natural resources such as fossil fuels, air, water, sand and gravel, and other building materials is an irreversible commitment which would be mandated by implementation of the proposed project. These materials will be used for preparation of the site, construction of onsite buildings, and improvements to the roadway, water, sewer, flood control, electrical, natural gas, and telephone systems within the study area.

Approval of the proposed project, as discussed in the Project Description section of this document, will result in a commitment of the site to uses of an equivalent intensity as are presently proposed.

Expected irreversible environmental changes which will result from development of the proposed project are:

- 1) An incremental increase in local air pollutant emissions, and a decrease in air quality downwind of the study area.
- 2) An incremental increase in consumption of water and an attendant increase in the generation of wastewater, compared to existing levels.
- 3) An increase in the amount of runoff produced by the project area during storms, compared to existing levels.
- 4) An incremental decrease in the lifetime of the Milliken and Fontana landfills.
- 5) An increase in local traffic volumes compared to existing levels.
- 6) An increase in local noise levels, compared to existing levels.

## CUMULATIVE AND GROWTH-INDUCING IMPACTS

### VIII. CUMULATIVE AND GROWTH-INDUCING IMPACTS

#### A. UTILITIES DEMAND

##### 1. Water Consumption

Projected water consumption within the study area will increase, compared to existing levels, although ultimate consumption levels will be essentially equivalent to those expected under the "No Project" scenario.

##### 2. Sewage Generation

Sewage generation within the study area will increase, compared to existing levels, although ultimate generation levels will be essentially equivalent to those expected under the "No Project" scenario.

##### 3. Solid Waste

Future solid waste generation will increase, compared to existing levels, although ultimate generation will be essentially equivalent to presently projected levels for the "No Project" scenario.

## CUMULATIVE AND GROWTH-INDUCING IMPACTS

### B. CIRCULATION

Local traffic volumes will increase, compared to existing levels, resulting in acceptable levels of service on all project vicinity roadways, with the exception of Etiwanda Avenue which will experience unacceptable levels of service due to the configuration of the railroad overpass and freeway ramp system in this area. Ultimate traffic volumes generated by the proposed project will be essentially the same as those which would occur under the "No Project" scenario.

## CUMULATIVE AND GROWTH-INDUCING IMPACTS

### C. ENERGY CONSUMPTION

Consumption of electricity and natural gas within the project area will increase compared to existing levels, although ultimate levels will be essentially equivalent to those expected under the "No Project" scenario.

## PREPARATION RESOURCES

### IX. DOCUMENT PREPARATION RESOURCES

#### A. Preparation Staff

##### 1. Environmental: Planning Network

Lloyd B. Zola, AICP (714) 983-4144  
Eric Norris

##### 2. Land Planning: Forma

Rick Gomez (714) 989-2232

##### 3. Engineering: Krueper Engineering and Associates, Inc.

(714) 884-2159

## PREPARATION RESOURCES

### B. Persons and Organizations Consulted

#### City of Fontana

Police Department: Lt. Sam Scott (714) 350-7740

#### Central Valley Fire Protection District

James Allman (714) 829-4441  
Dave Nunez  
Gary Boyles

#### City of Los Angeles Department of Airports

Ontario Airport: Michael DiGirolamo (714) 983-8282  
Chuck Zieman

#### County of San Bernardino

Sheriff's Department: Sgt. Tom Denham (714) 383-3731

#### Federal Aviation Administration

Bud Whitfield (213) 536-6656

#### Southern California Edison Company

Marion Watkins (213) 491-2835

#### South Coast Air Quality Management District

Michael Manfredi (213) 572-6200  
Brian Farris

#### State of California

Department of Transportation (District 8)

Bill McKinney (714) 383-4561  
Maurice Loge (714) 383-4643

## PREPARATION RESOURCES

### C. References

City of Fontana, General Plan

City of Ontario, Final Environmental Impact Report 853, August 1985.

County of San Bernardino, Preliminary Solid Waste Management Master Plan, 1985.

J. J. Van Houten and Associates, California Noise Insulation Standards: Requirements, Measurements, Analysis, Compliance, October 1974.

Krueper Engineering and Associates, Inc., Analysis/Summary Kaiser Specific Plan, June 23, 1986.

South Coast Air Quality Management District, Air Quality Handbook for Environmental Impact Reports, December 1983.

United States Environmental Protection Agency, Compilation of Air Pollutant Emission Factors, Third Edition, August 1977.

**Appendix A**

**Environmental Description Form**

**and Agency Responses**

City of  
Fontana  
Planning

PEDE  
Preliminary  
Environmental  
Description Form

OFFICE USE ONLY

Project Title \_\_\_\_\_

Received By \_\_\_\_\_

Date Received \_\_\_\_\_

Env. Log. # \_\_\_\_\_

INTRODUCTION: The questions in this form are designed to obtain enough data about your proposed project and your project site to allow the staff to adequately assess your project.

Applications will not be accepted for processing unless they are completely filled out, signed and all appropriate attachments are made. Additional information required for completion of environmental assessment may be requested of the applicant.

I. GENERAL INFORMATION

A. Applicant: Kaiser Steel Corp. Phone (714) 350-5204

Address: P.O. Box 5050 Corporate Office Building, COB9 Fontana,  
California 92335

B. General Location of the project: South of I-10 Freeway, north of Jurupa Avenue, west of Mulberry Avenue, east of Etiwanda Avenue.

C. Project Address / Assessor's No.(s): See general location

D. Current General Plan Designation: Industrial

E. Present Zoning: County zoning presently Proposed Zoning: \* Industrial

F. Present Use of Site: Primarily vacant land with some industrial sites.

G. Does This Proposal Involve a: ZONE CHANGE  CONDITIONAL USE PERMIT

GPA

DRB

VARIANCE

TENTATIVE TRACT MAP

H. Is Proponent Requesting Exemption Status? No

I. Is There an Existing EIR For The Proposal? No

B. Commercial and Industrial Projects: Information for these two sections not available until completion of the Specific Plan.

1. Type of facilities:

Neighborhood  Community  Convenience

2. Square footage of:

Sales area \_\_\_\_\_ Storage area \_\_\_\_\_

3. Hours of operation \_\_\_\_\_

4. Total number of employees \_\_\_\_\_

5. Employees per shift \_\_\_\_\_

6. Percent of total project proposed for:

Building \_\_\_\_\_ Paving \_\_\_\_\_

Parking \_\_\_\_\_ Landscaping \_\_\_\_\_

7. Number of parking spaces required \_\_\_\_\_; number provided \_\_\_\_\_.

8. Estimated amount of solid refuse \_\_\_\_\_

C. Institutional Projects:

1. Type of facilities: \_\_\_\_\_

\_\_\_\_\_

2. Square footage of buildings \_\_\_\_\_

3. Hours of operation \_\_\_\_\_

4. Number of employees \_\_\_\_\_

5. Number of parking spaces required \_\_\_\_\_; number provided \_\_\_\_\_.

6. Estimated amount of solid refuse per year \_\_\_\_\_

IV. PHYSICAL AND ENVIRONMENTAL INFORMATION

Are the following items applicable to the project or its effects?

Discuss below all items checked yes. (Attach additional sheets if necessary.)

Environmental Issues continued

UNKNOWN    YES    NO

X                13. Will there be any alternative means of generating energy to be used for this project in lieu of fossil fuel?

X                14. Could the project involve the possibility of contaminating a public water supply system or adversely affect ground water recharge?

B. Physical Issues

UNKNOWN    YES    NO

      X          1. Will the project involve additions to existing structures which increases the floor area by 50% or by 2,500 sq. ft.?

      X          2. Will the project involve more than dividing an existing multiple family rental into condominiums?

X                3. Will the project demolish or remove structures or features which are of historical, archeological or architectural consequences?

      X          4. Will the project involve lot line adjustments which create a new parcel or a change in land use or density?

      X          5. Will the project require the issuance of a major encroachment permit?

      X          6. Will the project change the demand for City services? (police, fire, water, sewage, etc.)?

      X          7. Will the project result in the dislocation of people who currently live or work at the site?

      X          8. Will this project have any impact upon the existing character of the immediate area?

      X          9. Will the project create additional demands for public recreational areas or open space?

APPENDIX I  
ENVIRONMENTAL CHECKLIST FORM  
(To Be Completed By Lead Agency)

I. Background

1. Name of Proponent Kaiser Steel Corporation
2. Address and Phone Number of Proponent P.O. Box 5050  
Corporate Office Building (COB 9)  
Fontana, CA. 92335 (714) 350-5204
3. Date of Checklist Submitted 10/25/84
4. Agency Requiring Checklist City of Fontana
5. Name of Proposal, if applicable Amendment #4 to the Southwest Industrial Park & Specific Plan

II. Environmental Impacts

(Explanations of all "yes" and "maybe" answers are required on attached sheets.)

Yes   Maybe   No

I. Earth. Will the proposal result in:

- a. Unstable earth conditions or in changes in geologic substructures?               X
- b. Disruptions, displacements, compaction or overcovering of the soil?        X
- c. Change in topography or ground surface relief features?        X
- d. The destruction, covering or modification of any unique geologic or physical features?               X
- e. Any increase in wind or water erosion of soils, either on or off the site? X
- f. Changes in deposition or erosion of beach sands, or changes in siltation, deposition or erosion which may modify the channel of a river or stream or the bed of the ocean or any bay, inlet or lake?        X

	<u>Yes</u>	<u>Maybe</u>	<u>No</u>
g. Exposure of people or property to geologic hazards such as earthquakes, landslides, mudslides, ground failure, or similar hazards?	—	—	X
2. Air. Will the proposal result in:			
a. Substantial air emissions or deterioration of ambient air quality?	—	X	—
b. The creation of objectionable odors?	—	X	—
c. Alteration of air movement, moisture, or temperature, or any change in climate, either locally or regionally?	—	—	X
3. Water. Will the proposal result in:			
a. Changes in currents, or the course of direction of water movements, in either marine or fresh waters?	—	—	X
b. Changes in absorption rates, drainage patterns, or the rate and amount of surface runoff?	X	—	—
c. Alterations to the course or flow of flood waters?	X	—	—
d. Change in the amount of surface water in any water body?	—	X	—
e. Discharge into surface waters, or in any alteration of surface water quality, including but not limited to temperature, dissolved oxygen or turbidity?	—	X	—
f. Alteration of the direction or rate of flow of ground waters?	—	X	—
g. Change in the quantity of ground waters, either through direct additions or withdrawals, or through interception of an aquifer by cuts or excavations?	—	X	—
h. Substantial reduction in the amount of water otherwise available for public water supplies?	—	X	—
i. Exposure of people or property to water related hazards such as flooding or tidal waves?	—	X	—

	<u>Yes</u>	<u>Maybe</u>	<u>No</u>
4. <b>Plant Life.</b> Will the proposal result in:			
a. Change in the diversity of species, or number of any species of plants (including trees, shrubs, grass, crops, and aquatic plants)?			X
b. Reduction of the numbers of any unique, rare or endangered species of plants?			X
c. Introduction of new species of plants into an area, or in a barrier to the normal replenishment of existing species?	X		
d. Reduction in acreage of any agricultural crop?			X
5. <b>Animal Life.</b> Will the proposal result in:			
a. Change in the diversity of species, or numbers of any species of animals (birds, land animals including reptiles, fish and shellfish, benthic organisms or insects)?			X
b. Reduction of the numbers of any unique, rare or endangered species of animals?			X
c. Introduction of new species of animals into an area, or result in a barrier to the migration or movement of animals?			X
d. Deterioration to existing fish or wildlife habitat?		X	
6. <b>Noise.</b> Will the proposal result in:	X		
a. Increases in existing noise levels?			
b. Exposure of people to severe noise levels?		X	
7. <b>Light and Glare.</b> Will the proposal produce new light or glare?		X	
8. <b>Land Use.</b> Will the proposal result in a substantial alteration of the present or planned land use of an area?			X
9. <b>Natural Resources.</b> Will the proposal result in:			
a. Increase in the rate of use of any natural resources?	X		

	<u>Yes</u>	<u>Maybe</u>	<u>No</u>
b. Substantial depletion of any nonrenewable natural resource?	—	X	—
10. Risk of Upset. Will the proposal involve:			
a. A risk of an explosion or the release of hazardous substances (including, but not limited to, oil, pesticides, chemicals or radiation) in the event of an accident or upset conditions?	—	X	—
b. Possible interference with an emergency response plan or an emergency evacuation plan?	—	—	X
11. Population. Will the proposal alter the location, distribution, density, or growth rate of the human population of an area?	—	—	X
12. Housing. Will the proposal affect existing housing, or create a demand for additional housing?	—	—	X
13. Transportation/Circulation. Will the proposal result in:			
a. Generation of substantial additional vehicular movement?	X	—	—
b. Effects on existing parking facilities, or demand for new parking?	X	—	—
c. Substantial impact upon existing transportation systems?	—	X	—
d. Alterations to present patterns of circulation or movement of people and/or goods?	X	—	—
e. Alterations to waterborne, rail or air traffic?	X	—	—
f. Increase in traffic hazards to motor vehicles, bicyclists or pedestrians?	—	X	—
14. Public Services. Will the proposal have an effect upon, or result in a need for new or altered governmental services in any of the following areas:			
a. Fire protection?	X	—	—
b. Police protection?	X	—	—
c. Schools?	—	—	X

	<u>Yes</u>	<u>Maybe</u>	<u>No</u>
d. Parks or other recreational facilities?	—	—	X
e. Maintenance of public facilities, including roads?	X	—	—
f. Other governmental services?	—	X	—
15. <b>Energy.</b> Will the proposal result in:			
a. Use of substantial amounts of fuel or energy?	—	X	—
b. Substantial increase in demand upon existing sources of energy, or require the development of new sources of energy?	—	X	—
16. <b>Utilities.</b> Will the proposal result in a need for new systems, or substantial alterations to the following utilities:			
a. Power or natural gas?	—	X	—
b. Communications systems?	—	X	—
c. Water?	X	—	—
d. Sewer or septic tanks?	X	—	—
e. Storm water drainage?	X	—	—
f. Solid waste and disposal?	X	—	—
17. <b>Human Health.</b> Will the proposal result in:			X
a. Creation of any health hazard or potential health hazard (excluding mental health)?	—	—	—
b. Exposure of people to potential health hazards?	—	—	X
18. <b>Aesthetics.</b> Will the proposal result in the obstruction of any scenic vista or view open to the public, or will the proposal result in the creation of an aesthetically offensive site open to public view?			X
19. <b>Recreation.</b> Will the proposal result in an impact upon the quality or quantity of existing recreational opportunities?			X
20. <b>Cultural Resources.</b>			
a. Will the proposal result in the alteration of or the destruction of a prehistoric or historic archaeological site?	—	X	—

	<u>Yes</u>	<u>Maybe</u>	<u>No</u>
b. Will the proposal result in adverse physical or aesthetic effects to a prehistoric or historic building, structure, or object?		X	
c. Does the proposal have the potential to cause a physical change which would affect unique ethnic cultural values?			X
d. Will the proposal restrict existing religious or sacred uses within the potential impact area?			X

#### 21. Mandatory Findings of Significance.

a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?			X
b. Does the project have the potential to achieve short-term, to the disadvantage of long-term, environmental goals? (A short-term impact on the environment is one which occurs in a relatively brief, definitive period of time while long-term impacts will endure well into the future.)		X	
c. Does the project have impacts which are individually limited, but cumulatively considerable? (A project may impact on two or more separate resources where the impact on each resource is relatively small, but where the effect of the total of those impacts on the environment is significant.)		X	
d. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?		X	

#### III. Discussion of Environmental Evaluation

#### IV. Determination (To be completed by the Lead Agency)

On the basis of this initial evaluation:

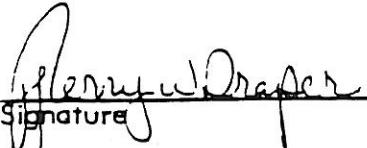
I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. A NEGATIVE DECLARATION WILL BE PREPARED.

I find the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

10/29/84

Date

  
Signature

For City of Fontana

(Note: This is only a suggested form. Public agencies are free to devise their own format for initial studies.)

## OFFICE OF PLANNING AND RESEARCH

400 TENTH STREET  
SACRAMENTO, CA 95814

DATE: July 28, 1986

TO: Reviewing Agencies

RE: The City of Fontana's NOP for  
Kaiser Specific Plan  
SCH# 86072803

Attached for your comment is the City of Fontana's Notice of Preparation of a draft Environmental Impact Report (EIR) for the Kaiser Specific Plan.

Responsible agencies must transmit their concerns and comments on the scope and content of the EIR, focusing on specific information related to their own statutory responsibility, within 30 days of receipt of this notice. We encourage commenting agencies to respond to this notice and express their concerns early in the environmental review process.

Please direct your comments to:

Laurie Lawhorn  
City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335

with a copy to the Office of Planning and Research. Please refer to the SCI number noted above in all correspondence concerning this project.

If you have any questions about the review process, call Peggy Osborn at 916/445-0613.

Sincerely,

John B. Chanian  
Chief Deputy Director

Attachments

cc: Laurie Lawhorn

S - Sent by Land Agency

Anne Geraghty  
Air Resources Board  
1111 S Street  
Sacramento, CA 95814  
916/322-3161

Barbara Kertow  
Dept. of Boating & Waterways  
1629 S Street  
Sacramento, CA 95814  
916/322-3488

Gary L. Holloway  
California Coastal Commission  
631 Howard Street, 4th Floor  
San Francisco, CA 94103  
415/547-3555

Kim Brantetter  
California Energy Commission  
1518 Ninth Street, P.O. 200  
Sacramento, CA 95814  
916/324-3573

Earl Tucker  
Caltrans - Division of Aeronautics  
1120 N Street  
Sacramento, CA 95814  
916/322-3945

Mary Kelly  
Caltrans - Planning  
1120 N Street  
Sacramento, CA 95814  
916/322-7222

Dennis O'Bryant  
Dept. of Conservation  
1419 Ninth Street, Room 103-2  
Sacramento, CA 95814  
916/322-3873

Div. of Mines and Geology  
 Div. of Oil and Gas  
 Land Resources Protect. Unit

Mike Lebowis  
Dept. of Food and Agriculture  
1420 N Street, Room 104  
Sacramento, CA 95814  
916/322-1292

Dennis O'Rourke  
Dept. of Forestry  
1418 Ninth Street, Room 103-2  
Sacramento, CA 95814  
916/322-7413

James Ellingson  
Dept. of General Services  
1423 Ninth Street  
Sacramento, CA 95814  
916/324-3209

Dennis Kiser  
Dept. of Health  
714 P Street, Room 1033  
Sacramento, CA 95814  
916/445-1248

X - Sent by Clearinghouse

Bill Murphy  
Dept. of Housing & Community Dev't.  
921 - 10th Street, 5th Floor  
Sacramento, CA 95814-  
916/324-3857

Paige Tally  
Native American Heritage Comm.  
915 Capitol Mall, Room 288  
Sacramento, CA 95814  
916/322-7791

Sam Eisenberg  
Office of Historic  
Preservation  
P.O. Box 2290  
Sacramento, CA 95811  
916/445-3005

James M. Doyle  
Dept. of Parks and Recreation  
P.O. Box 2290  
Sacramento, CA 95811  
916/324-3421

Mike Barnes  
Public Utilities Commission  
928 J Street, Suite 1400  
Sacramento, CA 95814  
916/322-7313

Mike Stewart  
Public Works Board  
1025 P Street, 4th Floor  
Sacramento, CA 95814  
916/445-3132

Mel Schwartz  
Reclamation Board  
1415 Ninth Street  
Sacramento, CA 95814  
916/445-2458

Morris Williams  
S.F. Bay Conservation & Dev't. Comm.  
30 7th Street Avenue, Room 3011  
San Francisco, CA 94102  
415/357-3686

Eric Ulmer  
Calif. Parks Management Board  
1020 Ninth Street, Room 100  
Sacramento, CA 95814  
916/322-2674

Ted Pilkington  
State Lands Commission  
1307 - 14th Street  
Sacramento, CA 95814  
916/322-7112

Ted Pilkington  
Dept. of Water Resources  
1418 Ninth Street  
Sacramento, CA 95814  
916/445-7416

Ted Pilkington  
State Coastal Conservancy  
1510 Broadway, Suite 1100  
Oakland, CA 94612  
415/484-1013



# CITY OF RANCHO CUCAMONGA

Mayor Jeffrey King

Commissioners

Charles J. Buquet II  
Richard M. Dahl

Jon D. Mikels  
Pamela J. Wright



August 1, 1986

Laurie Lawhorn  
Planning Department  
City of Fontana  
8353 Sierra Avenue  
Fontana, California 92335

SUBJECT: NOTICE OF PREPARATION - KAISER SPECIFIC PLAN DRAFT EIR

Dear Ms. Lawhorn:

This is in response to the Notice of Preparation for the Kaiser Specific Plan Draft EIR received by the City of Rancho Cucamonga on July 28, 1986. While the City has no comments to make at this time, we would like to be placed upon the EIR distribution list so that we may receive copies of the Draft and Final EIRs.

Thank you for your consideration.

Sincerely,

COMMUNITY DEVELOPMENT DEPARTMENT  
PLANNING DIVISION

Lisa Wininger  
Assistant Planner

LW:ko

# ENGINEERING CONTRACT SERVICES DEPARTMENT

25 East Third Street • San Bernardino, CA 92415-0834

COUNTY OF SAN BERNARDINO  
ENVIRONMENTAL  
PUBLIC WORKS AGENCY

TIM KELLY  
Director

REAL PROPERTY DIVISION  
WASTE MANAGEMENT  
WASTE-TO-ENERGY MANAGEMENT



August 4, 1986

File: SAR 278/18

City of Fontana  
8353 Sierra Avenue  
Fontana, California 92335

Attention: Ms. Laurie Lawhorn

Re: **NOTICE OF PREPARATION**  
Draft Environmental  
Impact Report-  
Kaiser Specific Plan

Dear Ms. Lawhorn:

We would like to take this opportunity to acknowledge receipt of your "Notice of Preparation" dated July 18, 1986. The following is offered for your information:

1. The Milliken Sanitary Landfill is the site that would best serve the Project Area. This Site is located on Milliken Avenue at the northwest corner of Mission Boulevard and Milliken Avenue.
2. The Landfill is expected to reach capacity in 1995.
3. The Milliken Landfill has the capacity to serve the **Specific Plan Area** although additional landfill equipment and associated personnel may be required to meet the increased service demand. The long range impact, however, may be the shortening of the landfill life expectancy to some degree.

The following is current waste generation factors and is offered for your information:

Residential Wastes - 3.25 lbs per capita per day

Commercial Wastes - 10 lbs per employee per day

Industrial Wastes - 22.5 lbs per employee per day

*1786*  
City of Montclair  
Page 2  
August 4, 1986

If there are any further questions, or if additional information is needed, please contact the undersigned at, Area Code (714) 387-2765.

Very truly yours,

**D. MAX BUCHANAN, P.E.**  
Solid Waste Operations Manager

BY: *DMB*  
**DENNIS. M. BROWN**  
Solid Waste Management  
Planning/Engineering

DMB:DMB:eh

SOUTHERN CALIFORNIA  COMPANY

1981 LUGONIA AVENUE, REDLANDS, CALIFORNIA

MAILING ADDRESS, P O BOX 3003, REDLANDS, CALIFORNIA 92373-0308

July 31, 1986

City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335

Attention: Laurie Lawhorn

RE: Kaiser Specific Plan



This letter is not to be interpreted as a contractual commitment to serve the proposed project; but only as an information service. Its intent is to notify you that the Southern California Gas Company has facilities in the area where the above named project is proposed. Gas service to the project would be provided from the nearest existing gas mains without any significant impact on the environment. The service would be in accordance with the Company's policies and extension rules on file with the California Public Utilities Commission at the time contractual arrangements are made.

The availability of natural gas service, as set forth in this letter, is based upon present conditions of gas supply and regulatory policies. As a public utility, the Southern California Gas Company is under the jurisdiction of the California Public Utilities Commission. We can also be affected by actions of federal regulatory agencies. Should these agencies take any action which affects gas supply or the condition under which service is available, gas service will be provided in accordance with revised conditions.

We have developed several programs which are available, upon request, to provide assistance in selecting the most effective applications of energy conservation techniques for a particular project. If you desire further information on any of our energy conservation programs, please contact our Area Market Services Manager, P.O. Box 3003, Redlands, CA 92373, phone (714) 798-7760.

Very truly yours,

*Frank Terry*  
Frank Terry  
Technical Supervisor

Attachment  
cc: Ed Patterson

CONSTRUCTION EMISSIONS  
Emission Factors for Heavy-Duty  
Diesel-Powered Construction Equipment

Type Of Equipment	POLLUTANT (gm/hr)				
	Carbon Monoxide	Exhaust Hydrocarbons	Nitrogen Oxides	Sulfur Oxides	Particulate
Tracktype Tractor	157.01	55.06	570.70	62.3	50.7
Wheeled Tractor	1622.77	85.26	575.84	40.9	61.5
Wheeled <sup>a)</sup> Dozer	--	--	--	158	75
Scraper	568.19	128.15	1740.74	210	184
Motor Grader	68.46	18.07	324.43	39	27.7
Wheeled Loader	259.58	113.17	858.19	82.5	77.9
Tracktype Loader	91.15	44.55	375.22	34.4	26.4
Off-Highway Truck	816.81	86.84	1889.16	206	116
Roller	137.97	30.58	392.9	30.5	22.7
Miscellaneous	306.37	69.35	767.3	64.7	63.2

<sup>a)</sup> The wheeled dozer HC/CO/NOx emissions are included in the off-highway truck category. Source: EPA-AP-42, Volume II, September 1985

Emission Factors for Heavy-Duty  
Gasoline-Powered Construction Equipment

Type of Equipment	POLLUTANT (gm/hr)						
	Carbon Mono-oxide	Exhaust Hydro-carbons	Evapo-rative Hydro-carbons	Crank-case Hydro-carbons	Nitrogen Oxides	Sulfur Dioxide	Partic-ulates
Wheeled Tractor	4320	164	30.9	32.6	195	7.03	10.9
Motor Grader	5490	186	30.0	37.1	145	7.59	9.4
Wheeled Loader	7060	241	29.7	48.2	235	10.6	13.5
Roller	6080	277	28.2	55.5	164	8.38	11.8
Miscellaneous	7720	254	25.4	50.7	187	10.6	11.7

Dust Emissions

1.2 tons per acre are of construction per month of activity, or 110 lbs. per acre per working day.

Source for all above data: EPA-AP-42, Volume II, September 1985

## OFFICE OF PLANNING AND RESEARCH

100 TENTH STREET  
SACRAMENTO, CA 95814



December 24, 1986

Laurie Lawhorn  
City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335



Subject: Fontana Gateway Specific Plan  
SCH# 86072803

Dear Ms. Lawhorn:

The State Clearinghouse submitted the above named draft Environmental Impact Report (EIR) to selected state agencies for review. The review period is closed and the comments of the individual agency(ies) is(are) enclosed. Also, on the enclosed Notice of Completion, the Clearinghouse has checked which agencies have commented. Please review the Notice of Completion to ensure that your comment package is complete. If the package is not in order, please notify the State Clearinghouse immediately. Your eight digit State Clearinghouse number should be used so that we may reply promptly.

Please note that recent legislation requires that a responsible agency or other public agency shall only make substantive comments on a project which are within the area of the agency's expertise or which relate to activities which that agency must carry out or approve. (AB 2583, Ch. 151<sup>4</sup>, Stats. 1984.)

These comments are forwarded for your use in preparing your final EIR. If you need more information or clarification, we suggest you contact the commenting agency at your earliest convenience.

Please contact Peggy Osborn at 916/445-0613 if you have any questions regarding the environmental review process.

Sincerely,

John B. Chanian  
Chief Deputy Director  
Office of Planning and Research

cc: Resources Agency

Enclosures

11/15

35072803

(بِسْمِ اللَّهِ الرَّحْمَنِ الرَّحِيمِ ۖ)

DEC 1 7 1986

# RIVERSIDE COUNTY PLANNING DEPARTMENT

November 24, 1986

Ms. Laurie Lawhorn, Assistant Director  
Advance Planning  
City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335



RE: Fontana Gateway Specific Plan and EIR

Dear Ms. Lawhorn:

The Riverside County Planning Department has reviewed the above-referenced project and, while having no comments at this time on the specific plan itself, would like to offer the following comments in regards to the EIR:

1. The Proposed Project: Statistical Summary table has an incorrect total under the "acres" column. The correct total is 790. 2
2. The Biotic Resources Section (Section IV-D) is incomplete. Our copy does not have page IV-D-2 or any additional pages which may belong in this section. 3
3. It is stated on page IV-E-3 that "project-related traffic will comprise a relatively small portion of traffic on perimeter roadways -- generally between 13 and 35 percent...." It would be hard to consider 30-35 percent a "small portion." This increment is sizeable enough to warrant special consideration in the noise and circulation sections of the EIR, and should be addressed as such. 4
4. The Noise Impact Section of the EIR states that "development of the proposed project will not result in significant changes in traffic levels and will thus not increase projected noise levels...." However, the Circulation Section states that "traffic on local roadways will increase as the result of development of the proposed project." Since the Specific Plan calls for the widening of the local roadways in order to accommodate increased traffic, it is reasonable to expect noise levels to increase in proportion to the increase in traffic. This impact should be addressed in the Noise Impact Section. 5
5. Page IV-E-5 makes reference to the light industrial uses in the eastern portion of the development and that there will be no noise impact to offsite areas. However, there is no mention of the industrial uses in the western portion, nor is there any mention of the impact that this western portion may have on offsite areas. 6

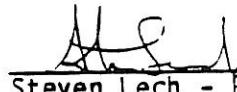
November 24, 1986

6. The presence of objectionable odors may be a significant impact because of the residential land uses near by. The Safety/Muisance Section should be expanded to include possible impacts to residents and a discussion of appropriate mitigation measures. 7
7. Page IV-G-2 references the "Riverside Avenue/Etiwanda Avenue Interchange." It may be necessary to change this to read "Etiwanda Avenue/Mission Boulevard Interchange" to be consistent with existing freeway signs and to alleviate confusion. 8
8. The third paragraph under Section 3a on page IV-H-2 and the fourth paragraph under Section 3b on page IV-H-3 are both mitigation measures listed under the impact heading and should be moved to Section 4 on page IV-H-4. 9
9. The part of the first paragraph under the fire protection heading on page IV-H-4 beginning with "However" is an impact, not mitigation. This should be moved to the Fire Protection Section under the Environmental Impacts of Public Services. 10
10. Reference is made on page V-C-1 to table V-C-A. However, the table on this page is labeled V-D-A. This will need to be corrected. 11

Thank you for the opportunity to comment on this project. If you have any questions, please feel free to call me at (714) 787-2297.

Very truly yours,

RIVERSIDE COUNTY PLANNING DEPARTMENT  
Roger S. Streeter - Planning Director



---

Steven Lech - Planner

SL:rd



South Coast  
AIR QUALITY MANAGEMENT DISTRICT  
9150 FLAIR DRIVE, EL MONTE, CA 91731 (818) 572-6200



December 10, 1986

Ms. Laurie Lawhorn  
Advanced Planning  
City of Fontana  
8353 Sierra Ave.  
Fontana, CA 92335

Dear Ms. Lawhorn:

Draft Environmental Impact Report  
Fontana Gateway Specific Plan, Fontana, CA  
SCAQMD #B61112E

We have the following comments on the above project:

1. The air quality data should be updated to include 1985 air quality information. I have enclosed a copy of the "Air Quality Data, 1985, South Coast Air Quality Management District" for your assistance. 12
2. We would like to see a list of the existing industrial facilities in the industrial park. Additional industrial development will require permits from the District. You should contact the District's Engineering Division, (818) 572-6431, for further information. 13
3. Emissions from construction-related activities should be calculated. I have included a copy of the emission factors for heavy duty diesel and gasoline-powered construction equipment that you should use to calculate the emissions. 14
4. Page VII-A-1 indicates that impacts on air quality are considered significant irreversible environmental changes. We suggest that any mitigation measures available to reduce the air quality impacts of the proposed plan be considered for inclusion in the final EIR. In addition to the measures proposed to mitigate the impacts from increased traffic, we suggest you consider the inclusion of a transportation management program. Such a program could include the following: 15

December 10, 1986

- a. carpooling/vanpooling;
- b. financial incentives for ridesharing;
- c. subsidization of parking for ridesharing employees;
- d. flexible or modified work hours for ridesharing employees;
- e. full or partial subsidization of carpooling, vanpooling, buspooling, or use of public transit; and
- f. transit line hook-up with existing transit lines, including bus stops and bus shelters;
- g. assignment of preferential or free parking for vehicles used for ridesharing.

5. Mitigation measures should be proposed to minimize the impacts on air quality during the construction phase of the proposed development. We recommend the following measures be included in the final Environmental Impact Report:

- a. controlling fugitive dust by regular watering, paving construction roads or other dust preventive measures in order to comply with District Rule 403,
- b. maintaining equipment engines in proper tune,
- c. using water trucks or sprinkler systems to keep all areas of vehicle movement damp enough to prevent dust raised when leaving the site,
- d. wetting down areas in the late morning and after work is completed for the day,
- e. phasing and scheduling construction activities to level emission peaks and,
- f. discontinuing construction activities during first and second stage smog alerts.

If you have any questions please contact me at (818) 572-6405.

Very truly yours,



Gay Muttersbach  
Air Quality Planner  
Planning Division

GM:ak

Enclosures

16

## Memorandum

To : State Clearinghouse  
Office of Planning & Research  
1400 10th Street  
Sacramento, CA 95814

Date: December 11, 1986

File: 08-SBd-10-11.1

SCH# 86072803

Attention: Peggy Osborn

From : DEPARTMENT OF TRANSPORTATION  
District 8

Subject: DEIR - Fontana Gateway Specific Plan

We have reviewed the above-referenced document and request consideration of the following:

o We will not permit any signs or towers on State right of way stating Gateway Industrial Park as shown on signage plan Figure 11 Specific Plan. 17

o Traffic section should address cumulative plus project volumes impact on Route 10 and Etiwanda peak hour volumes including possible mitigation measures. 18

o As other potential developments in this area are under consideration, the County should consider requiring a developer's fee as a means of financing the eventual necessary improvements to the transportation system. 19

Should any work be required within State highway right of way, Caltrans would be a responsible agency and may require that certain mitigation measures be provided as a condition of permit issuance. 20

We urge early and continuous liaison with Caltrans on proposed plans as they affect State highways. 21

If you have any questions, please contact Marie J. Petry at (714) 383-4541.

*S. Visbal*

GUY J. VISBAL  
Chief, Transportation Planning  
Branch A

MP:lb  
cc: FDHusum, Plan Coordination Unit, DOTP  
File (2)



Tack S. Joe  
557-9884

December 23, 1986

183-36/EIR



Peggy L. Osborn  
Office of Planning & Research  
1400 Tenth Street, Room 121  
Sacramento, CA 95814

Dear Miss Osborn:

This is in response to the draft Environmental Impact Report (EIR) for the Fontana Gateway-Specific Plan, SCH #86072803.

After reviewing this proposal, the staff has the following comments:

1. Under the mitigation measures for the "Circulation" impacts it is stated that issuance of a certificate of occupancy should be issued only after the installation or assurance of an adequate internal roadway system. The staff would ask, why only the internal roadway system. We believe it should be issued only after improvements are completed for all major roadways being affected in the area, especially Etiwanda Avenue, north of Slover Avenue. 22
2. We note that Etiwanda Avenue, north of Slover Avenue will be the most impacted section of roadway yet this is the only section of roadway where no improvements are actually proposed. Since the L.O.S. will be "E", it would appear that this section of roadway should be of prime concern. 23
3. The Slover Avenue grade crossing, P.U.C. B-527.61-C, has 2 Standard No. 1R Crossing signs (General Order 75-C) which are passive devices as warning devices at the crossing. If this specific plan is implemented, the Commission will probably require improvement of these passive devices to automatic warning devices. 24
4. We would remind the City that any new railroad-highway crossing will require permit/decision authority from the Commission. 25

Peggy L. Osborn  
December 23, 1986  
Page 2

5. Page 18 (10-21-86 report) under 3.3.2.6 Access indicates future access to parcels located between I-10 and the railroad tracks will be through access from Slover Avenue. The staff will strongly object to any proposed access traversing the railroad tracks unless it is grade separated. We take this position in view of the numerous train movements over this area and the significant safety hazards and accident potential that any at-grade crossing would pose over this line.

6. Figure 11 (10-21-86 report) "Landscape/Signage Plan", shows the proposed landscaping along the roadways. The staff would point out that all landscaping must be such as not to obscure and obstruct a motorist view of a grade crossing or of any approaching train.

26

27

Very truly yours,

*DR*  
DONALD R. CHEW, Supervisor  
Transportation Projects Section  
Railroad Operations & Safety Branch  
Transportation Division

cc: Laurie Lawhorn  
City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335

CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD  
SANTA ANA REGION  
6809 INDIANA AVENUE, SUITE 200  
RIVERSIDE, CALIFORNIA 92506  
PHONE: (714) 782-4130



December 19, 1986



Ms. Laurie Lawhorn  
City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335

DEIR: FONTANA GATEWAY - SPECIFIC PLAN AND DEIR, SCH #86072803

Dear Ms. Lawhorn:

We have reviewed the DEIR and specific Plan for the Fontana Gateway project.

We are concerned about the potential impacts on surface and ground water quality of industrial activities in the plan area. Specifically, we are concerned about the potential impacts of any toxics handling and/or disposal associated with these activities. As specific projects are proposed, impacts of toxic substances handling and/or disposal must be assessed.

28

In addition, the project proponent should be advised that the Environmental Protection Agency is currently developing regulation for "non-point" source discharges, including storm-water runoff from facilities such as that proposed. In order to comply with these regulations, the EPA may require you that the proponent file for a permit at a later date. Should you have any questions regarding these new regulations, please contact Hisam Baqai of our Regulations Section.

29

Sincerely,

Nancy A. Olson  
Sanitary Engineering Technician

cc: The Resources Agency - Dr. Gordon F. Snow

NAO:csi

LAW OFFICES  
**BURKE, WILLIAMS & SORENSEN**

ONE WILSHIRE BUILDING

MARTIN J. BURKE\*  
GEORGE W. TACKABURY\*  
JAMES T. BRADSHAW, JR.\*  
MARK C. ALLEN, JR.\*  
RICHARD R. TERZIAN\*  
MARTIN L. BURKE\*  
CARL K. NEWTON\*  
J. ROBERT FLANDRICK\*  
EDWARD M. FOX\*  
DENNIS R. BURKE\*  
LELAND C. DOLLEY\*  
COLIN LEONARD\*  
BRIAN J. SEERY\*  
THOMAS J. SEELEY\*  
NEIL F. YEAGER\*  
BRIAN A. PIERK\*  
KATHERINE E. STONE\*  
CHARLES M. CALDERON\*  
PETER M. THORSON\*  
THOMAS H. DOWNEY  
HAROLD A. BRIDGES

CHERYL J. XANIS  
RAYMOND J. FUENTES  
DON G. KIRCHER  
VIRGINIA R. PESOLA  
S. PAUL BRUGUERA  
MICHELE R. VADON  
B. DERIK STRATEMA  
SCOTT F. FIELD  
JOHN W. BISLIMER  
BENJAMIN S. KAUFMAN  
MICHAEL J. LONG  
ELLEN M. BENEDER  
GREGORY A. DOCIMO  
CYNTHIA D. GOEMA  
KEVIN S. MILLS  
DEBORAH J. FOX  
CAROL A. SCHWAB  
LISA E. KRAMITZ  
MARK S. BLACKMAN  
D. COLETTE GONZALEZ

624 SOUTH GRAND AVENUE, 11TH FLOOR  
LOS ANGELES, CALIFORNIA 90017  
(213) 623-1900

TELECOPIER: (213) 623-6297  
TELEX: 671-1271  
CABLE ADDRESS  
SWSLA UW

VENTURA OFFICE  
980 COUNTY SQUARE DRIVE  
SUITE 207  
VENTURA, CALIFORNIA 93003  
(805) 644-7480

HARRY C. WILLIAMS  
(818) 987-1987  
ROTH M. SORENSEN  
(818) 987-1983

OF COUNSEL  
DWIGHT A. NEWELL

December 19, 1986

\*PROFESSIONAL CORPORATION

Mr. Neil A. Stone  
Development Agency Director  
City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335

Re: Fontana Gateway Specific Plan

Dear Mr. Stone:

Our office represents the Adaya Family Trust, an owner of property within the 750-acre project known as Fontana Gateway Specific Plan.

Wednesday we received your letter of December 15, 1986 regarding the scheduled meetings for Thursday, December 18, and Friday, December 19, to discuss the above-referenced project. Due to the shortness of notice, we were unable to attend either meeting.

We did meet on Wednesday, December 17, with Mr. James Robertson of the Realvest Company and Gary Mitchell of Gary Mitchell & Associates. They provided us with a copy of portions of the Specific Plan and, in general, discussed the project with us.

While we have not reviewed the EIR and have had insufficient opportunity to study the Specific Plan, we are initially concerned that the proposed widening of Slover Avenue will necessitate the taking of excessive property from the affected property owners and, due to the proposed width of the street, may adversely impact ingress and egress to affected property owners. It is our preliminary view that Slover Avenue need be no wider than 85 feet. It also appears that the setback from the street could be eliminated

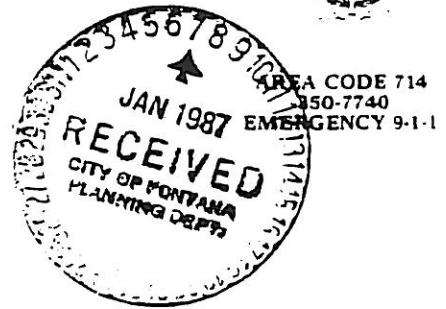


# Fontana Police Department



BEN L. ABERNATHY  
Chief of Police

17005 UPLAND AVENUE  
FONTANA, CA 92335



## MEMORANDUM

TO: Laurie Lawhorn, Assistant Director  
Advanced Planning

FROM: Ben Abernathy, Chief of Police

SUBJECT: PROJECTED POLICE SERVICES/FONTANA GATEWAY EIR  
(dated October, 1986)

DATE: December 31, 1986

### Introduction

The following information is being submitted as a response to projected Police services outlined in Chapter four (4) pp H-2, of the Fontana Gateway Environmental Impact Report (EIR), dated October, 1986.

This letter addresses how many officers and support staff the department currently has, emergency response time, Police problems associated with the project, and Police costs to the project site.

### Current Service Levels

The station that will provide service to the project site is located at 17005 Upland Avenue, Fontana, CA 92335. The Fontana Police Department currently has 71 sworn peace officer positions and 42 non-sworn civilian positions. All personnel are assigned to work on an eight hour, five-day work schedule. Depending on the particular position, personnel are on duty 24 hours each day.

31

### Emergency Response Time

Response time to the Gateway project site will be difficult to determine. The difficulty arises because response time depends on what the beat officer is assigned to at the time the call for service is placed. The department employs a six (6) beat system. Beat Six (6) will encompass the proposed project site and all the territory south of Valley Boulevard. Currently, one officer is assigned to handle "calls for service" within Beat Six.

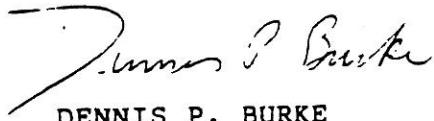
32

Mr. Neil A. Stone  
December 19, 1986  
Page 2

and still provide a more than adequate greenbelt area under normal building code requirements.

As indicated above, our review of the project has been very limited. However, it is our preliminary view that, with certain modifications, the adoption of the Specific Plan for the subject area is in the best interests of the City and the affected property owners.

Yours very truly,



DENNIS P. BURKE

DPB:rs

cc: Adaya Family Trust

VIA MESSENGER

STATE OF CALIFORNIA—BUSINESS AND TRANSPORTATION AGENCY

DEPARTMENT OF TRANSPORTATION

DISTRICT 8, P.O. BOX 231  
SAN BERNARDINO, CALIFORNIA 92402

August 7, 1986

08-SBD-10-11.13/12.14  
SCH # 86072803



Ms. Laurie Lawhorn  
Assistant Director  
Advance Planning  
City of Fontana  
P. O. Box 518  
Fontana, CA 92335

Dear Ms. Lawhorn:

This is in response to the Notice of Preparation of a Draft Environmental Impact Report for Kaiser Specific Plan in the City of Fontana.

We would appreciate the opportunity to review and comment on the proposed DEIR in order to evaluate possible impacts to the transportation system, particularly State Route 10.

Consideration should be given to the cumulative effects that continued development in the area will have on the transportation system from a "worst case" viewpoint. Discussion of the impacts to the transportation system should include traffic growth, traffic safety, drainage, and those associated with the construction, maintenance, and operation of any anticipated highway improvements. Mitigation for traffic impacts should consider the use of carpooling/vanpooling, public transit, the reservation of areas for Park and Ride facilities, and accommodations for both pedestrians and bicycles. Any industrial development should consider the use of flex-time work scheduling and rideshare coordinators. Costs related to any transportation improvements, potential for funding, and sources of funds should be discussed.

Should any work be required within State highway right of way, Caltrans would be a responsible agency and may require that certain mitigation measures be provided as a condition of permit issuance.

We urge early and continuous liaison with Caltrans on proposed plans as they affect State highways.

If you have any questions, please contact Marie J. Petry at (714) 383-4541.

Very truly yours,

GUY G. VISBAL  
Chief, Transportation Planning  
Branch A

SAN BERNARDINO  
COUNTY  
FIRE AGENCY



ADMINISTRATIVE OFFICES  
11611 Industry Avenue  
Fontana, California 92335  
(714) 823-2681

August 8, 1986

City of Fontana  
Attn: Laurie Lawhorn  
8353 Sierra Avenue  
Fontana, CA. 92335



Dear Ms. Lawhorn,

The following is a list of proposed mitigation measure required for fire protection for the Kaiser Specific Plan.

1. Implementation of requirements of the cities Fire Sprinkler Ordinance.
2. Approved water system capable of providing the required fire flow per Fire District Standards.
3. Adequate access per Fire Districts Standards.
4. Knox vaults on all occupancies using hazardous materials for the purpose of storing product data sheets and other related materials for Fire Department use only.
5. Provide specialized equipment and/or training to the local Fire Authority to mitigate any potential fire protection impacts created by any industrial process which exceeds the capability of the local fire authority. Fire protection mitigation measures shall be subject to the approval of the local fire authority.

If you have any questions, please feel free to call me at 823-2681.

Sincerely,

SAN BERNARDINO COUNTY FIRE AGENCY

*John B. Roberts*  
JOHN B. ROBERTS  
Division Chief

JRB:mlw

CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD  
SANTA ANA REGION  
6809 INDIANA AVENUE, SUITE 200  
RIVERSIDE, CALIFORNIA 92506  
PHONE: (714) 782-4130



July 19, 1986

Ms. Laurie Lawhorn  
City of Fontana  
8353 Sierra Avenue  
Fontana, CA 92335

NOP: KAISER SPECIFIC PLAN, #86072803

Dear Ms. Lawhorn:

We have reviewed the Notice of Preparation (NOP) for this project. In response to the statutory concerns of this office, the Draft EIR should address the following:

I. Water Quality

A. Potential impacts of the proposed project on surface and ground water quality:

- Construction activities (including grading) that could result in water quality impacts.
- Soil characteristics related to water quality (potential for erosion and subsequent siltation, increase or decrease in percolation).
- Impacts of waste generation, treatment and disposal.
- Impacts of toxic substances handling and/or disposal (if appropriate).
- Degree and seasonal variation of impact.

B. Mitigation of Adverse Impacts.

II. Water, Wastewater and Solid Waste Service

A. Water

- Availability of water for the proposed project.
- Existing infrastructure: location of water supply lines, tie-ins.
- Applications or permits required for water acquisition.
- Impact of calculated project demand on water supply.

July 19, 1986

B. Waste Disposal/Treatment

- Types and amounts of waste materials generated by project.
- Proposed waste treatment and disposal methods.
- Existing infrastructure:
  - \* treatment facilities: location, current capacity, treatment standards; master treatment facilities expansion plan (if appropriate)
  - \* treatment plant collector system: location of major trunk lines and tie-ins, current capacity
  - \* disposal facilities: location, capacity
  - \* location of discharge areas
- Applications or permits required to implement waste disposal.
- Impact of calculated project waste volume on capacity of existing and proposed treatment and disposal facilities.
- Need for pretreatment of industrial flows (if appropriate).

We look forward to review of the DEIR when it becomes available.

If you have any questions, please contact this office.

Sincerely,



Nancy A. Olson  
Sanitary Engineer Technician

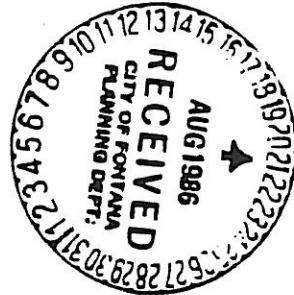
cc: Peggy Osborn, Office of Planning and Research

NAO:ww

August 20, 1986

Planning Department  
City of Fontana  
P. O. Box 518  
Fontana, California 92335

Attention: Laurie Lawhorn, Assistant Director,  
Advance Planning



CHINO BASIN  
MUNICIPAL  
WATER  
DISTRICT  
TELEPHONE (714) 987-1712

THEO T NOWAK  
GENERAL MANAGER

Subject: Notice of Preparation of a Draft Environmental Impact Report for the Kaiser Specific Plan

Thank you for the opportunity to respond to the Notice of Preparation of a Draft Environmental Impact Report for the Kaiser Specific Plan. Chino Basin Municipal Water District is involved in four primary service programs which may be affected by the project. These services include regional domestic wastewater treatment and disposal, non-reclaimable wastewater collection and disposal, importation of supplemental water and management of the Chino groundwater basin.

In order to assess the project's impacts on the District's service capabilities, the EIR should present a project-phasing schedule and the estimated wastewater flows generated from each phase. If the project intends to utilize the non-reclaimable waste system for the disposal of industrial strength wastewater, the EIR should also quantify this flow. It would also be helpful if the proposed connection point into the District's collection system(s) could be identified.

The District encourages the use of reclaimed water from our treatment facilities as a means of supplementing and extending potable water supplies. The project area is located within the planned reuse area of the District's proposed Regional Plant No. 4 facility. To assist the District in the long-range planning of a water reuse program, we would appreciate an assessment of the water reuse potential within the project area. Generally, water reuse is most suited to landscape irrigation and industrial process applications. If the EIR could quantify the potential for reuse with regard to these applications, it would be appreciated.

Two CBMWD facilities are located, or planned, in the vicinity of the project site. The District's non-reclaimable waste line is located on the west side of Etiwanda Avenue. The proposed alignment of the Fontana Interceptor Relief Sewer is situated within the Jurupa Avenue right-of-way. For additional information on these facilities, please contact Larry Miller at our administrative office.

Subject: Notice of Preparation of a Draft EIR for Kaiser  
Specific Plan, August 20, 1986

If additional information is necessary, contact the undersigned.

Sincerely,

Theo T. Nowak, General Manager  
CHINO BASIN MUNICIPAL WATER DISTRICT

*Mark N. Kinsey*  
By: Mark N. Kinsey, Planning and  
Administrative Services

MNK:sf

# RIVERSIDE COUNTY PLANNING DEPARTMENT

August 20, 1986

Ms. Laurie Lawhorn, Assistant Director  
Advanced Planning  
City of Fontana  
8353 Sierra Avenue  
Fontana, California 92335



RECEIVED  
AUG 27 1986

RE: Notice of Preparation of Draft  
Environmental Impact Report for  
the Kaiser Specific Plan.

Dear Ms. Lawhorn:

Thank you for giving us the opportunity to comment on the above-referenced document.

Etiwanda and Mulberry Avenues appear to be the main access routes to the Industrial Park. Therefore, the traffic impact analysis for the specific plan should include a focus on potential traffic impacts from Etiwanda Avenue southerly to Highway 60 and Mulberry Avenue through the residential areas to Country Village Road.

A discussion should also be included regarding any impacts on the quality for the surrounding areas. We would be interested in knowing what products are to be manufactured, the size of the plant, and the projected employment base.

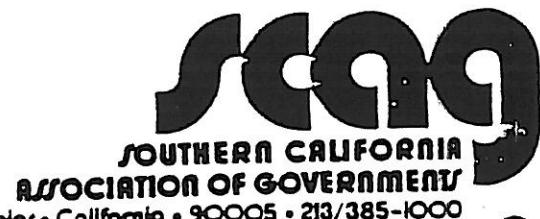
The San Sevaine Channel is a major flood control channel that bisects the project area. Consideration should be given to potential impacts to the channel in regards to material output from the industrial park. We would also recommend a drainage study in order to determine the location of the water runoff and any potential flood hazards.

Very truly yours,

RIVERSIDE COUNTY PLANNING DEPARTMENT  
Roger S. Streeter, Planning Director

*Jenifer J. Harper*  
Jenifer J. Harper, Associate Planner

JJH:jd



600 South Commonwealth Avenue • Suite 1000 • Los Angeles • California • 90005 • 213/385-1000



DATE: August 21, 1986

TO: Laurie Lawhorn  
Assistant Director, Advanced Planning  
City of Fontana  
8353 Sierra Ave.  
Fontana, CA 92335

FROM: Metropolitan Clearinghouse

SUBJECT: Kaiser Specific Plan  
SB-39753-NP

Thank you for submitting the Notice to Prepare the environmental document for the referenced project for SCAG review. SCAG staff does not have comments at this time but looks forward to reviewing the environmental document when available.

Sincerely,

*Wendy A. Murphy*  
WENDY A. MURPHY  
Clearinghouse Official

WAM:wp



600 South Commonwealth Avenue • Suite 1000 • Los Angeles • California • 90005 • 213/385-1000



DATE: August 21, 1986

TO: Laurie Lawhorn  
Assistant Director, Advanced Planning  
City of Fontana  
8353 Sierra Ave.  
Fontana, CA 92335

FROM: Metropolitan Clearinghouse

SUBJECT: Kaiser Specific Plan  
SB-39753-NP

Thank you for submitting the Notice to Prepare the environmental document for the referenced project for SCAG review. SCAG staff does not have comments at this time but looks forward to reviewing the environmental document when available.

Sincerely,

*Wendy A. Murphy*  
WENDY A. MURPHY  
Clearinghouse Official

WAM:wp



CALIFORNIA DIVISION OF  
MINES AND GEOLOGY



DMG  
NOTE 46

GUIDELINES FOR GEOLOGIC/SEISMIC CONSIDERATIONS  
IN ENVIRONMENTAL IMPACT REPORTS



The following guidelines were prepared by the Division of Mines and Geology with the cooperation of the State Water Resources Control Board to assist those who prepare and review environmental impact reports.

These guidelines will expedite the environmental review process by identifying the potential geologic problems and by providing a recognition of data needed for design analysis and mitigating measures. All statements should be documented by reference to material (including specific page and chart numbers) available to the public. Other statements should be considered as opinions and so stated.

1. CHECKLIST OF GEOLOGIC PROBLEMS FOR ENVIRONMENTAL IMPACT REPORTS

GEOLOGIC PROBLEMS		Could the project or mitigation avoid or minimize potential environmental problems		Is the environment contaminated or damaged by existing resource		
PROBLEM	ACTIVITY CAUSING PROBLEM	NO	YES	ENVIRONMENTAL PROBLEMS	NO	YES
EARTHQUAKE DAMAGE	Fault Movement					
	Liquefaction					
	Landslides					
	Differential Compaction/ Settling Settlement					
	Ground Rupture					
	Ground Shaking					
	Tsunami					
	Solutes					
LOSS OF MINERAL RESOURCES	Roof of Deposits					
	Deposits Covered by Changed Land-Use Conditions					
	Zoning Restrictions					
WASTE DISPOSAL PROBLEMS	Change in Groundwater Level					
	Deposit of Hazardous Materials					
	Penetration of Waste Materials					
SLOPE AND/OR FOUNDATION INSTABILITY	Landslides and Mudflows					
	Unstable Cut and Fill Slopes					
	Calcareous and Expansive Soil					
	Transform Stability					
EROSION, SEDIMENTATION, FLOODING	Creation of Graded Areas					
	Alteration of Runoff					
	Unprotected Drainage Ways					
	Increased Impervious Surfaces					
LAND SUBSIDENCE	Extraction of Groundwater, Gas, Oil, Geothermal Energy					
	Hydrocompaction, Poor Drainage					
	Land Fill					
VOLCANIC HAZARDS	Land Flow					
	Ash Fall					

(over)

STATE OF CALIFORNIA

THE RESOURCES AGENCY

DEPARTMENT OF CONSERVATION

For a list of geologic maps and reports available from the California Division of Mines and Geology, write to the California Division of Mines and Geology, P.O. Box 2880, Sacramento, CA 95812, or visit our District offices in SACRAMENTO, 2815 "O" Street, (916) 445-5716; SAN FRANCISCO, Room 2022, Ferry Building, (415) 567-0830; LOS ANGELES, Room 1086, 107 South Broadway, (213) 620-3560.

RECOMMENDED CHECKLIST OF GEOLOGIC REPORT ELEMENTS

DO NOT USE THIS FORM

CHECKLIST OF GEOLOGIC REPORT ELEMENTS

REPORT ELEMENT	YES	NO
A. General Description of Project Description of purpose of project. Description and map of area. Description and map of project office area.		
B. Geologic Element (water is checked) Are all the geologic problems mentioned? Are all the geologic problems adequately described?		
C. Mitigation Measures Are mitigation measures necessary? Is sufficient geologic information provided for the proper design of mitigating measures? Will the failure of mitigating measures cause an irrecoverable environmental impact?		
D. Alternatives Are alternatives necessary to reduce or prevent the irrecoverable environmental impact mentioned? Is sufficient geologic information provided for the proper assessment of alternatives? Are all the possible alternatives adequately described?		
E. Implementation of the Project Is the geologic report signed by a responsible geologist? Does the report provide the necessary regulations and performance criteria to implement the project?		

Required for interpretive geologic information.

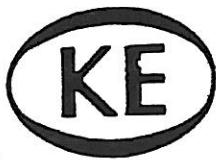
III. PUBLISHED REFERENCES (selected)

A. California Division of Mines and Geology Publications	7. Note No. 44. Recommended guidelines for preparing engineering geologic reports. 1974.	2. Bell, B.A. and Miller, R.D., 1971. Seismicity of northern and central California, 1850-1962. Bulletin of the Seismological Society of America, v. 61, no. 4.
1. Allora, J.T., et al., 1973. Urban geologic master plan for California. Bulletin 102.	8. Note No. 45. Recommended guidelines for preparing area recommendation plans. 1974.	3. California Department of Water Resources, 1964. Critical areas and fault movement investigation. Bulletin No. 116-2.
2. Greenholtz, R.W., 1974. Maximum credible rock massiveness from correlations in California. Map Sheet 22.	9. Potts, D.L., Reed, C.R., Tappanzeri, T.J.L., 1974. Earthquake Epicenter Map of California, showing events from 1850 through 1974.	4. Coffman, J.L. and von Hake, C.A., ed., 1973. Earthquake Activity of the United States: U.S. Department of Commerce, Publication 41-1.
3. Jennings, C.W., 1975. Fault Report 13 of California. GDM No. 1.	10. Reed, C.R., Tappanzeri, T.J.L. and Potts, D.L., 1974. Earthquake zoning of California, January 1, 1970-December 31, 1974 (interim).	5. Hansen, J.A., et al., 1973. Seismicity of the eastern California region, 1 January 1962 to 31 December 1972. California Institute of Technology, Contribution 230. Periodic updates to this are available.
4. Gephart, G.E., 1974. San Fernando, California, earthquake of 9 February 1971: Bulletin 102.		
5. Note No. 37. Guidelines to geologic/ seismic reports. 1973.		
6. Note No. 43. Recommended guidelines for determining the maximum credible and the maximum probable earthquakes. 1975.	B. Other Publications	
	1. Allor, C.R., et al., 1965. Relationship between rock mass and geologic structures in the southern California region. Bulletin of the Seismological Society of America, v. 55, no. 4.	

IV. PUBLIC AGENCIES WITH GEOLOGIC DATA

Source	Data Available			
	Seismology	Geology	Ground Water	Soils
Geology and Geophysics Department of California Universities	X	X	X	X
California Institute of Technology	X			
California Division of Mines and Geology (Sacramento, San Francisco, Los Angeles, CA)	X	X		
California Department of Water Resources (Sacramento, CA)		X		
California Department of Transportation (District Offices)				
County Soil & Water Conservation Districts				
County Engineer and Department of Building and Safety	X	X		X
County Highway Department				X
County Flood Control District				
U.S. Geological Survey (Menlo Park, CA)		X		
U.S. Corps of Engineers (District Engineers)		X		
U.S. Bureau of Reclamation (Regional Offices)		X		
U.S. Soil Conservation Service and Forest Service				X

**Appendix B**  
**Engineering and Infrastructure Analysis**  
**Krueper Engineering**



**KRUEPER ENGINEERING & ASSOCIATES, INC.**

CIVIL ENGINEERING • SURVEYING • ACCIDENT ANALYSIS AND CONSULTING

568 NORTH MT. VIEW AVENUE, SECOND FLOOR • SAN BERNARDINO, CALIFORNIA 92401-1291 • (714) 884-2159

File: 1163-11.03  
Date: June 23, 1986

Forma Planning Network  
195 North Euclid  
Ontario, CA 91762

Attention: Mr. Rick Gomez  
Senior Project Director

Regarding: KAISER SPECIFIC PLAN

Dear Rick:

Please find enclosed one copy of our Analysis/Summary regarding the questions of the infrastructure, within the Kaiser Specific Plan, that you posed in your May 19, 1986, letter. Should you require additional information or wish to discuss this further, please do not hesitate to contact our office.

Respectfully submitted,  
KRUEPER ENGINEERING & ASSOCIATES, INC.

A handwritten signature in black ink, appearing to read 'Melvin W. Thatcher, III, P.E.'

Melvin W. Thatcher, III, P.E.  
R.C.E. 39964

MWT:nes

Enclosure

cc: Mr. Glen Sams

**I. TRAFFIC**

**EXISTING AND PROPOSED ROADWAY CROSS SECTIONS**

- Interstate 10 Freeway - This eight lane facility is a major east/west corridor in this vicinity and is located approximately 0.3 miles north of the Specific Plan area. Access is provided to the study area via the interchange provided at Etiwanda Avenue. 1985 daily traffic volumes ranged from 90,000 to 100,000 vehicles.
- Interstate 15 Freeway - This eight lane facility is a major north/south corridor in this vicinity and is located approximately 1.5 miles west of the study area. An interchange has been provided at Jurupa Avenue, however, Jurupa Avenue is a noncontinuous roadway to and through the plan area, and access via Interstate 15 is not practical at the present time. When Jurupa Avenue is continued from Interstate 15 to Mulberry Avenue direct access to the freeway will be provided. 1985 daily traffic volumes ranged from 50,000 to 55,000 vehicles.
- State Route 60 Freeway - This six lane facility is a major east/west freeway in this vicinity and is located approximately 2.0 miles south of the study area. Access is provided to the study area via the "Riverside Avenue/Etiwanda Avenue" interchange. 1985 daily traffic volumes ranged from 46,000 to 49,500 vehicles.

- Etiwanda Avenue - Within the Specific Plan area Etiwanda Avenue is master planned to be a major highway, however, due to intermittent development various roadway cross sections exist between Interstate 10 and Jurupa Avenue. Figures 1 and 5 show the range of cross sections that exist along Etiwanda Avenue between Slover Avenue and Jurupa Avenue. These plates also compare the existing conditions to the proposed street sections showing ultimate pavement and right-of-way dimensions.

Existing traffic (1986) along Etiwanda Avenue was as follows: 9,900 vehicles per day north of Slover Avenue, 8,400 vehicles per day north of Jurupa Avenue and 6,700 vehicles per day south of Jurupa Avenue.

- Slover Avenue - Within the Specific Plan area Slover Avenue is master planned to be a major highway, due to intermittent development existing pavement widths range from 26 to 46 feet. This provides for two lanes of traffic (one in each direction) with widening at the signalized intersection of Slover Avenue and Etiwanda Avenue. Figures 2 and 3 show two typical existing cross sections along with the proposed street sections indicating ultimate pavement and right-of-way dimensions. Daily traffic volumes range from 3,800 vehicles per day near Etiwanda Avenue to 3,300 vehicles per day near Mulberry Avenue.

- Mulberry Avenue - South of Slover Avenue, Mulberry Avenue is basically a two lane roadway with an average pavement width of 26 feet, minor improvements have taken place between Slover Avenue and Jurupa Avenue. South of Jurupa Avenue, Mulberry Avenue is basically a three lane roadway (two northbound and one southbound) having an average pavement width of 48 feet. Figure 4 shows the typical existing cross section and the proposed street section indicating ultimate pavement and right-of-way dimensions. Daily traffic volumes range from 3,300 vehicles per day between Slover Avenue and Santa Ana Avenue, 3,000 vehicles per day between Santa Ana Avenue and Jurupa Avenue and 4,100 vehicles per day south of Jurupa Avenue.

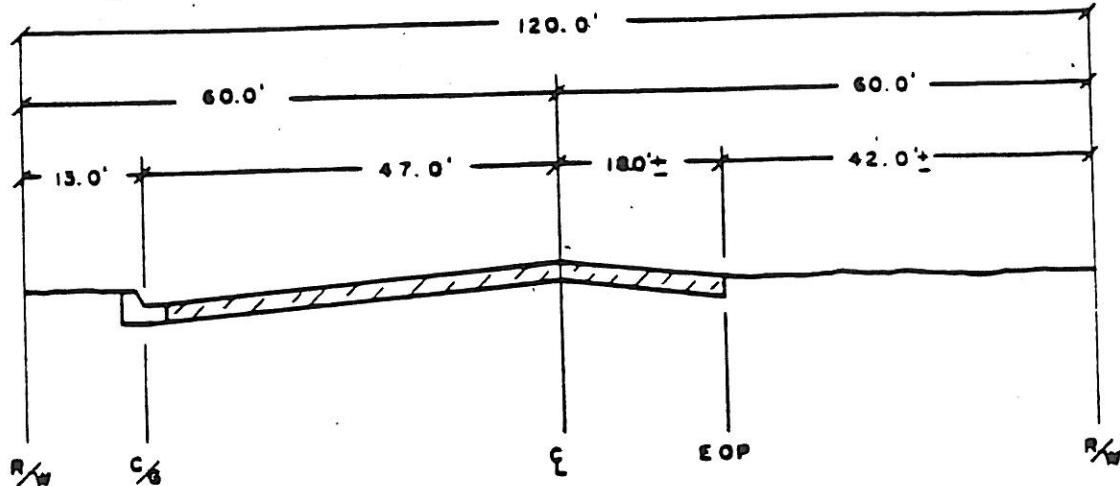
- Jurupa Avenue - East of Etiwanda Avenue to approximately 400 feet west of the San Savaine Channel, Jurupa Avenue has been fully improved to a 80 foot pavement section. Daily traffic volumes along this section of roadway amount to 200 vehicles per day. The typical existing cross section and proposed cross section are depicted on Figure 6.

Jurupa Avenue, west of Mulberry Avenue to a point 400 feet west of the San Savaine Channel has not been constructed and does not exist at the present time. Figure 7 shows the proposed cross section for the section of roadway.

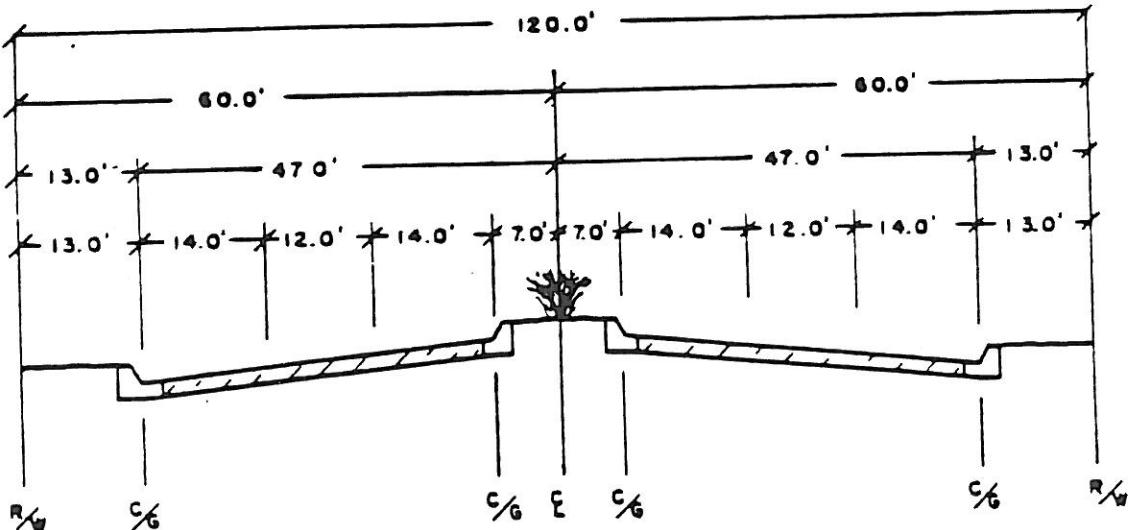
Jurupa Avenue, just east of Mulberry Avenue has been partially improved to an average pavement section of 46 feet. Daily traffic volumes along this section of roadway amount to 1,900 vehicles.

- Interior Streets - The streets that are interior to the Specific Plan area will have an ultimate cross section of a 44 foot pavement section within a 66 foot right-of-way. Santa Ana Avenue between Etiwanda Avenue and Jasmine Street, Jasmine Street between Santa Ana Avenue and Dahlia Street and Dahlia Street between Etiwanda Avenue and Jasmine Street, have all been constructed to their ultimate cross section, reference Figure 8. Santa Ana Avenue between A Street and Mulberry Avenue, Jasmine Street between Santa Ana Avenue and Slover Avenue, A Street, B Street, and C Street will be new streets within this area and will have a cross section as indicated in Figure 9.

**1 ETIWANDA AVENUE - BETWEEN SLOVER AVENUE AND  
SANTA ANA AVENUE**

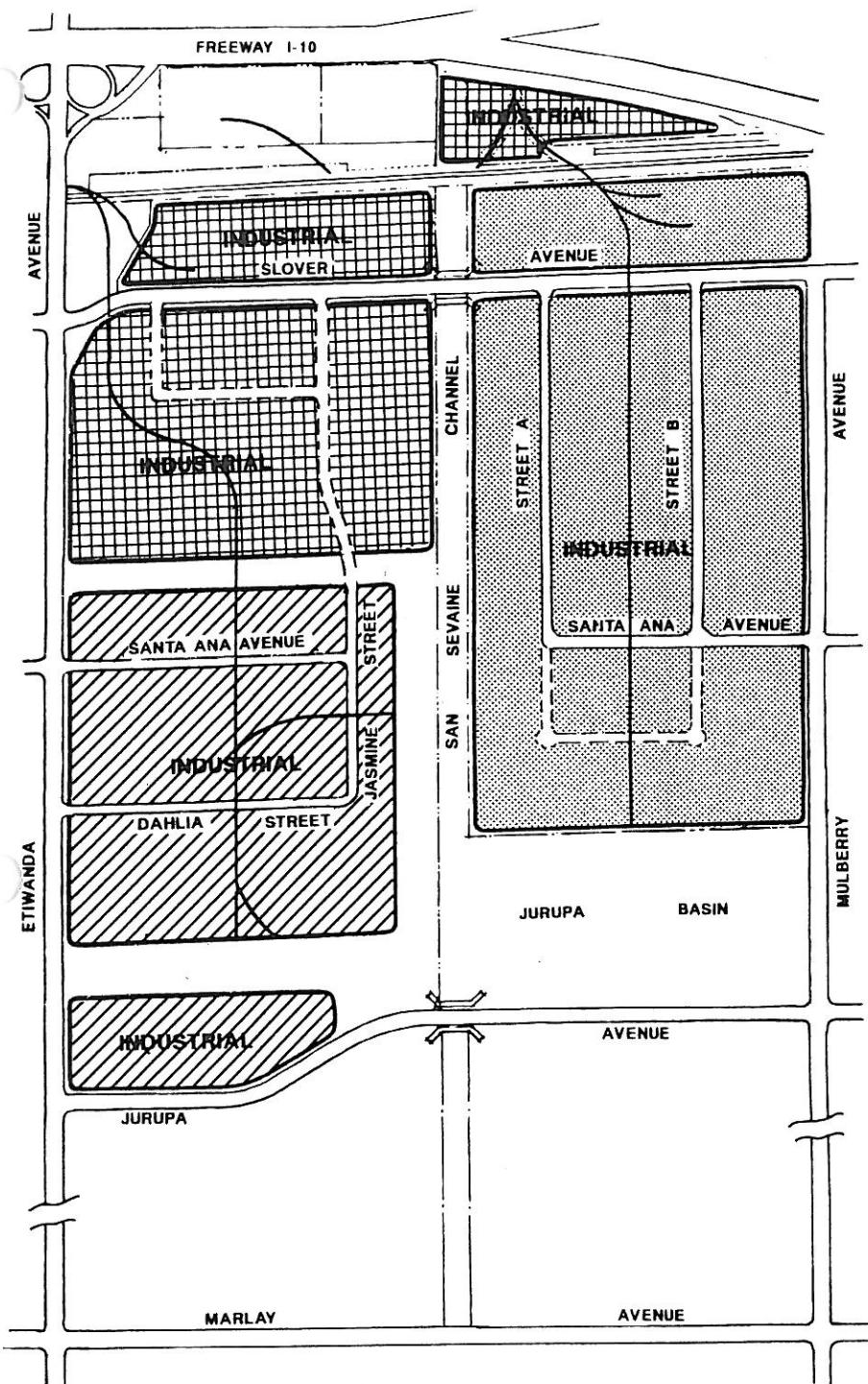


**EXISTING CROSS-SECTION**



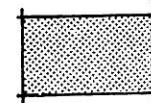
**PROPOSED CROSS-SECTION**

# PROPOSED PHASING PROGRAM

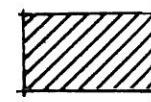


NOTE:  
THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

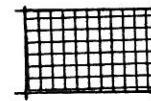
## LEGEND



PHASE I



PHASE II



PHASE III

V-2

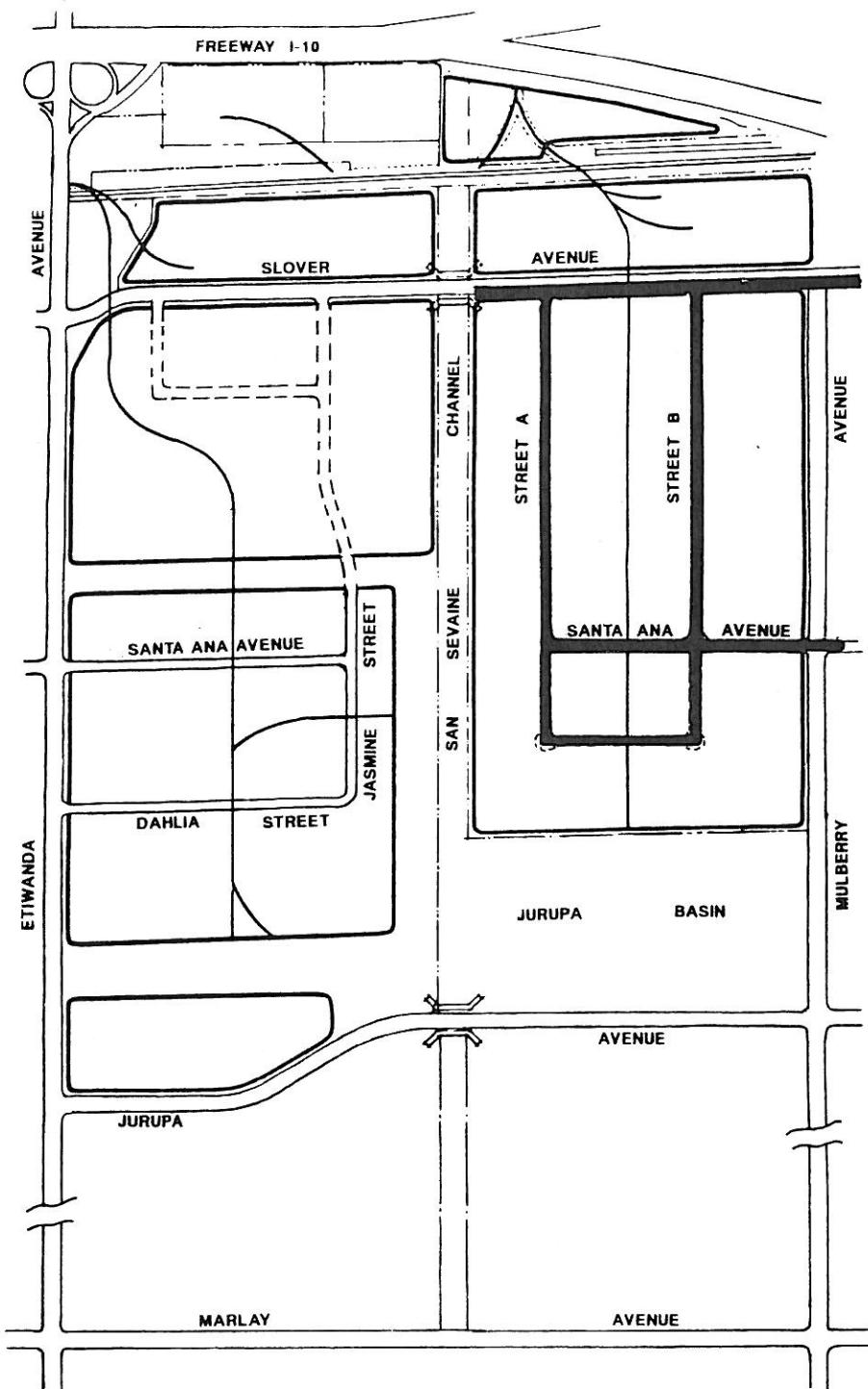
  
NO SCALE

CITY OF FONTANA

**FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

FIGURE 1



**NOTE:**  
 THE DEPICTED OPTIONAL ROADWAYS AND  
 ALL RELATED PUBLIC UTILITY FACILITIES  
 ARE CONCEPTUAL ONLY AND WILL BE  
 INSTALLED ONLY IN THE DISCRETION OF  
 EACH AFFECTED PROPERTY OWNER. NO  
 IMPLICATION OF PUBLIC UTILITY EASEMENT  
 OR PUBLIC UTILITY FACILITY LOCATION IS  
 TO BE DERIVED FROM THE LOCATION OF  
 THESE CONCEPTUAL ROADWAYS.

**LEGEND**



**PROPOSED**

  
**NO SCALE**

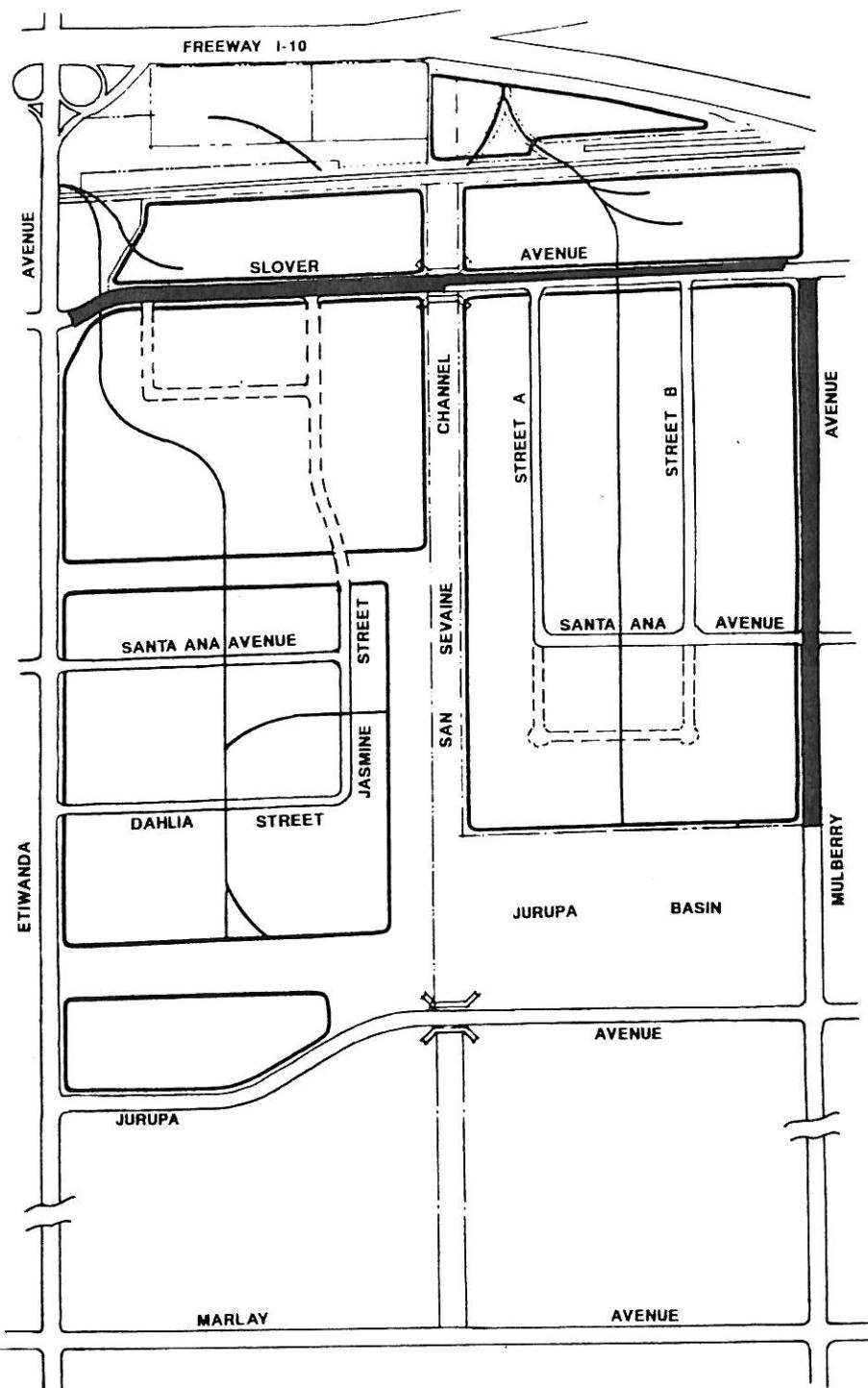
THE STREETS SOUTH OF SANTA ANA IN PHASE 1A MAY OR MAY NOT  
 BE CONSTRUCTED, AT THE OPTION OF THE DEVELOPER.

V - 3

**CITY OF FONTANA**  
**FONTANA GATEWAY SPECIFIC PLAN**

**WILLIAMS - KUEBELBECK & Associates, Inc.**  
**FORMA-PLANNING NETWORK**

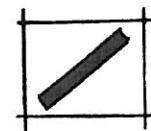
FIGURE 2A



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND



PROPOSED



NO SCALE

THE STREETS SOUTH OF SANTA ANA IN PHASE 1A MAY OR MAY NOT BE CONSTRUCTED, AT THE OPTION OF THE DEVELOPER.

V-3

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

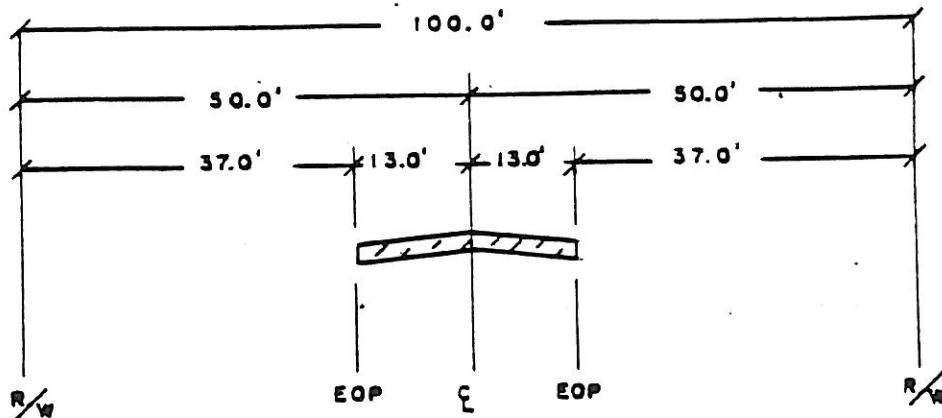
WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

Capital Outlay Expense - One-Time Costs

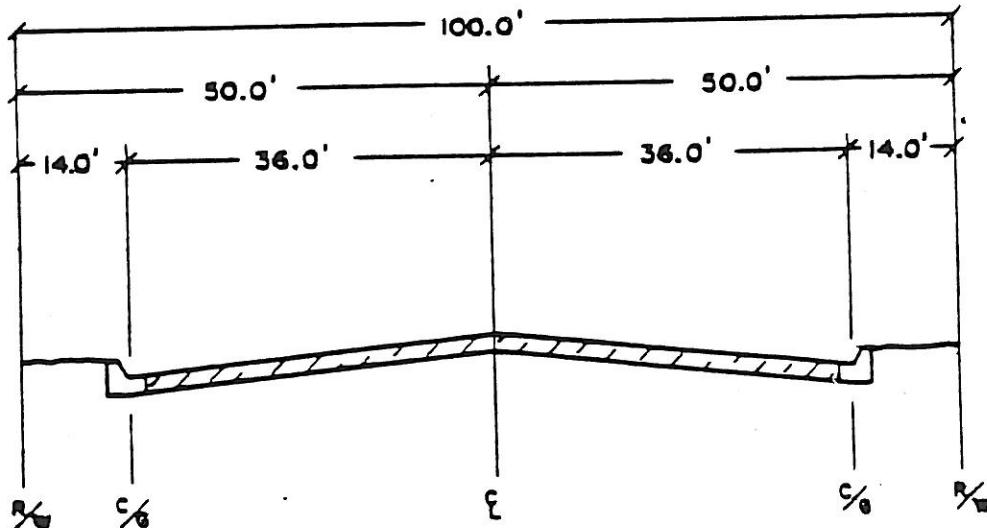
One (1) Blk/White Vehicle.....	\$13,500
(One (1) Car for Every 3 Officers)	
Machinery & Equipment.....	\$ 8,330
(Unit Radio \$2,500, Cage \$175, Unitrol \$475, Lightbar \$500, Pushbar \$200, Shotgun \$300, CAD Terminal \$4,000, Siren Speaker \$175, Installation \$450, = 8,330 Per Vehicle)	
Handi-Talkie Radios (1 for every 3 officers).....	\$1,370
Furniture Fixtures:	
Desks (one x 420).....	\$ 420
Chairs (one x 225).....	\$ 225
Total Capital Outlay Expenses	<u>\$23,845</u>

2

**SLOVER AVENUE - BETWEEN MULBERRY AVENUE  
AND STREET B**



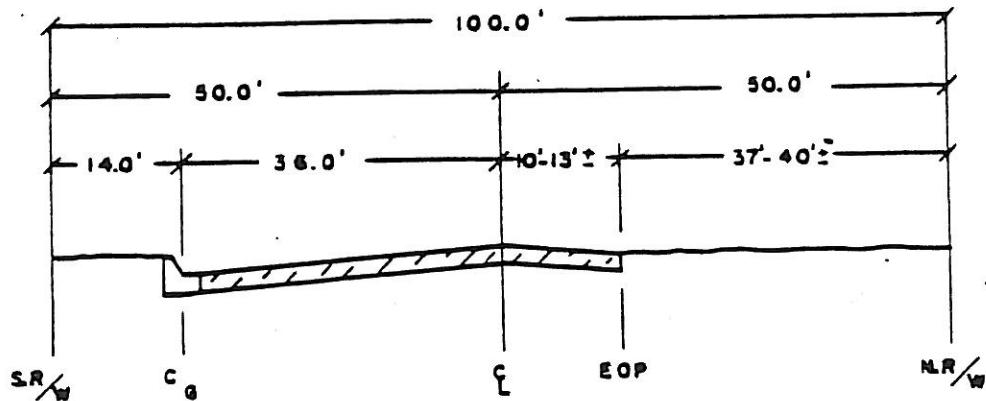
**EXISTING CROSS-SECTION**



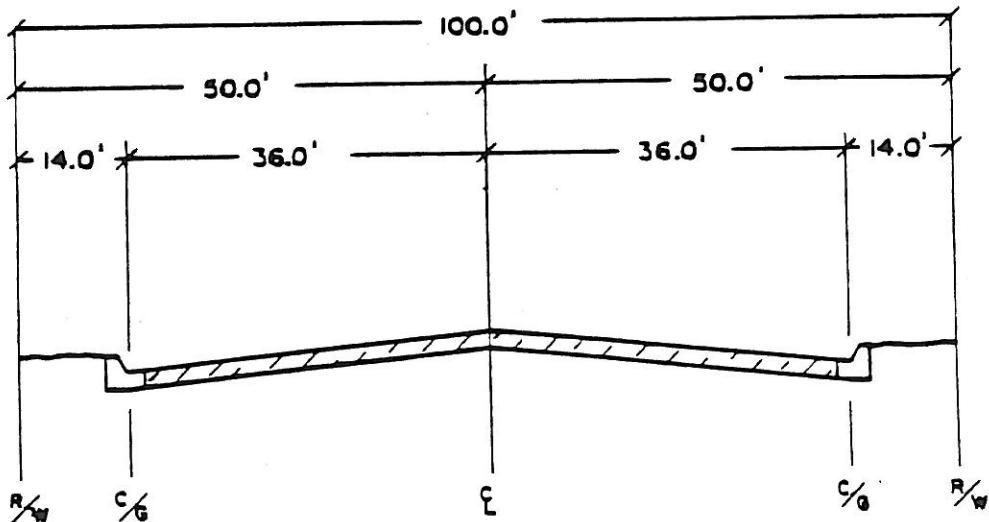
**PROPOSED CROSS-SECTION**

3

SLOVER AVENUE - BETWEEN STREET B AND  
ETIWANDA AVENUE



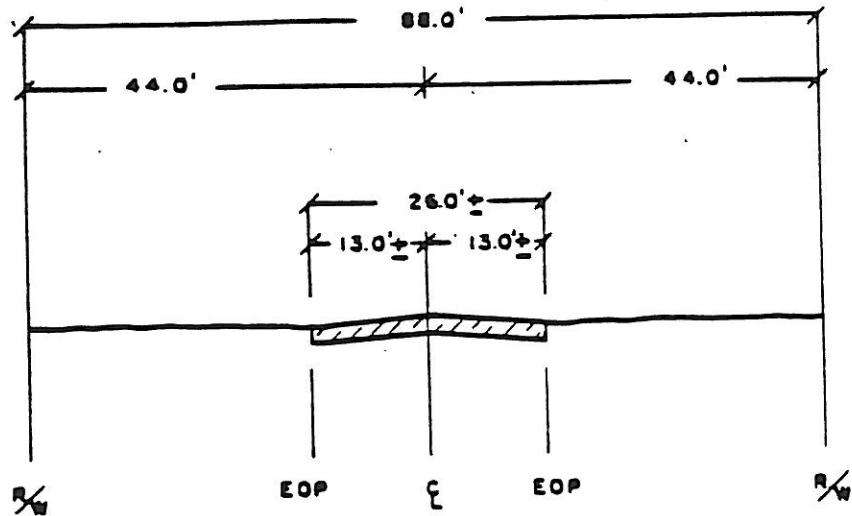
EXISTING CROSS-SECTION



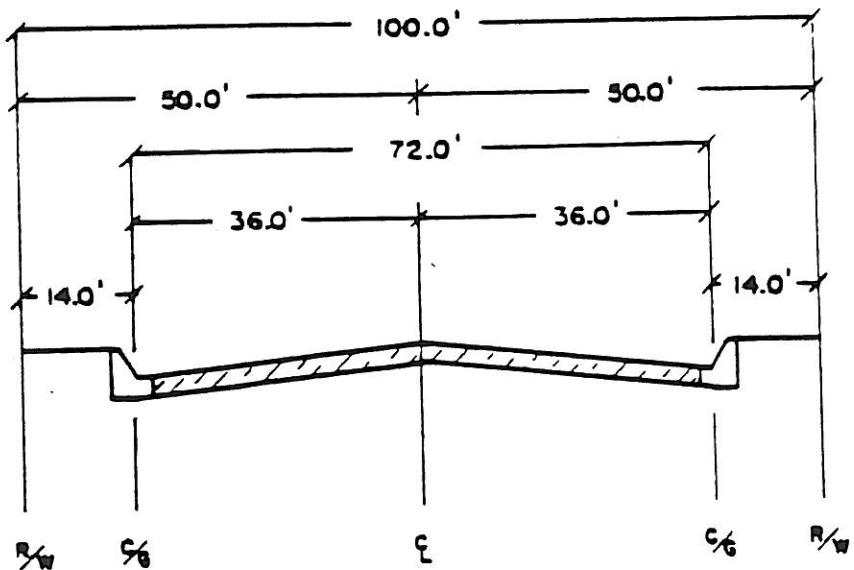
PROPOSED CROSS-SECTION

4

**MULBERRY AVENUE - BETWEEN SLOVER AVENUE  
AND JURUPA AVENUE**



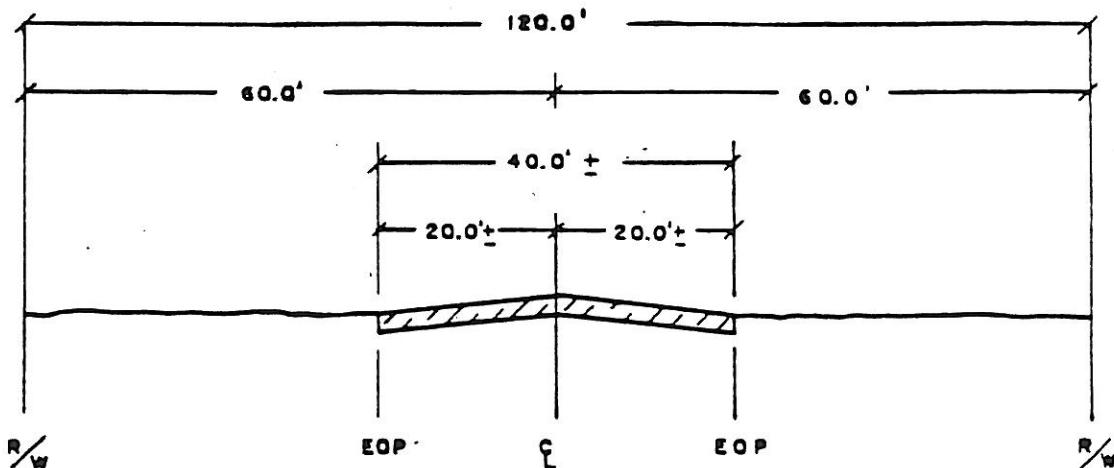
**EXISTING CROSS-SECTION**



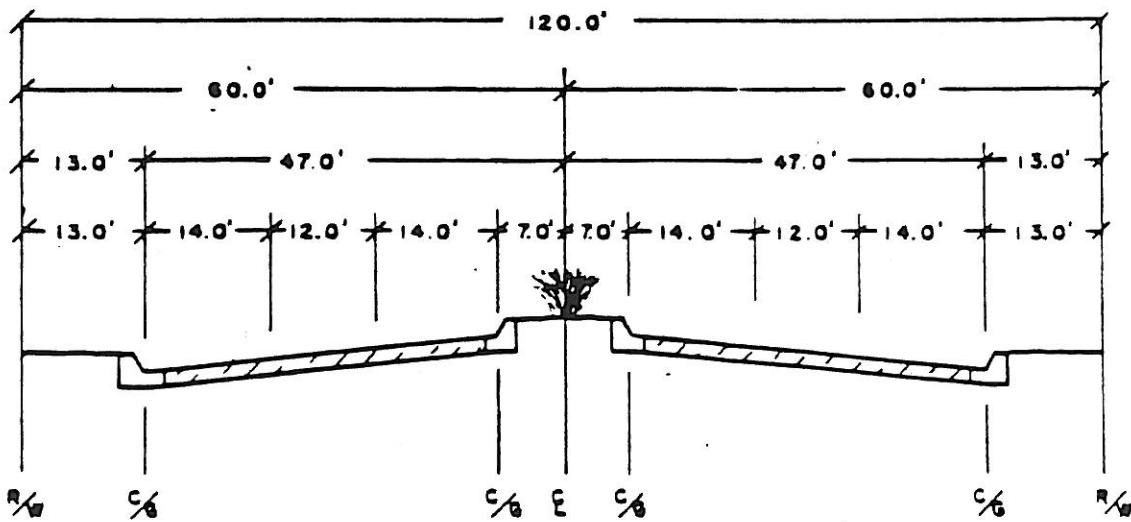
**PROPOSED CROSS-SECTION**

5

ETIWANDA AVENUE - BETWEEN SANTA ANA AVENUE  
AND DAHLIA STREET



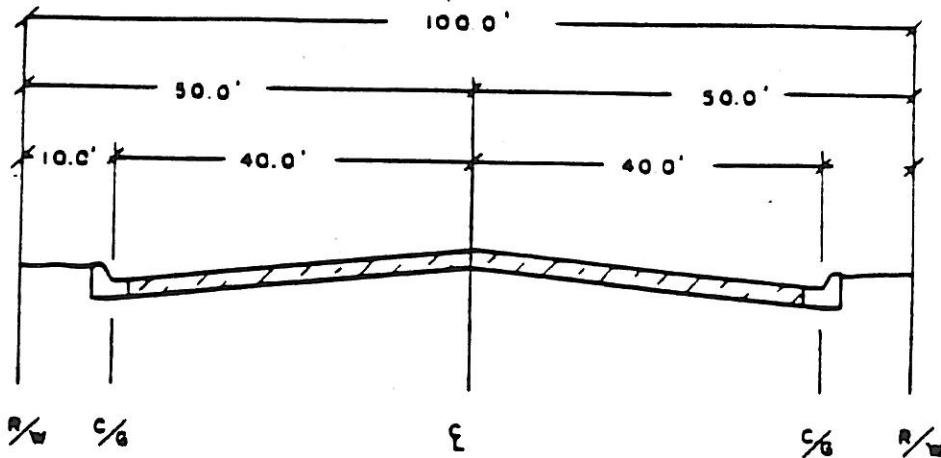
EXISTING CROSS-SECTION



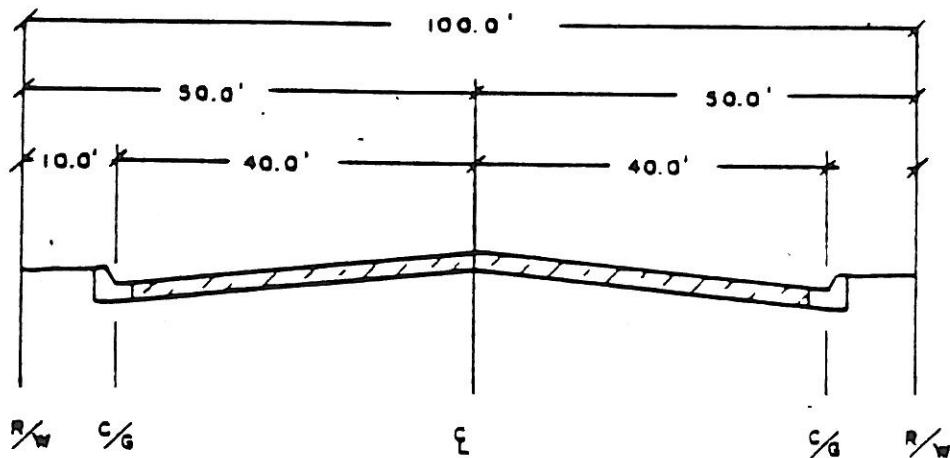
PROPOSED CROSS-SECTION

6

JURUPA AVENUE - BETWEEN ETIWANDA AVENUE  
AND APPROXIMATELY 2,000' EAST



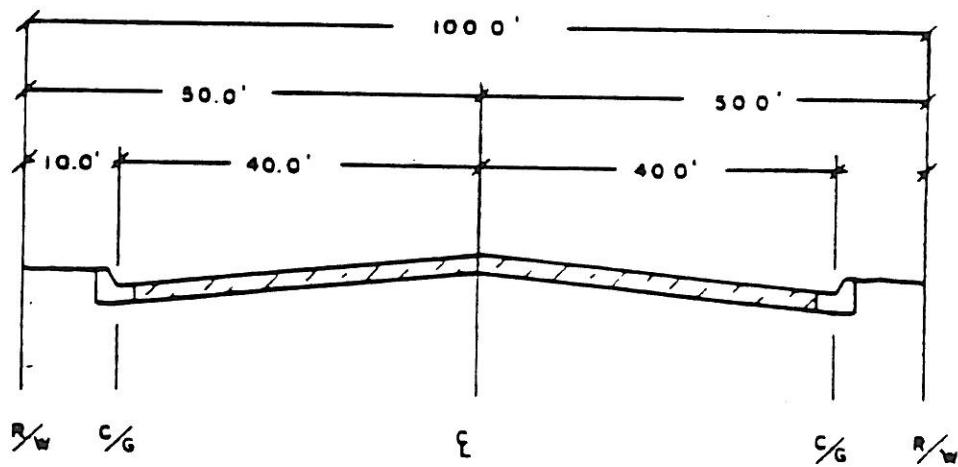
EXISTING CROSS-SECTION



PROPOSED CROSS-SECTION

7

JURUPA AVENUE - BETWEEN A POINT 2,000 EAST  
OF ETIWANDA AVENUE AND  
MULBERRY AVENUE

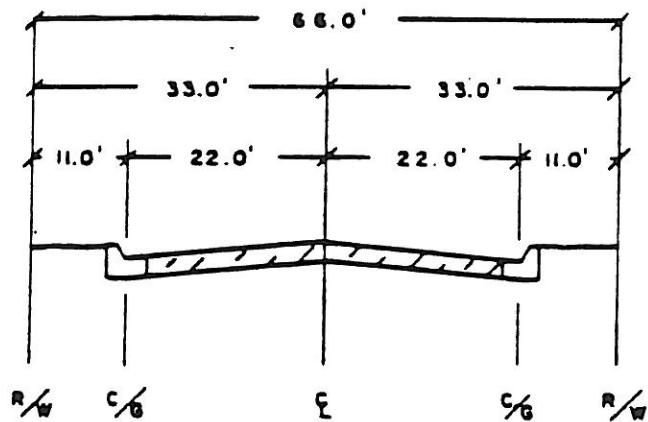


PROPOSED CROSS - SECTION

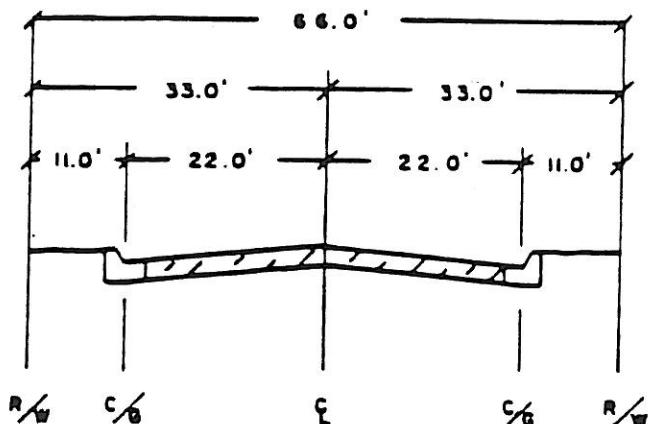
**SANTA ANA AVENUE** - BETWEEN ETIWANDA AVENUE  
AND JASMINE STREET

**8 JASMINE STREET** - BETWEEN SANTA ANA AVENUE  
AND DAHLIA STREET

**DAHLIA STREET** - BETWEEN ETIWANDA AVENUE  
AND JASMINE STREET



**EXISTING CROSS-SECTION**



**PROPOSED CROSS-SECTION**

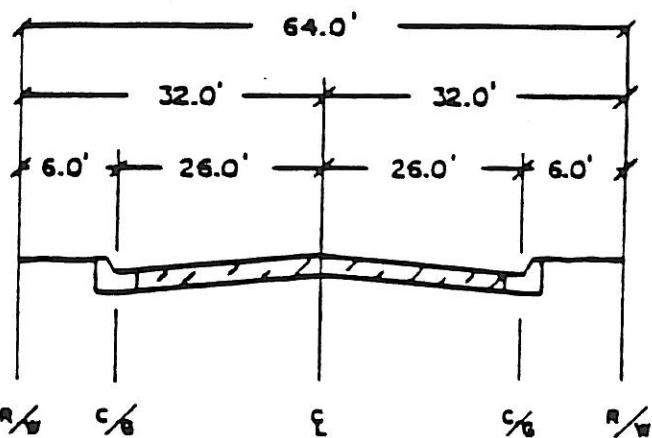
9

SANTA ANA AVENUE - BETWEEN STREET A AND  
MULBERRY AVENUE  
JASMINE STREET - EXTENSION

STREET A

STREET B

STREET C



PROPOSED CROSS - SECTION

**EXISTING TRAFFIC VOLUMES**

The existing traffic volumes were obtained from the following sources:

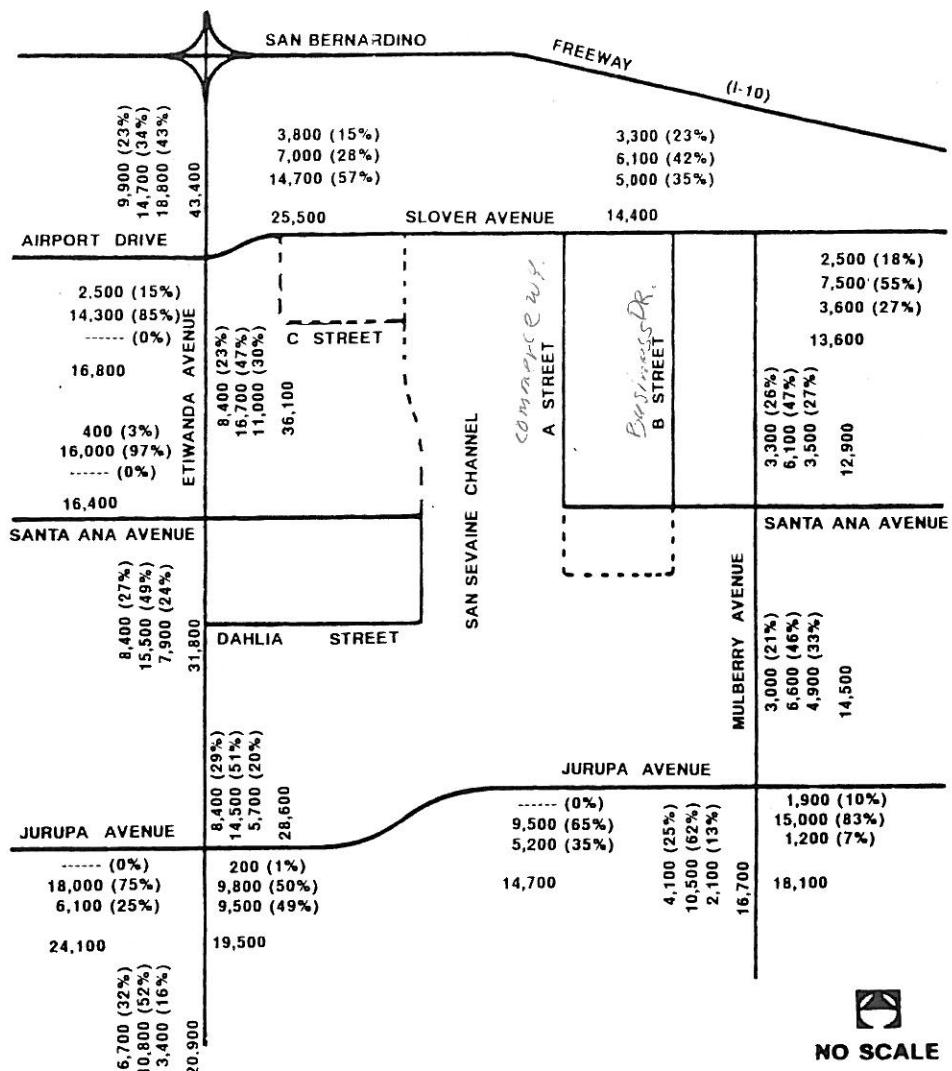
1. 1985 State of California Traffic Volumes.
2. San Bernardino County Transportation Department.
3. City of Fontana.
4. City of Ontario.
5. Traffic study dated June 1983 as prepared by Krueper Engineering & Associates.

Where necessary existing counts were projected at a 6 percent straight line growth to represent 1986 conditions. Figure 10 shows the existing daily traffic volumes.

**PROJECTED STUDY AREA TRAFFIC VOLUMES**

Figure 11 shows the area breakdown of the Specific Plan and respective projected traffic from those areas. Figure 10 shows the routing of the traffic as generated by the Specific Plan area. Figure 10 also shows the projected traffic volumes from the nonproject area.

### CUMULATIVE TRAFFIC VOLUMES



**NO SCALE**

**NOTE:**

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATIONS IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

**EXISTING TRAFFIC (1986)**

#### TRAFFIC GENERATED BY NON-PROJECT AREA (1997)

#### TRAFFIC GENERATED BY SPECIFIC PLAN AREA (1997)

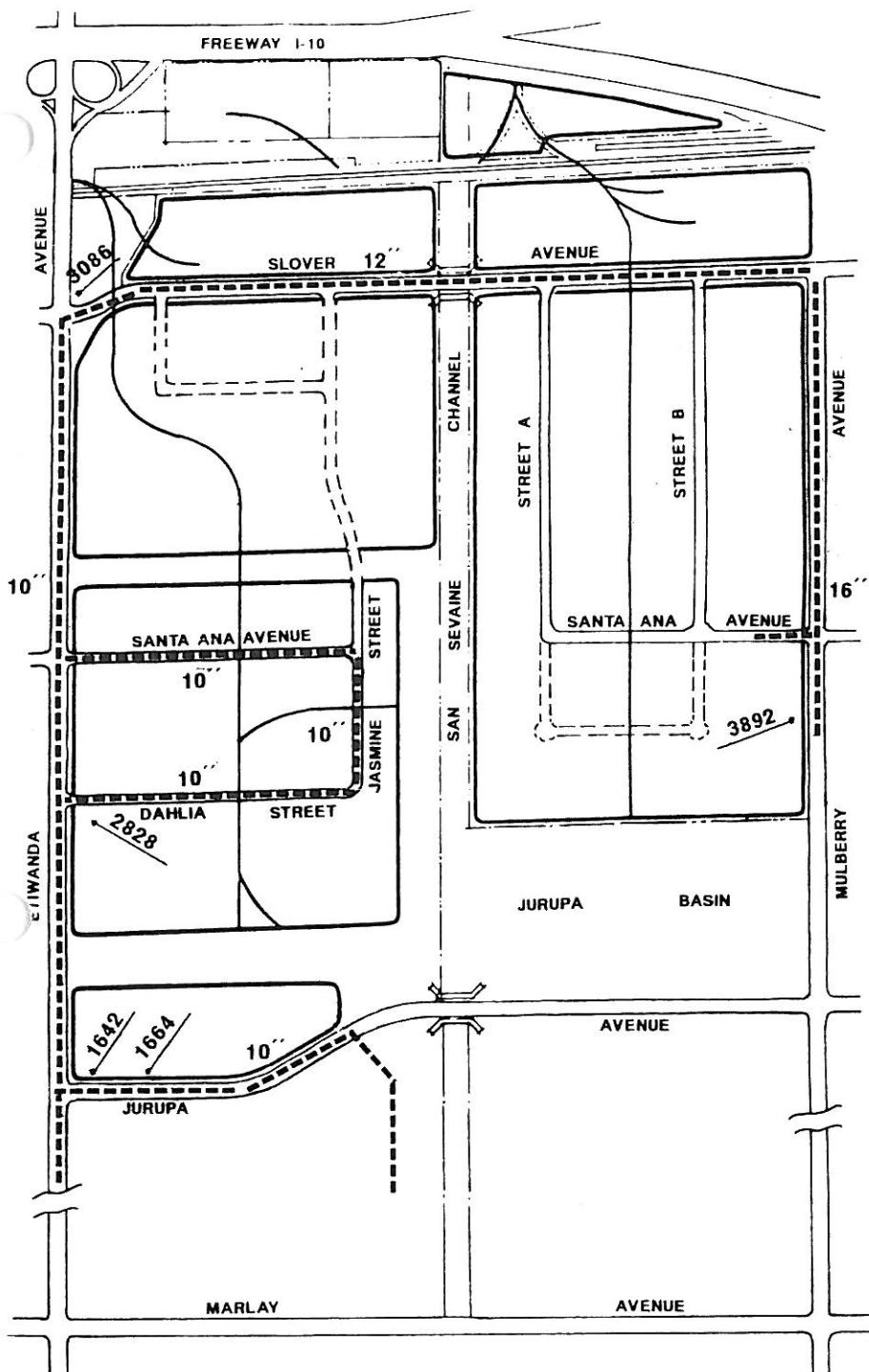
1-15

#### TOTAL AVERAGE DAILY TRAFFIC (1997)

CITY OF FONTANA  
**FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# EXISTING WATER SYSTEM



NO SCALE

## NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

XXXX GPM FIRE FLOW AT 20 psi.

III-2

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

**FUTURE LEVEL OF SERVICE**  
**ULTIMATE ROADWAY CROSS SECTION**

<b>Roadway</b>	<b>Section</b>	<b>Existing + Specific Plan</b>			<b>Existing + Specific Plan + Area Growth</b>		
		<b>VOL (1)</b>	<b>CAP (1)</b>	<b>LOS</b>	<b>VOL (1)</b>	<b>CAP (1)</b>	<b>LOS</b>
<b>Etiwanda Avenue</b>	North of Slover	28.7	45.0	0.64/B	43.4	45.0	0.96/E
	South of Slover	19.4	75.0	0.26/A	36.1	75.0	0.48/B
	South of Santa Ana	16.3	75.0	0.22/A	31.8	75.0	0.43/B
	South of Jurupa	14.1	75.0	0.19/A	28.6	75.0	0.38/B
<b>Slover Avenue</b>	East of Etiwanda	18.7	38.0	0.49/B	25.5	38.0	0.67/B
	West of Mulberry	8.3	38.0	0.22/A	14.4	38.0	0.38/A
	East of Mulberry	6.1	38.0	0.16/A	13.6	38.0	0.36/A
<b>Mulberry Avenue</b>	South of Slover	6.1	30.0	0.20/A	12.9	30.0	0.43/B
	South of Santa Ana	7.9	30.0	0.26/A	14.5	30.0	0.48/B
	South of Jurupa	6.2	30.0	0.21/A	16.7	30.0	0.56/B
<b>Jurupa Avenue</b>	West of Etiwanda	6.1	75.0	0.08/A	24.1	75.0	0.32/A
	East of Etiwanda	9.7	38.0	0.26/A	19.5	38.0	0.52/B
	West of Mulberry	5.2	38.0	0.14/A	14.7	38.0	0.39/B
	East of Mulberry	3.1	38.0	0.08/A	18.1	38.0	0.48/B

VOL - Volume

CAP - Capacity

LOS - Level of Service

(1) Volume and capacity values in thousands.

## DISCUSSION

The future traffic volumes, Levels of Service and proposed roadway cross sections on the roadway network summarized on Figure 10, Table 1 and Figures 1 through 9 illustrate roadway improvements which are necessary by 1997. These improvements were developed to minimize the impact the future nonproject area traffic flows would have on the roadway circulation system, as well as to accommodate the traffic volumes generated by the Specific Plan area.

### A. Traffic Signal Installation

Traffic signals would be required by 1997 at the following intersections.

- Slover Avenue - Mulberry Avenue (new)
- Slover Avenue - Jurupa Avenue (new)
- Etiwanda Avenue - Santa Ana Avenue (new)
- Etiwanda Avenue - Airport Drive/Slover Avenue (modify existing)
- Etiwanda Avenue - Jurupa Avenue (new)

**B. Roadway Improvements**

The following streets, at a minimum are to be upgraded to their ultimate section by the year 1997 to aid in moving traffic through the surrounding roadway circulation system.

- Etiwanda Avenue - 120 feet right-of-way with two 40 foot pavement sections separated by a 14 foot median including left turn pockets at key intersections from Slover Avenue to Jurupa Avenue.
- Slover Avenue - 104 foot right-of-way with 80 foot pavement section from Etiwanda Avenue to Mulberry Avenue.
- Jurupa Avenue - 104 foot right-of-way with an 80 foot pavement section from 2,000 feet east of Etiwanda Avenue to Mulberry Avenue.
- Mulberry Avenue - 88 foot right-of-way with a 64 foot pavement section from Slover Avenue to Mulberry Avenue.
- Internal Collector Roadway - 66 foot right-of-way with 44 foot pavement section.
  - A Street
  - B Street
  - C Street
  - Jasmine Street (extension)
  - Santa Ana Avenue (extension)

**C. Channel Crossings**

All weather crossings of the San Savaine Channel are to be installed at the following locations.

- Slover Avenue (by completion of Phase I)
- Jurupa Avenue (by completion of Phase III - 1997)

The construction of these all weather crossing should be coordinated with the proposed improvements to the San Savaine Channel and Jurupa Basin.

## **II. WASTEWATER**

**EXISTING SEWER SYSTEM**

Chino Basin Municipal Water District provides domestic sewer treatment for the Specific Plan area. The only existing sewer main within the Specific Plan area is in Jurupa Avenue. This 8 inch main drains to a north/south 8 inch main in Pacific Avenue to the 33 inch trunk line located in Marlay Avenue. This 33 inch trunk line is the main interceptor for the Specific Plan area. The existing sewer system within the Specific Plan area is shown in Figure 1.

**PROPOSED "BACKBONE" WASTEWATER SYSTEMS**

At the present time the City of Fontana has contracted with Hall T. Foreman Inc. to design a 36 inch sewer main for Etiwanda Avenue from Marlay Avenue to Slover Avenue then northerly north of Interstate 10.

Also the City of Fontana has designated Mulberry Avenue a main carrier for sewage and will require a 21 inch sewer main in Mulberry Avenue between Marlay Avenue and Slover Avenue due to development to the north.

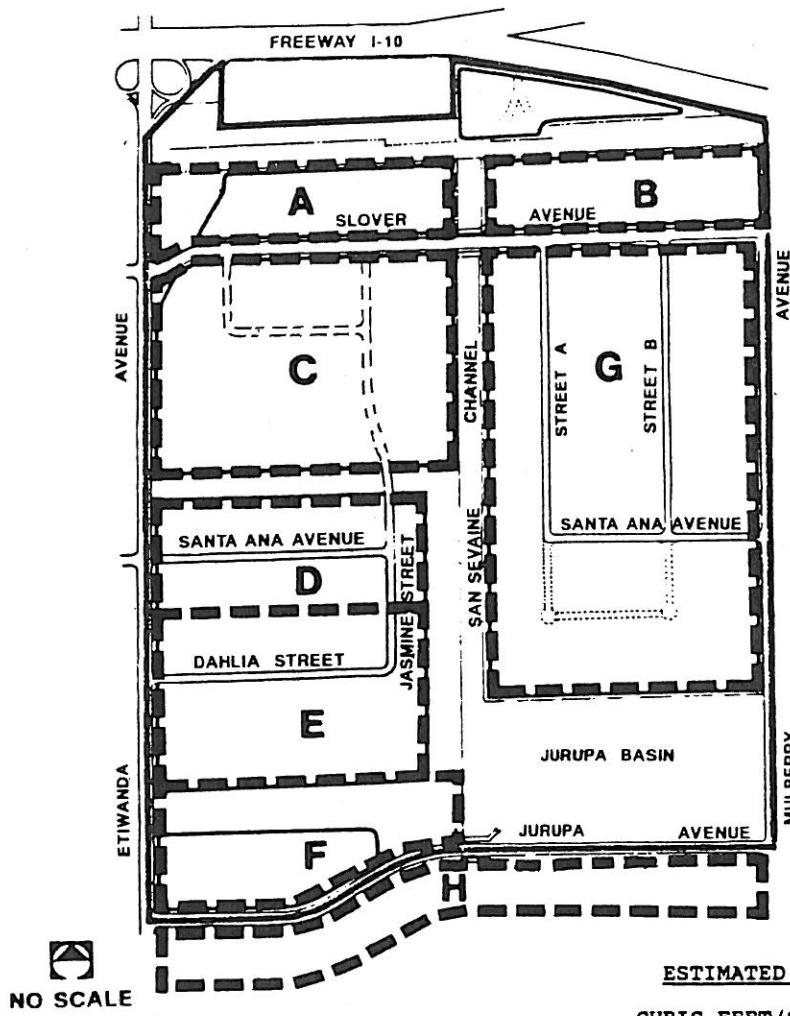
**PROJECTED SEWAGE GENERATION**

It is estimated that the Specific Plan area (639± acres) will generate 377,500 gallons of sewage per day or 1.405 cubic feet per second. Figure 2 provides a summary of the sub area generation calculations for the above mentioned total. The average flow generation factor used for those calculations was 0.0022 cfs/acre.

**PROPOSED SEWER SYSTEM**

Utilizing the quantities obtained through above mentioned calculations and a review of the topography of the Specific Plan area the overall sewer system as shown on Figure 3 has been developed. Said figure shows main locations and their relative sizes for the ultimate build-out of the Specific Plan area.

# SEWER QUANTITY GENERATION ANALYSIS



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

ESTIMATED WASTEWATER GENERATION

AREA	ACREAGE	CUBIC FEET/SECOND <u>cfs</u>	GALLONS/DAY <u>gpd</u>
A	39	0.086	23,100
B	36	0.079	21,200
C	110	0.242	65,000
D	57	0.125	33,600
E	84	0.185	49,700
F	53	0.117	31,400
G	218	0.479	128,800
H	<u>42</u>	<u>0.092</u>	<u>24,700</u>
639 acres		*1.405 cfs	377,500 gpd

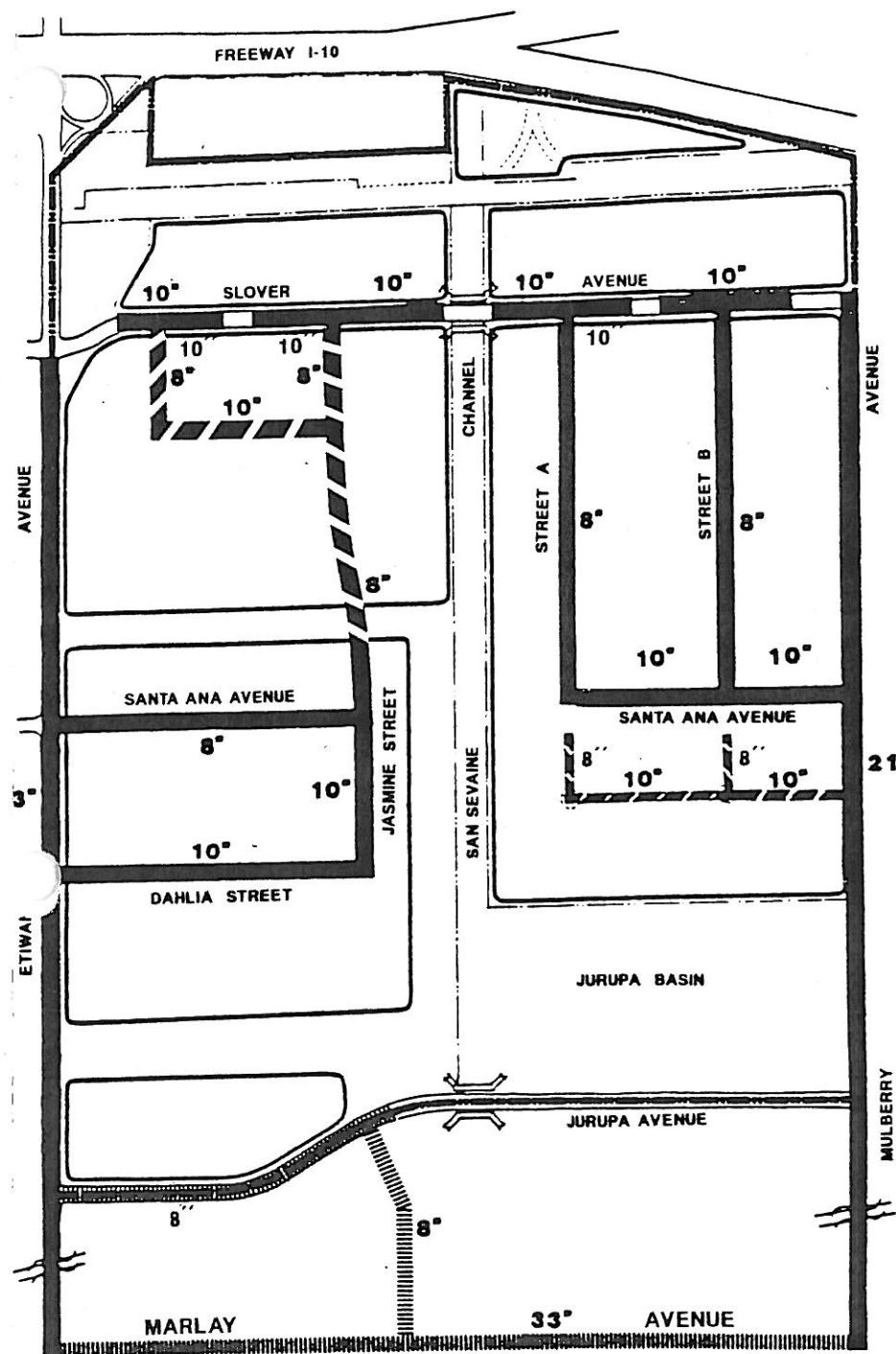
\* Average flow (peak flow equals 2.5 times the average flow)

II-4

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# PROPOSED SEWER SYSTEM



## NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

- EXISTING
- PROPOSED
- OPTIONAL

THE INTERIOR SANITARY SEWERS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA

II-5

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

**III. WATER**

**EXISTING WATER SYSTEM**

Water is provided to the Specific Plan area by Fontana Water Company, a division of San Gabriel Water Company. Locations of existing mains and their respective sizes are shown on Figure 1. A series of fire flow tests have been performed by Fontana Water Company. The results and locations of the tests are also shown on Figure 1.

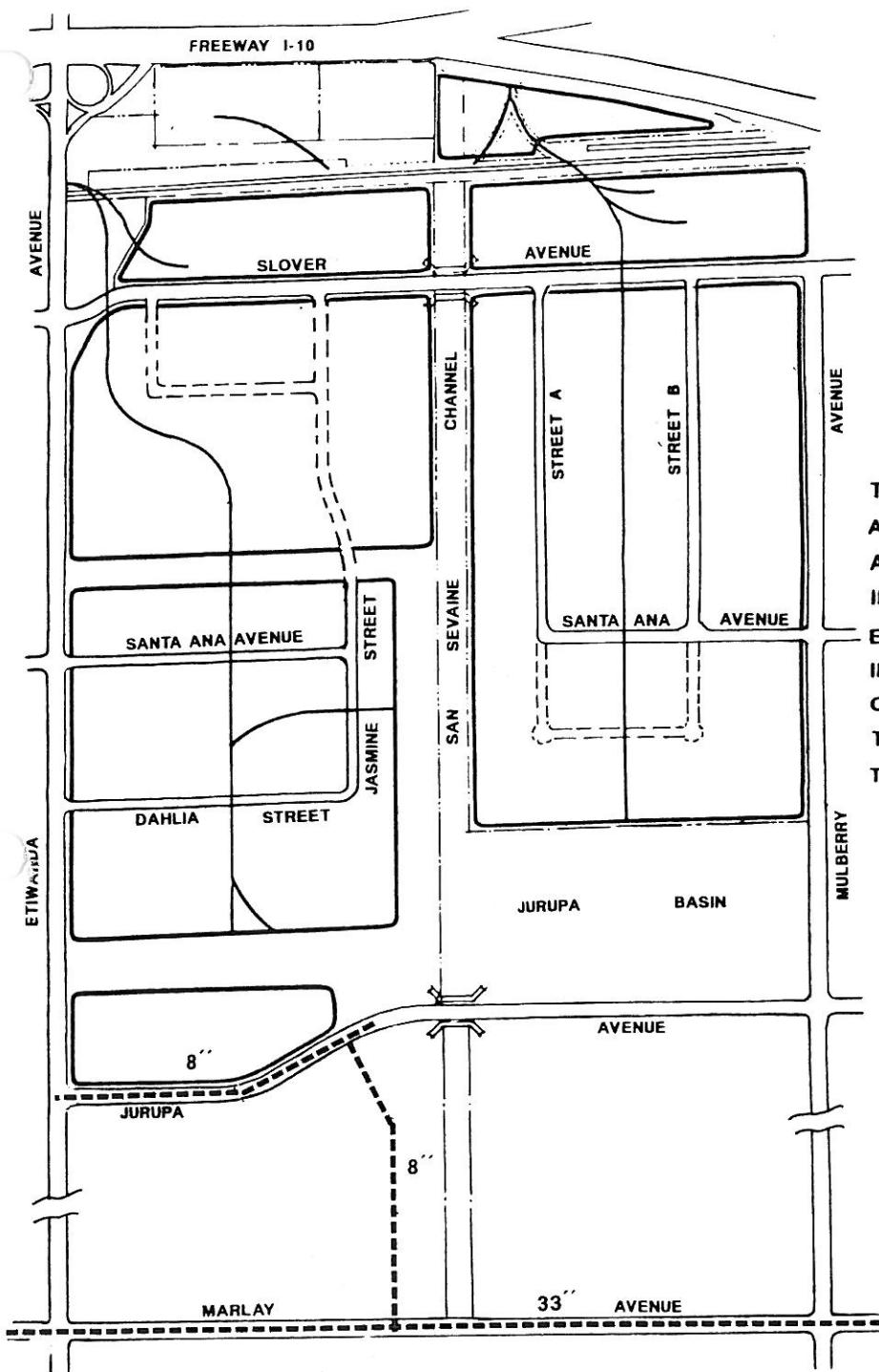
**PROJECTED WATER DEMAND**

The projected water demand for the Specific Plan area was calculated by using the subareas as shown in Figure 2. The daily water demand for the Specific Plan area was calculated to be 480,000 gallons per day (797 gpm average flow). However, in terms of sizing mains within the area the fire flow requirements would dictate since a 4,000 gallon per minute, 4 hour flow duration would be required.

**PROPOSED WATER SYSTEM**

Figure 3 depicts the location and size of the proposed water mains to meet service requirements for the Specific Plan area. Fire flows should also be met by the new mains that are proposed. However, existing tests show that the 10 inch lines in Santa Ana Avenue, Jasmine Street, Dahlia Street, Etiwanda Avenue and Jurupa Avenue in the southwest portion of the Specific Plan area, do not meet the 4,000 gallon per minute requirement and may require upgrading.

# EXISTING SEWER SYSTEM



**NOTE:**

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

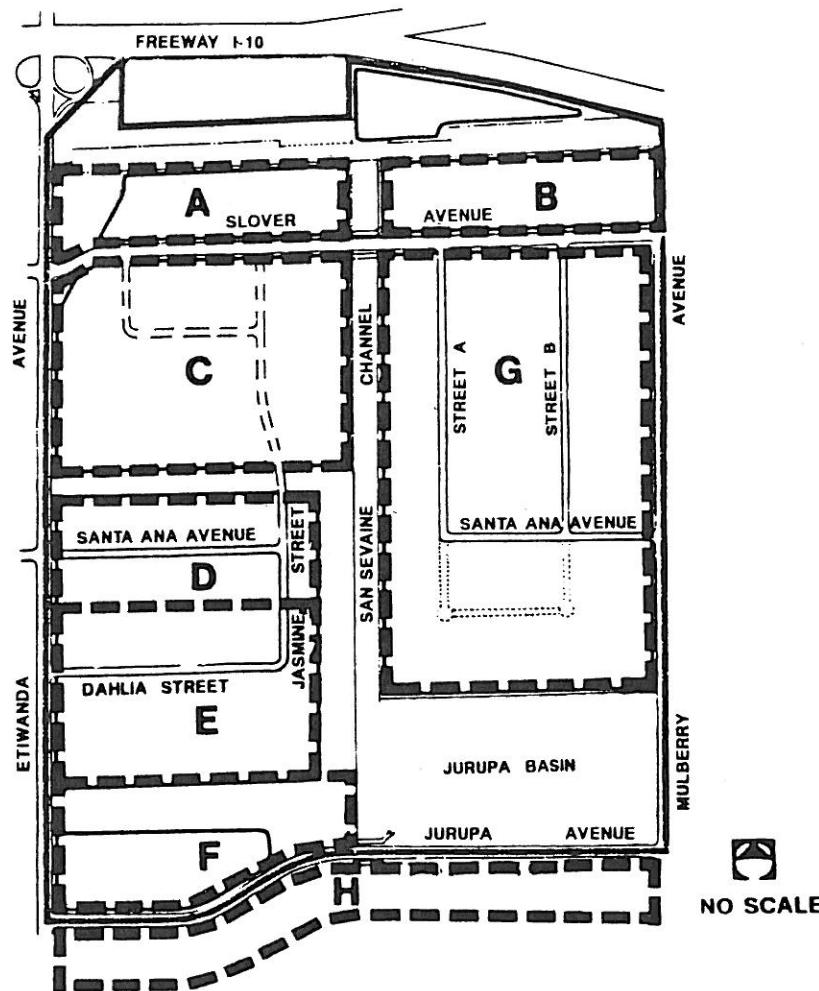
  
NO SCALE

II-2

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# WATER QUANTITY GENERATION ANALYSIS



## ESTIMATED WATER DEMAND

AREA	ACREAGE	GALLONS/MIN. (gpm)	GALLONS/DAY (gpd)
A	39	49	29,250
B	36	45	27,000
C	110	137	82,500
D	57	71	42,750
E	84	105	63,000
F	53	66	39,750
G	218	272	163,500
H	42	52	31,500
TOTAL	639 acres	797 gpm	479,250 gpd

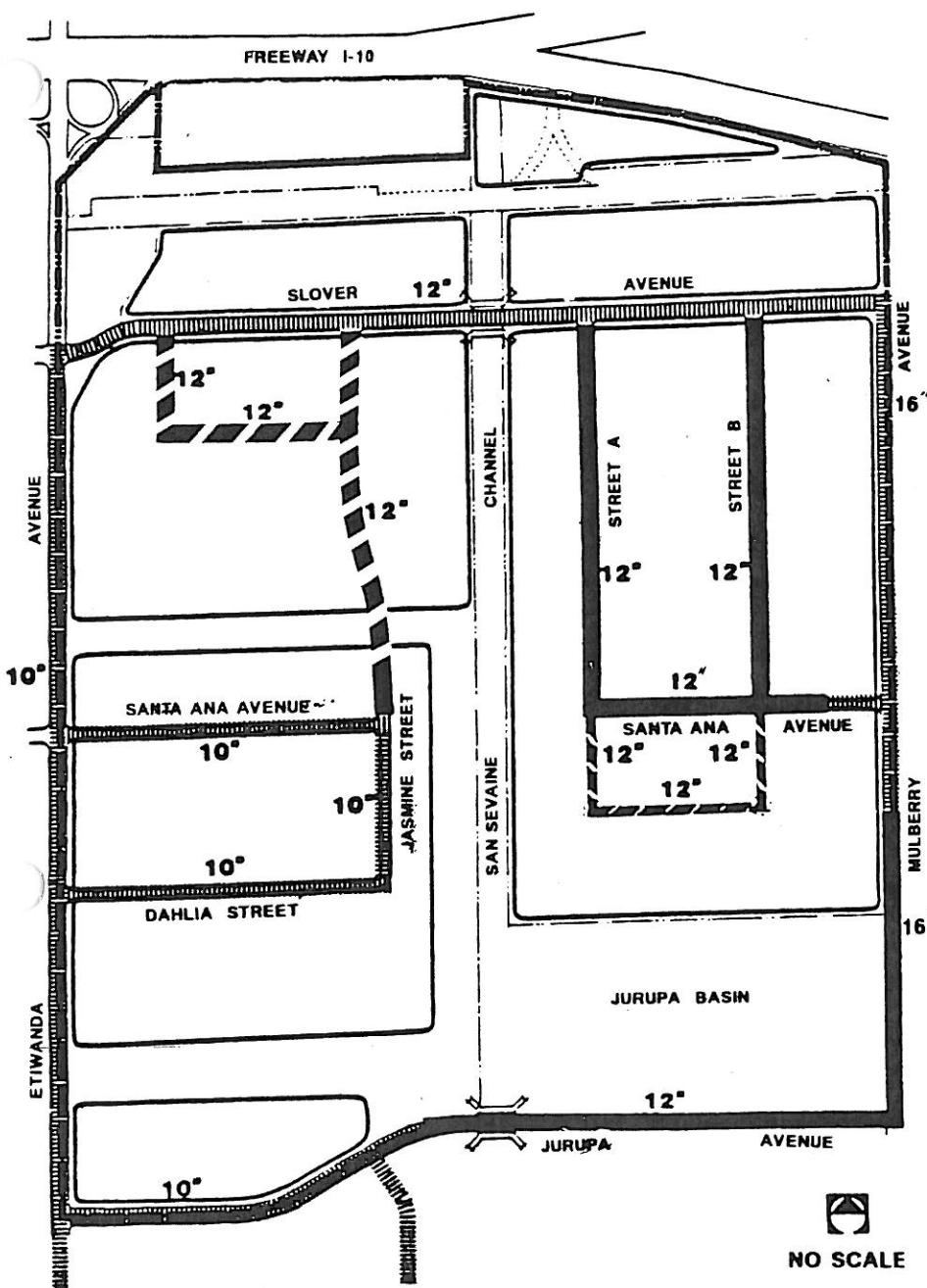
NOTE:  
THE DEPICTED OPTIONAL ROADWAYS AND  
ALL RELATED PUBLIC UTILITY FACILITIES  
ARE CONCEPTUAL ONLY AND WILL BE  
INSTALLED ONLY IN THE DISCRETION OF  
EACH AFFECTED PROPERTY OWNER. NO  
IMPLICATION OF PUBLIC UTILITY EASEMENT  
OR PUBLIC UTILITY FACILITY LOCATION IS  
TO BE DERIVED FROM THE LOCATION OF  
THESE CONCEPTUAL ROADWAYS.

III-3

# CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# PROPOSED WATER SYSTEM



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

- EXISTING
- PROPOSED
- OPTIONAL

THE WATER MAINS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA

III-4

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

FIGURE 3

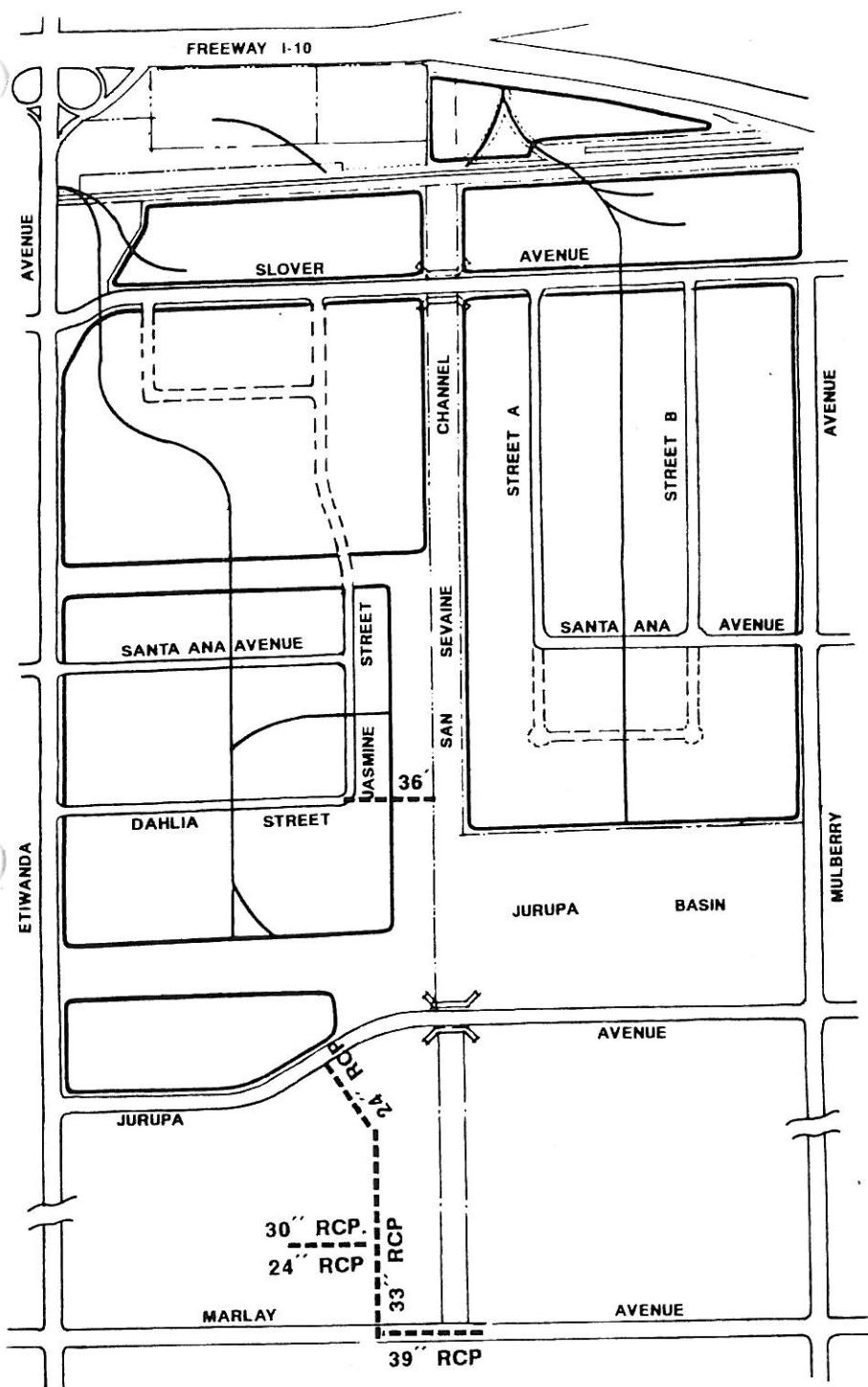
**IV. STORM DRAIN**

**EXISTING DRAINAGE SYSTEM**

Located within and adjacent to the Specific Plan area are two major water carrying channels. The San Savaine Channel bisects the Specific Plan area in a north to south direction and has an ultimate design capacity of 18,850 cfs. The lower Etiwanda Channel is located westerly of Etiwanda Avenue within the City of Ontario and has an ultimate design capacity of 3,500 cfs.

There are only two existing minor storm drains located within the Specific Plan area which are used at present to dewater isolated areas. The existing storm drain systems within the Specific Plan area are shown on Figure 1.

# EXISTING STORM DRAINS



IV-2



NO SCALE

CITY OF FONTANA

## FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

FIGURE 1

**MASTER PLANNED DRAINAGE SYSTEM**

The County of San Bernardino has performed two major drainage studies within the drainage basin that covers the Specific Plan area.

1. Day, Etiwanda and San Savaine Creek System Drainage Plan by Bill Mann & Associates dated March 1983.

2. San Bernardino County Comprehensive Storm Drain Plan, Project No. 2, by Moffatt and Nichol dated March 1969.

These studies outline the improvements to the major drainage courses, San Savaine Channel and Lower Etiwanda Creek, as well as the proposed backbone storm drain system which would be required.

Of main importance is the storm drain link between Etiwanda Avenue and the lower Etiwanda Channel which would be required to aid in dewatering Etiwanda Avenue.

**PROJECTED DRAINAGE FLOWS**

It is estimated that the Specific Plan area has a contributory drainage area of 629 acres and would generate at full development, 1,120 cubic feet per second based on a 100 year storm. This would be an increase in drainage flow generated by the Specific Plan area of 306 cubic feet per second above the existing conditions.

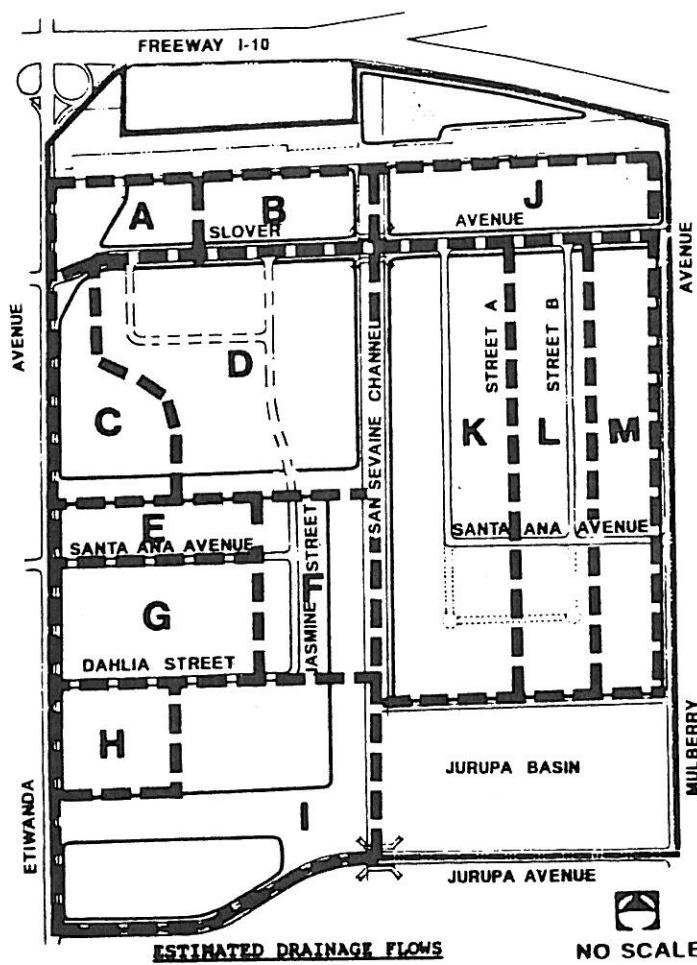
Figure 2 provides a summary of the subarea generation calculation including a comparison of existing conditions to full development conditions.

#### PROPOSED DRAINAGE SYSTEM

Utilizing the quantities obtained through the drainage subarea calculation and a review of the topography of the Specific Plan area, the overall storm drain system as shown on Figure 3 has been developed. Said figure shows the main locations and their relative sizes for the ultimate build-out of the Specific Plan area.

Special attention should be made to the dewatering of Etiwanda Avenue, since this is contingent on the development of the Master Storm Drain Backbone System to the lower Etiwanda Channel. That backbone system has been outlined and discussed in the previously noted reports. Negotiations between the City of Ontario and the City of Fontana will be required for phasing and installation of this system.

**DRAINAGE AREA FLOW COMPARISON**



AREA	ACREAGE	EXISTING	FULL	INCREASE	THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.
		CONDITIONS Q <sub>100</sub> (cfs)	DEVELOPMENT Q <sub>100</sub> (cfs)	Q <sub>100</sub> (cfs)	
A	20	26	36	10	
B	20	26	36	10	
C	40	52	72	20	
D	80	104	144	40	
E	25	33	44	11	
F	30	39	53	14	
G	50	65	88	23	
H	30	39	53	14	
I	80	104	144	40	
J	36	46	64	18	
K	87	112	154	42	
L	87	112	154	42	
M	44	52	78	22	
TOTAL	629 acres	814 cfs	1120 cfs	306 cfs	

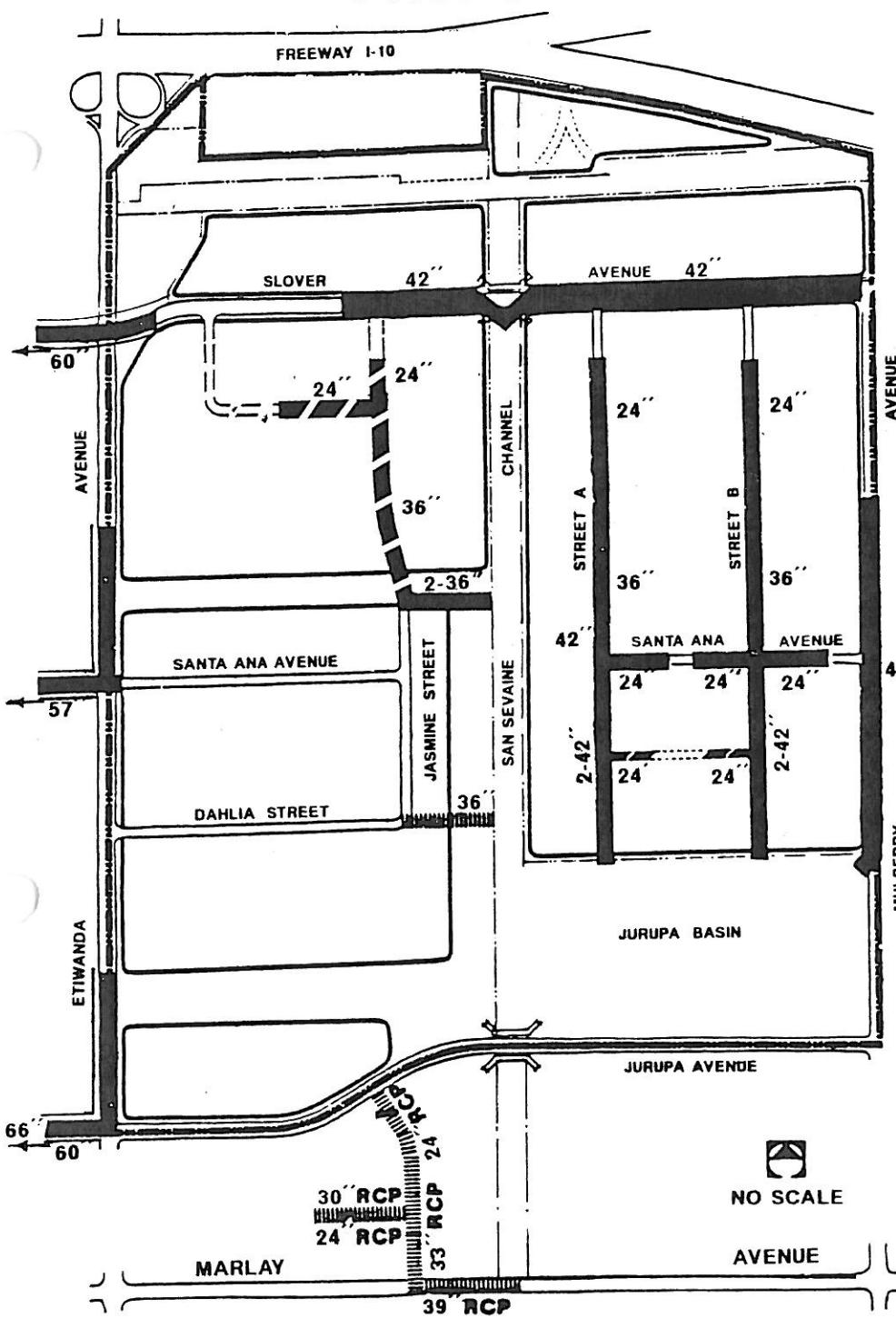
IV-5

**CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

FIGURE 2

# PROPOSED STORM DRAIN



**NOTE:**  
THE DEPICTED OPTIONAL ROADWAYS AND  
ALL RELATED PUBLIC UTILITY FACILITIES  
ARE CONCEPTUAL ONLY AND WILL BE  
INSTALLED ONLY IN THE DISCRETION OF  
EACH AFFECTED PROPERTY OWNER. NO  
IMPLICATION OF PUBLIC UTILITY EASEMENT  
OR PUBLIC UTILITY FACILITY LOCATION IS  
TO BE DERIVED FROM THE LOCATION OF  
THESE CONCEPTUAL ROADWAYS.

## LEGEND

**EXISTING**

**PROPOSED**

**OPTIONAL**

**BACKBONE STORM DRAIN SYSTEM TO LOWER ETIWANDA CHANNEL**

THE STORM DRAIN EXTENSION ON DAHLIA IN PHASE 1A WILL BE CONSTRUCTED  
IF THE DEVELOPER CHOOSES TO INSTALL THE STREET EXTENSION OF DAHLIA

IV-6

# CITY OF FONTANA

# FONTANA GATEWAY SPECIFIC PLAN

**WILLIAMS • KUEBELBECK & Associates, Inc.**  
**FORMA-PLANNING NETWORK**

**V. PHASING**

#### PHASING/IMPLEMENTATION

We have been provided with the Proposed Land Use Phasing Program as prepared by Williams, Kuebelbeck and Associates, Inc., and Forma Planning Network. Figure 1 is a copy of that phasing program.

Pursuant to our previous discussion regarding required improvements to the Specific Plan area and the phasing program shown in Figure 1, we have prepared the following figures to indicate the phasing of those improvements.

Figures 2 - 4 Street Improvements

Figures 5 - 7 Sewer Improvements

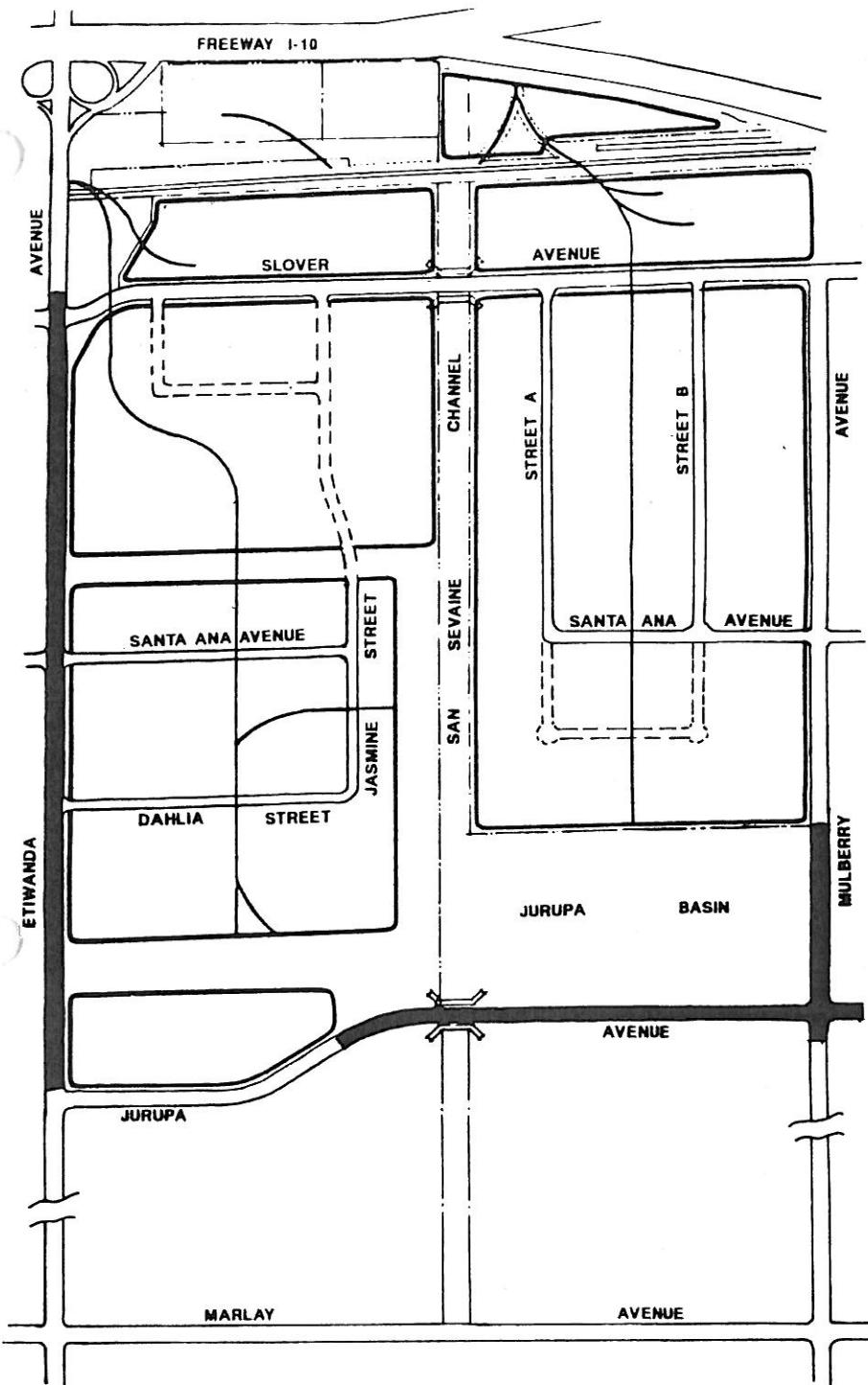
Figures 8 - 10 Water Improvements

Figures 11 - 13 Storm Drain Improvements

#### INFRASTRUCTURE SIZING AND LOCATION

Infrastructure sizes are subject to adjustment upward or downward based upon future engineering recommendations or requirements.

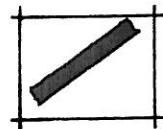
Location of sewer lines, waterlines and storm drain improvements are conceptually depicted. Exact locations will be determined by final engineering design.



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND



PROPOSED

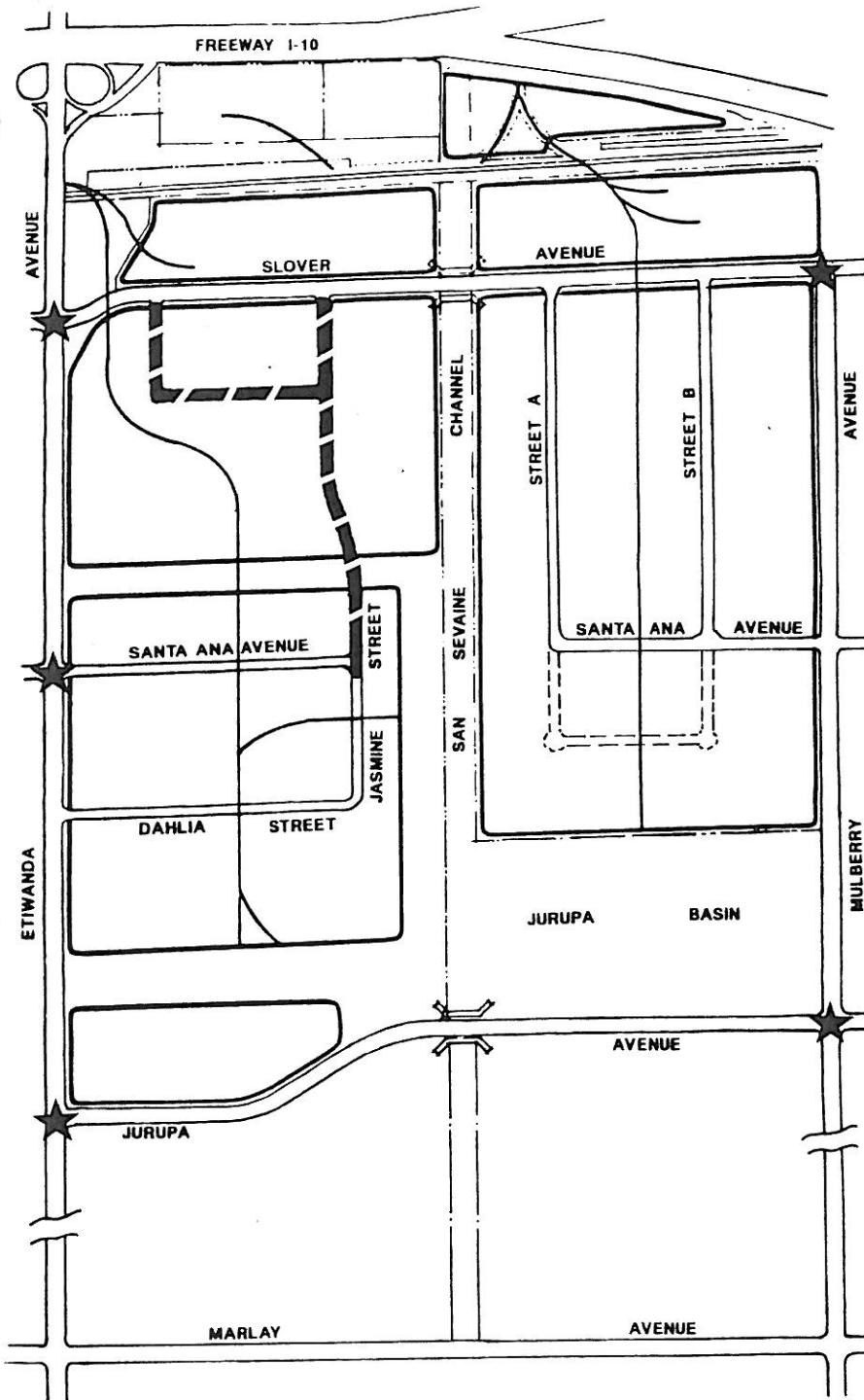
 NO SCALE

THE STREETS SOUTH OF SANTA ANA IN PHASE 1A MAY OR MAY NOT BE CONSTRUCTED, AT THE OPTION OF THE DEVELOPER.

V-4

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

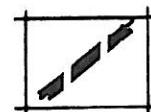
WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND



PROPOSED  
(OPTIONAL)

★ NEW TRAFFIC SIGNAL  
LOCATION OR UPGRADE

NO SCALE

THE STREETS SOUTH OF SANTA ANA IN PHASE 1A MAY OR MAY NOT BE CONSTRUCTED, AT THE OPTION OF THE DEVELOPER.

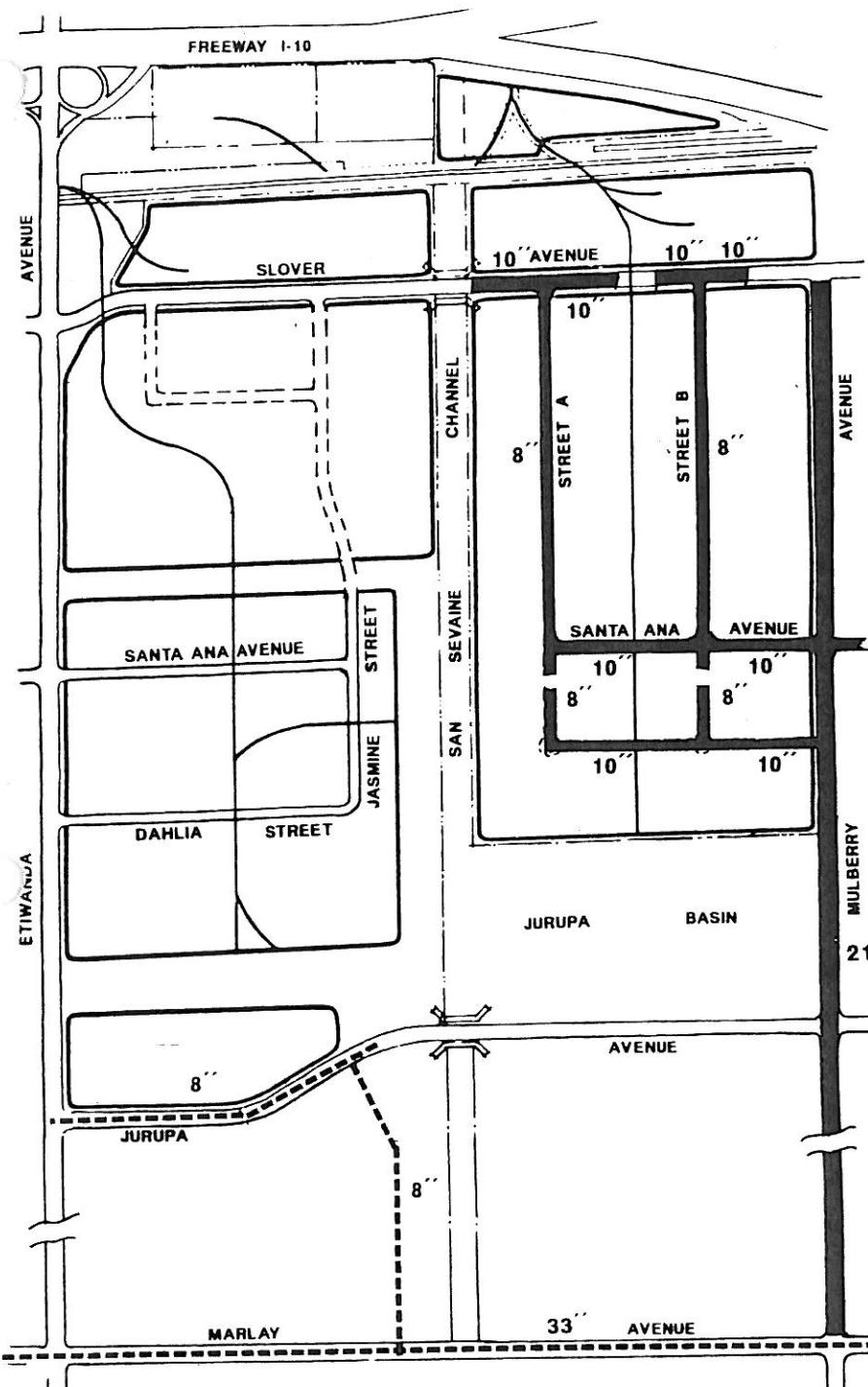
V-5

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

FIGURE 4

# PHASE 1A - SEWER SYSTEM



## NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND

EXISTING

PROPOSED

NO SCALE

THE INTERIOR SANITARY SEWERS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

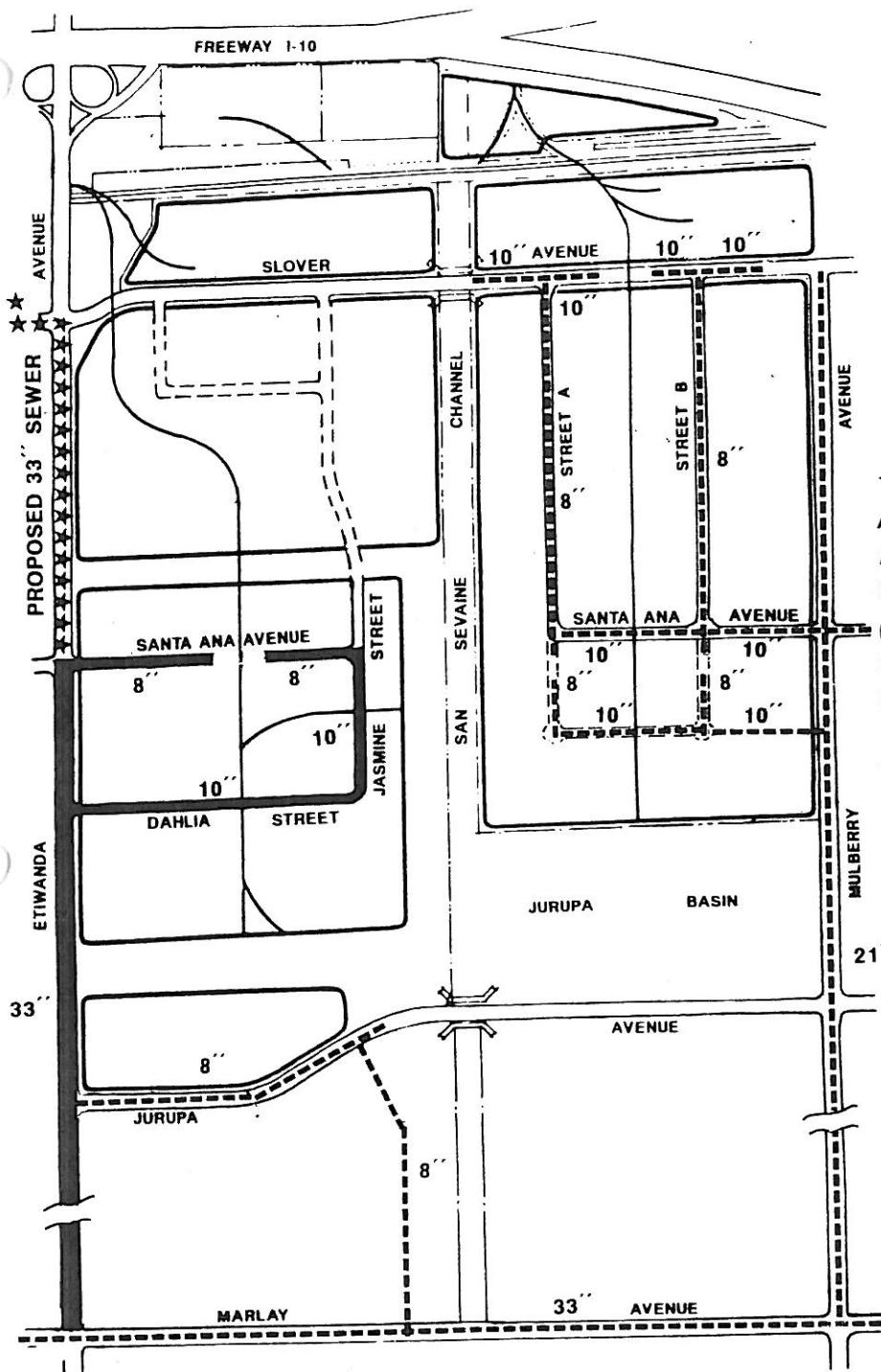
V-6

CITY OF FONTANA

## FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

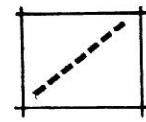
# PHASE II - SEWER SYSTEM



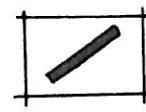
#### NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

#### LEGEND



EXISTING



PROPOSED



NO SCALE

THE INTERIOR SANITARY SEWERS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

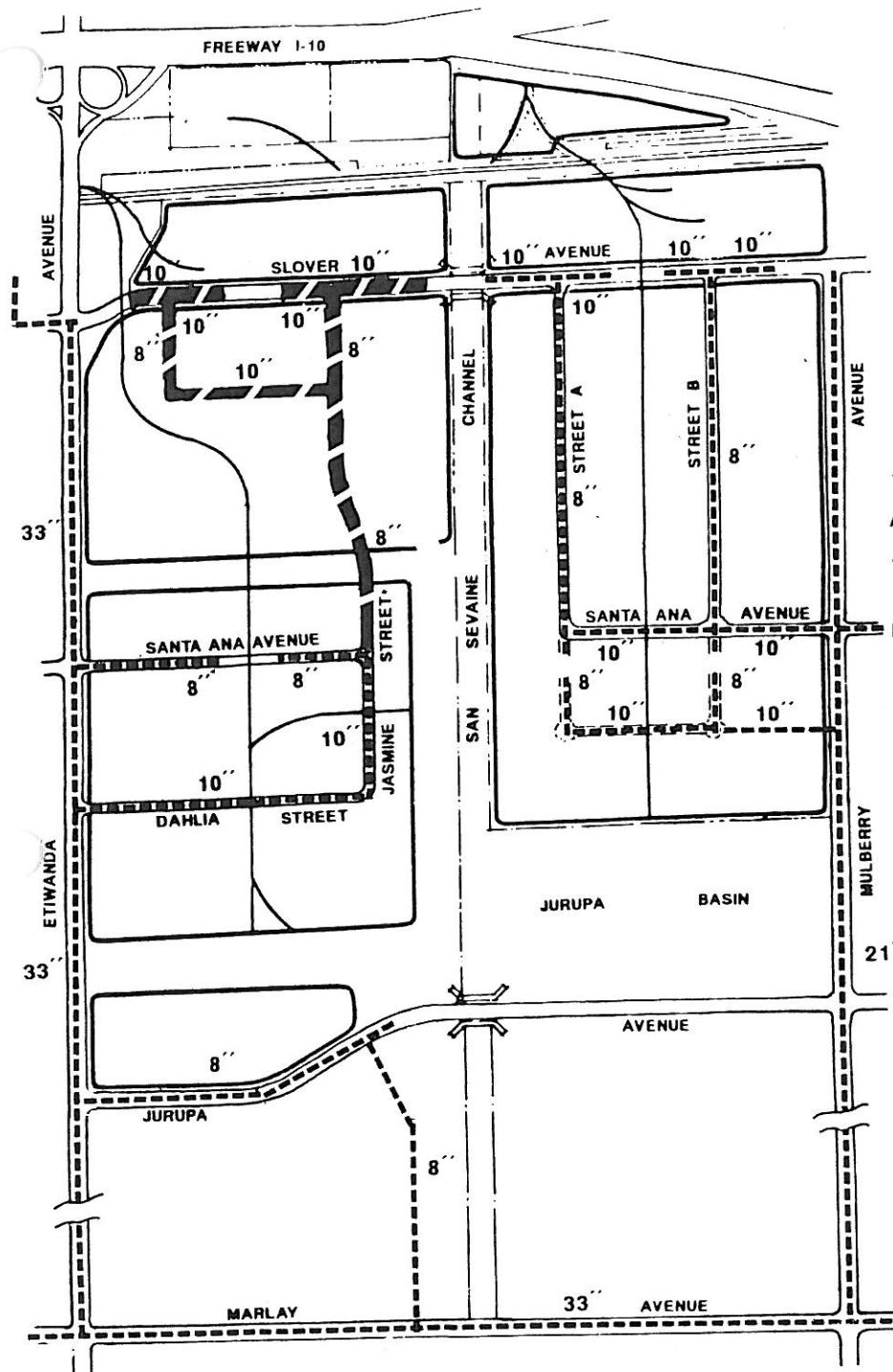
V-7

CITY OF FONTANA

## FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

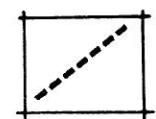
# PHASE III - SEWER SYSTEM



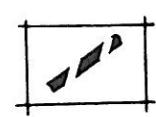
NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND



EXISTING



PROPOSED  
(OPTIONAL)



NO SCALE

THE INTERIOR SANITARY SEWERS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

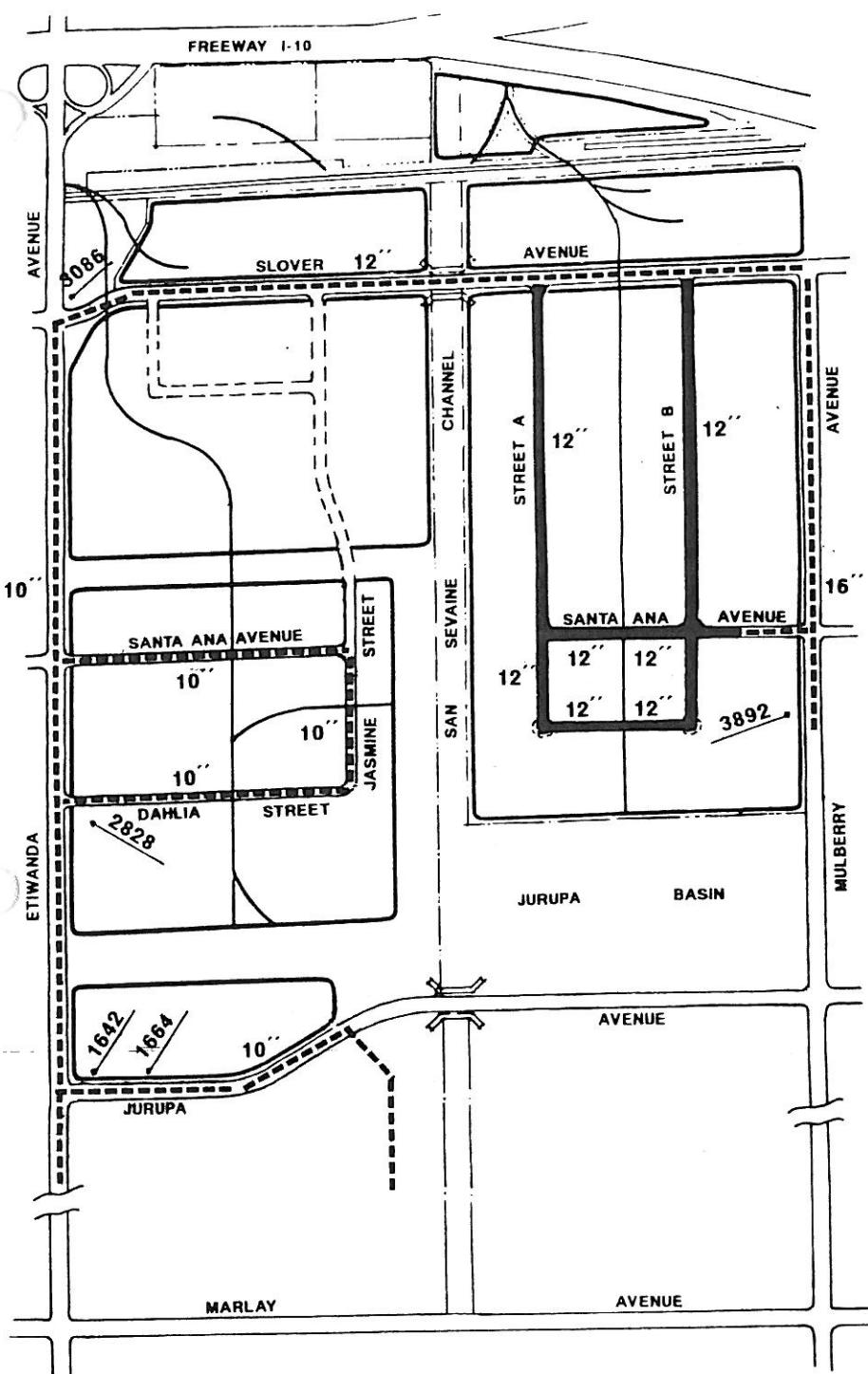
V-8

CITY OF FONTANA

**FONTANA GATEWAY SPECIFIC PLAN**

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

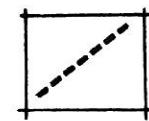
# PHASE 1A - WATER SYSTEM



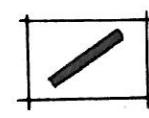
## NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND



EXISTING



PROPOSED

XXXX GPM FIRE FLOW AT 20 psi.



NO SCALE

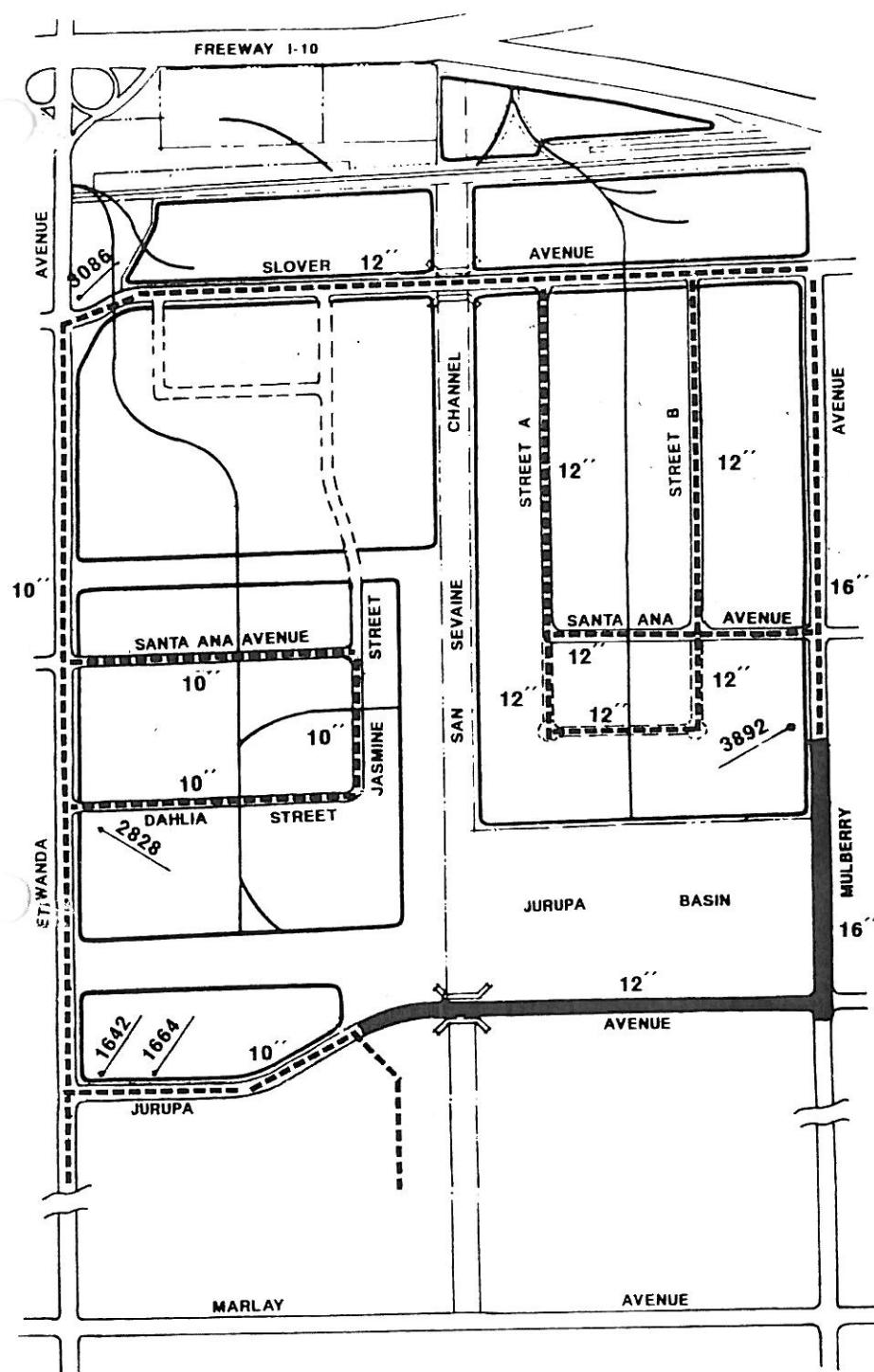
THE WATER MAINS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED  
IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

V-9

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

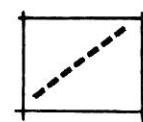
## PHASE II - WATER SYSTEM



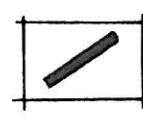
### NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

### LEGEND



EXISTING



PROPOSED

XXXX FIRE FLOW AT 20 psi.  
GPM



NO SCALE

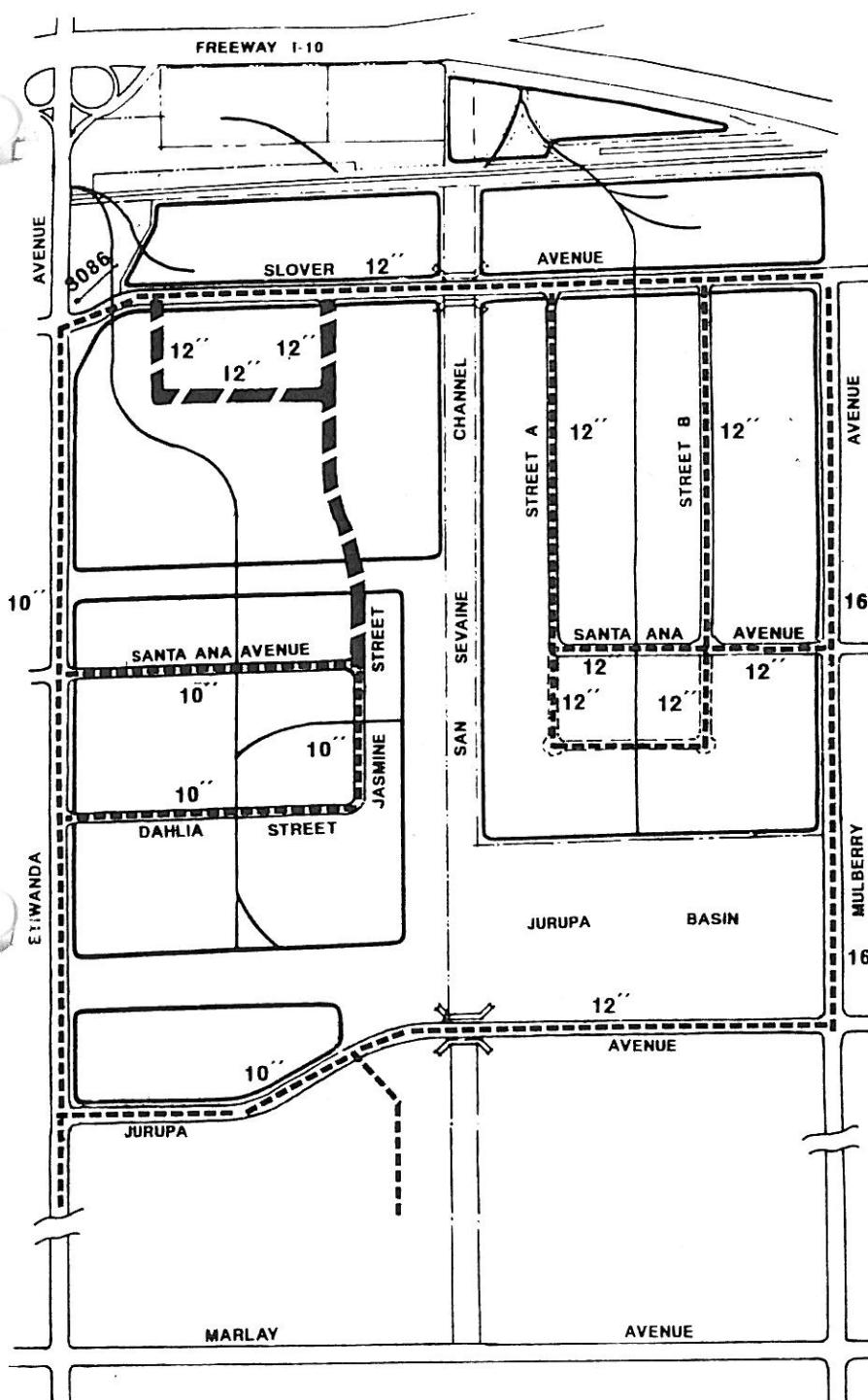
THE WATER MAINS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED  
IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

V-10

CITY OF FONTANA  
FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

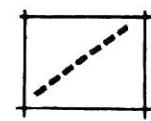
# PHASE III - WATER SYSTEM



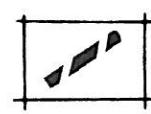
NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND



EXISTING



PROPOSED  
(OPTIONAL)



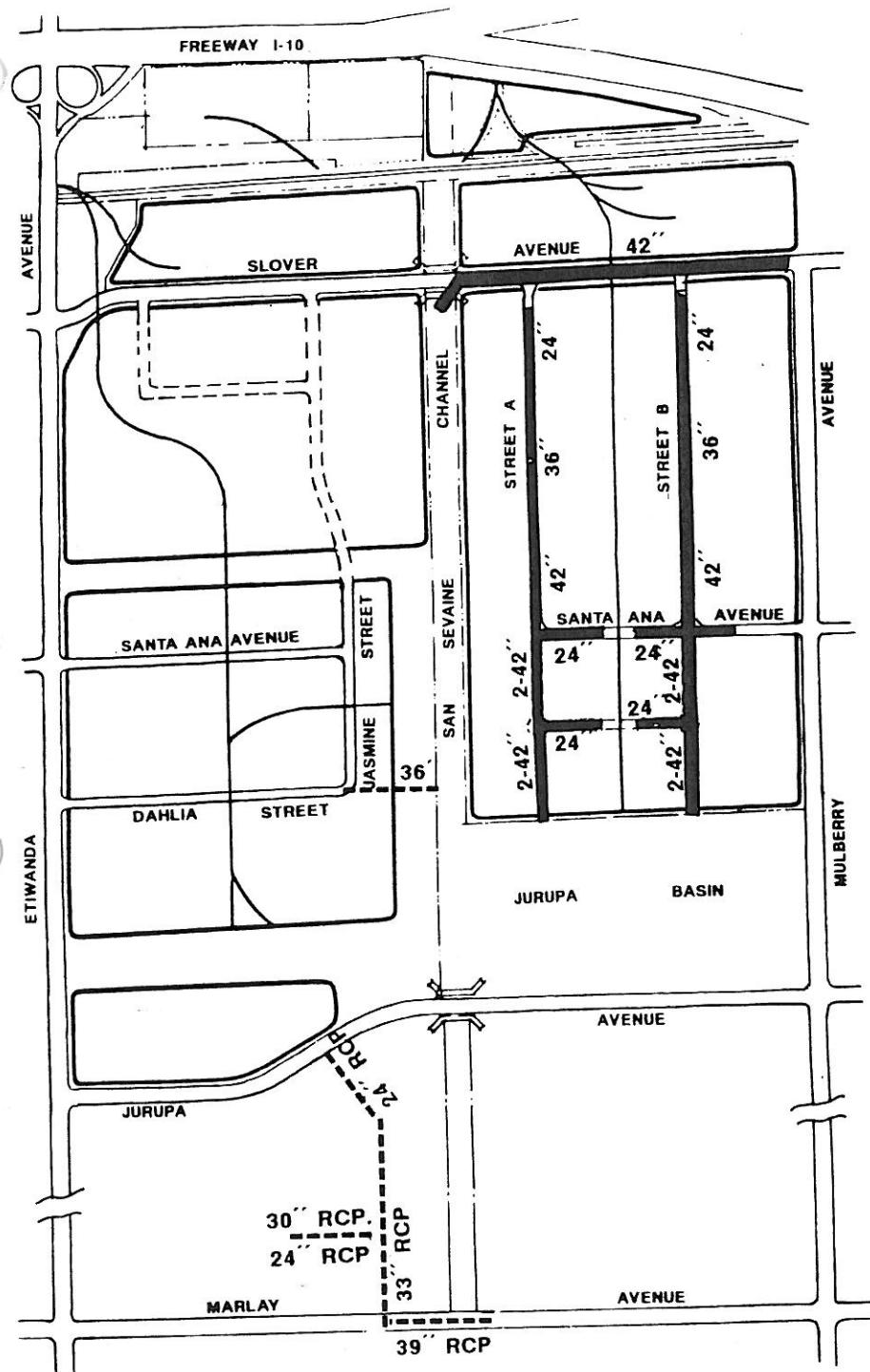
THE WATER MAINS SOUTH OF SANTA ANA IN PHASE 1A WILL BE CONSTRUCTED  
IF THE DEVELOPER CHOOSES TO INSTALL THE STREETS SOUTH OF SANTA ANA.

V-11

## CITY OF FONTANA FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS - KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

## **PHASE 1A - STORM DRAIN SYSTEM**



THE STORM DRAIN EXTENTION ON DAHLIA IN PHASE 1A WILL BE  
CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE  
STREET EXTENTION ON DAHLIA.

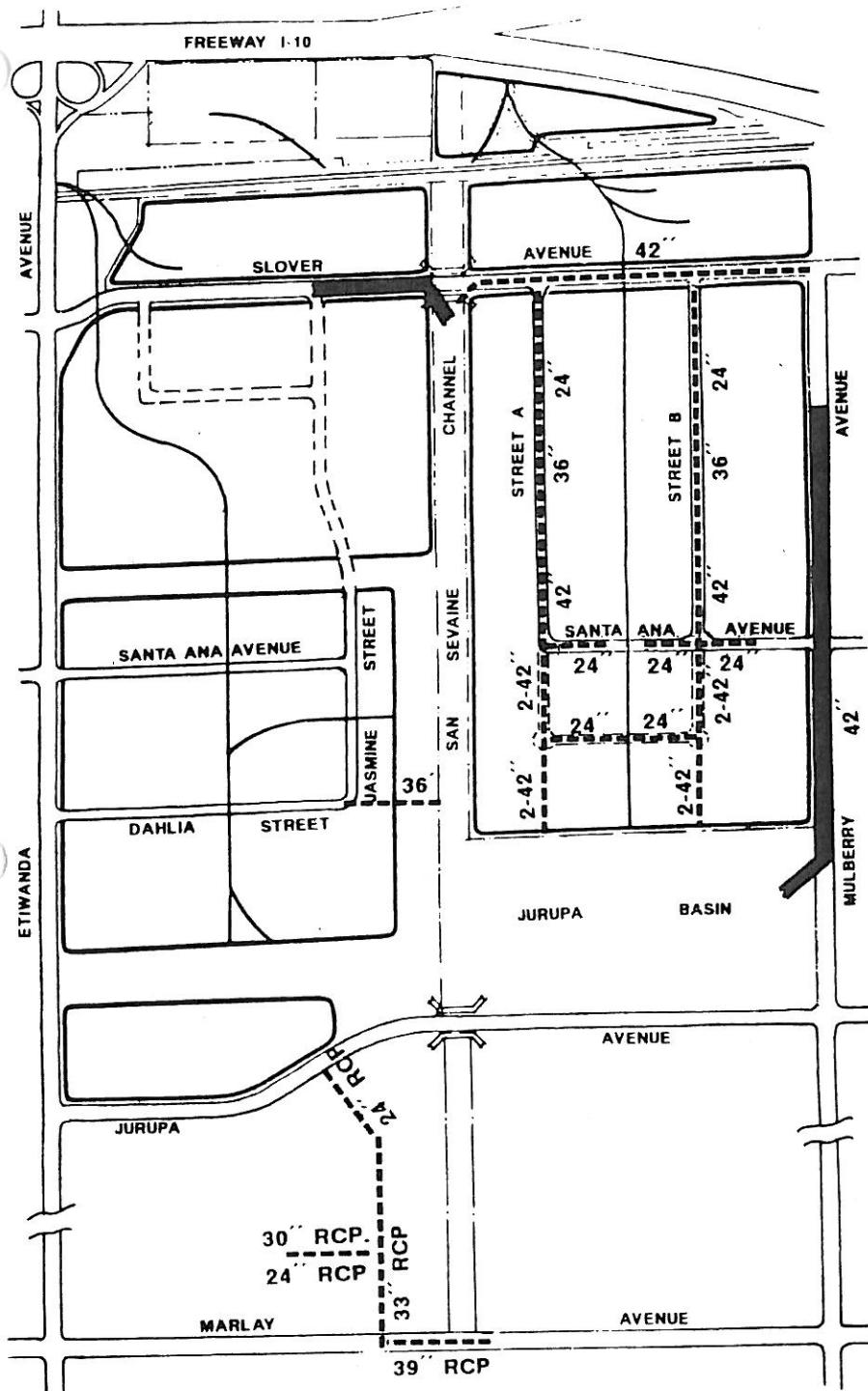
V-12

## CITY OF FONTANA

# FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

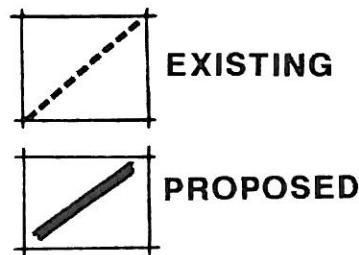
# PHASE 1B - STORM DRAIN SYSTEM



NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND



NO SCALE

THE STORM DRAIN EXTENTION ON DAHLIA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREET EXTENTION ON DAHLIA.

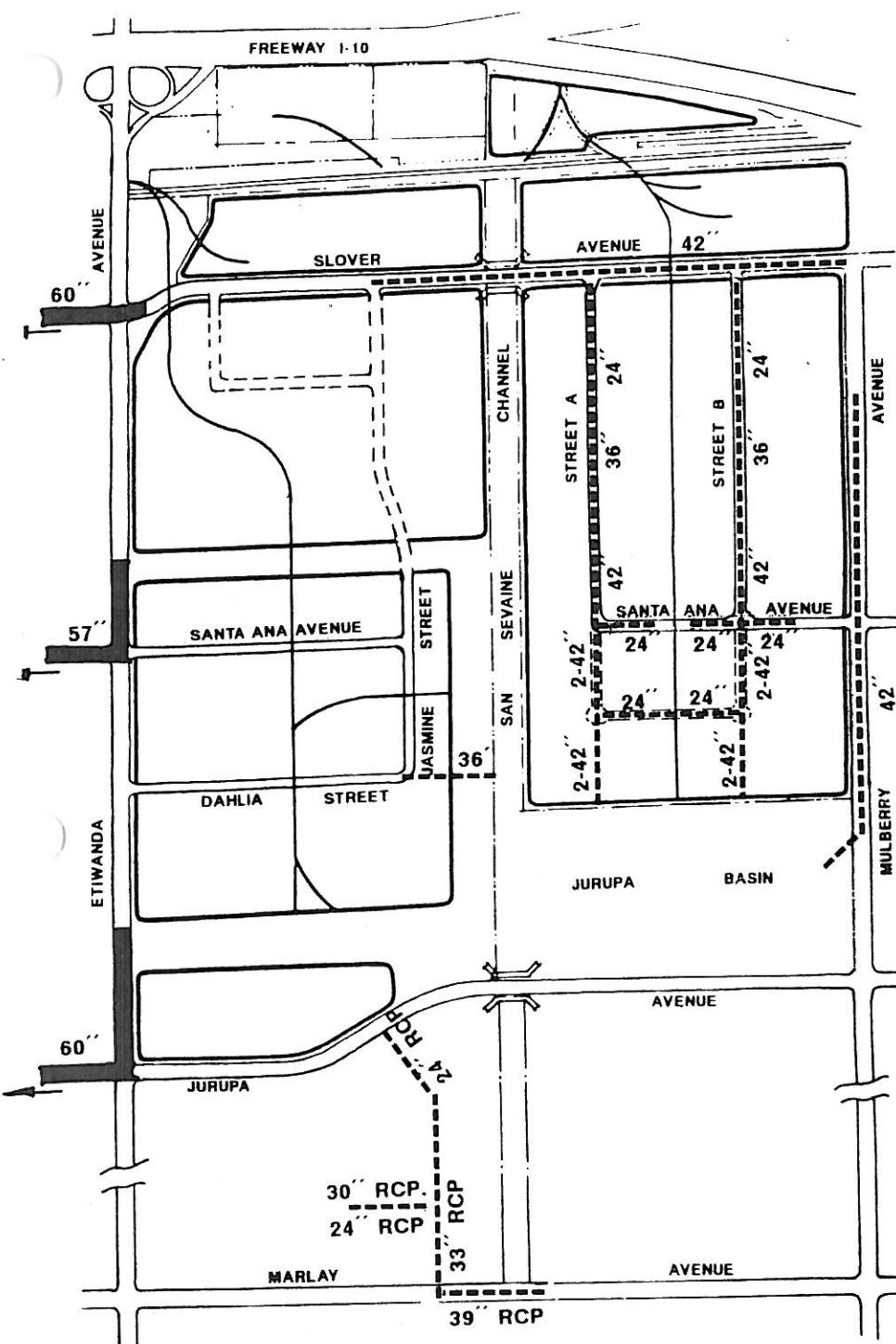
V-12

CITY OF FONTANA

FONTANA GATEWAY SPECIFIC PLAN

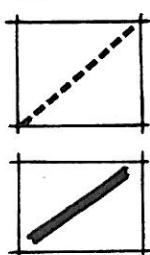
WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

# PHASE II - STORM DRAIN SYSTEM



NOTE:  
THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

## LEGEND



EXISTING



PROPOSED  
BACKBONE STORM  
DRAIN SYSTEM TO  
LOWER ETIWANDA  
CHANNEL.



NO SCALE

THE STORM DRAIN EXTENTION ON DAHLIA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREET EXTENTION ON DAHLIA.

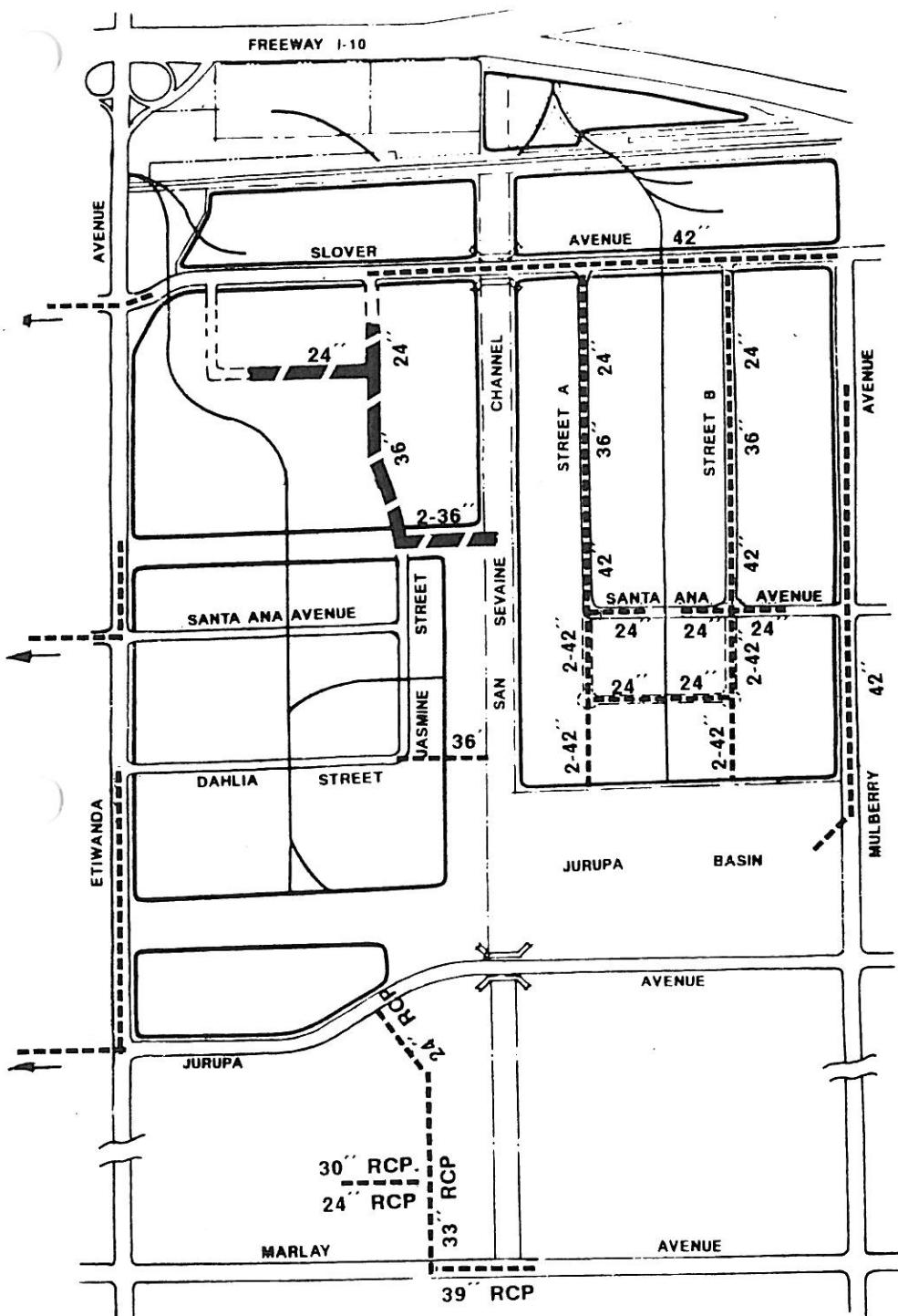
V-13

CITY OF FONTANA

## FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS - KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK

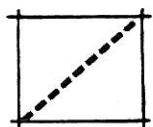
# PHASE III - STORM DRAIN SYSTEM



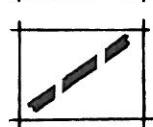
NOTE:

THE DEPICTED OPTIONAL ROADWAYS AND ALL RELATED PUBLIC UTILITY FACILITIES ARE CONCEPTUAL ONLY AND WILL BE INSTALLED ONLY IN THE DISCRETION OF EACH AFFECTED PROPERTY OWNER. NO IMPLICATION OF PUBLIC UTILITY EASEMENT OR PUBLIC UTILITY FACILITY LOCATION IS TO BE DERIVED FROM THE LOCATION OF THESE CONCEPTUAL ROADWAYS.

LEGEND



EXISTING



PROPOSED  
(OPTIONAL)



NO SCALE

THE STORM DRAIN EXTENTION ON DAHLIA IN PHASE 1A WILL BE CONSTRUCTED IF THE DEVELOPER CHOOSES TO INSTALL THE STREET EXTENTION ON DAHLIA.

V-14

CITY OF FONTANA

FONTANA GATEWAY SPECIFIC PLAN

WILLIAMS • KUEBELBECK & Associates, Inc.  
FORMA-PLANNING NETWORK



# KRUEPER ENGINEERING & ASSOCIATES, INC.

CIVIL ENGINEERING • SURVEYING • ACCIDENT ANALYSIS AND CONSULTING

568 NORTH MT. VIEW AVENUE, SECOND FLOOR • SAN BERNARDINO, CALIFORNIA 92401-1291 • (714) 884-2159

File: 1163-11.03

Date: July 2, 1986

Forma Planning Network  
1055 North Euclid  
Ontario, CA 91762

Attention: Mr. Rick Gomez  
Senior Project Director

Regarding: KAISER SPECIFIC PLAN

Dear Rick:

Please find attached the answers to the questions and additional data requested by Mr. Eric Norris during our telephone conversation on June 30, 1986.

Should you require additional information or wish to discuss this further, please do not hesitate to contact our office.

Respectfully submitted,  
KRUEPER ENGINEERING & ASSOCIATES, INC.

*Fred J. Babbitt*

Fred J. Babbitt  
R.C.E. 27674

FJB:nes

cc: Mr. Glen Sams

1. Question: What is the level of service of the roadway system under existing traffic and roadway conditions?

Response: Based upon the traffic volume data shown on Figure 10, Page I-15 of our analysis/summary dated June 23, 1986, additional level of service calculations were performed. The attached table summarizes the results of these calculations.

2. Question: What is the level of service for the roadway system under existing plus area growth traffic condition?

Response: The attached table contains the results of the level of service calculations based upon the above scenario.

3. Question: What was the area traffic generation rate used in preparing Figure 11, Page I-16?

Response: The traffic generation for the Specific Plan area is based upon a trip generation rate for an industrial area of 56 vehicle trips per acre. This generation factor was obtained from a publication entitled "Trip Generation, and Informational Report"; third edition as prepared by the Institute of Transportation Engineers.

4. Question: What is the cause for the level of Service E designation for that section of Etiwanda Avenue north of Slover Avenue?

Response: The lower roadway operating capacity on Etiwanda Avenue north of Slover Avenue is created by the configuration of the dual bridge system located within this roadway link.

The railroad overpass structure just northerly of the intersection of Etiwanda Avenue and Slover Avenue is limited in width which only allows for four (4) lanes of traffic.

The I-10 Freeway overpass and on/off-ramp combination further restricts operating efficiency of this roadway link.

Further study would be required to evaluate if rechannelization/striping of the roadway or overpass reconstruction will be required to increase this roadway links operating capacity.

5. Question: What role does the Jurupa Basin play within the overall area drainage system and what is the capacity of the San Savaine Channel within this area?

Response: The Jurupa Water Conservation Basin is located on the east side of the San Savaine Channel just north of Jurupa Avenue. The channel design flow above the basin is 18,850 cfs and the channel design flow below the basin is 12,100 cfs. The difference in the design flow is taken care of by the 1,300 acre-feet of designed storage capacity in the basin.

The basin is designed as a bypass system with a proposed over-side spillway designed to remove the peak flow from the channel. The basin will have a spillway to direct excess flow back to the channel.

6. Question: What was the sewer generation rate used in preparing the Specific Plan areas sewage generation estimates?

Response: The generation of sewage from an industrial area is a factor closely related to the type of industry contained within that area. When the types of industrial uses are not strictly defined a general sewage generation factor must be used to best approximate future conditions.

Discussions held, with the City of San Bernardino and the City of Redlands on what design parameter they use in designing their systems within industrial areas, resulted in an average flow generation factor per acre of 0.0022 cubic feet per second or 590 gallons per day.

These values were used to calculate the sewage generation estimates.

7. Question: What was the water consumption rate used in preparing the Specific Plan areas daily water demand?

Response: Discussion held with Fontana Water Company with reference to water demand for an industrial park revealed that the fire flow requirement would govern the sizing of the water system. Water consumption within an industrial area is dependent on the types of industries that are contained within that area.

Based upon further research, it was also found that the following relationship is generally accepted:

Sewage generation is approximately 70 to 80 percent of the water consumption.

A water consumption rate of 750 gallons per day per acre was utilized in calculating the water demand values for the Specific Plan area.

**FUTURE LEVEL OF SERVICE**  
**ULTIMATE ROADWAY CROSS SECTION**

Roadway	Section	Existing			Existing + Area Growth		
		VOL (1)	CAP (1,2)	LOS	VOL (1)	CAP (1)	LOS
Etiwanda Avenue	North of Slover	9.9	45.0	0.22/A	24.6	45.0	0.55/B
	South of Slover	8.4	18.0	0.47/B	25.1	75.0	0.33/A
	South of Santa Ana	8.4	18.0	0.47/B	23.9	75.0	0.32/A
	South of Jurupa	8.4	18.0	0.47/B	22.9	75.0	0.31/B
Slover Avenue	East of Etiwanda	3.8	18.0	0.21/A	10.8	38.0	0.28/A
	West of Mulberry	3.3	18.0	0.18/A	9.4	38.0	0.25/A
	East of Mulberry	2.5	18.0	0.14/A	10.0	38.0	0.26/A
Mulberry Avenue	South of Slover	3.3	18.0	0.18/A	9.4	30.0	0.31/A
	South of Santa Ana	3.0	18.0	0.17/A	9.6	30.0	0.32/A
	South of Jurupa	4.1	18.0	0.23/A	14.6	30.0	0.49/B
Jurupa Avenue	West of Etiwanda	*	*	*	18.0	75.0	0.24/A
	East of Etiwanda	0.2	38.0	0.01/A	10.0	38.0	0.26/A
	West of Mulberry	*	*	*	9.5	38.0	0.25/A
	East of Mulberry	1.9	30.0	0.06/A	16.9	38.0	0.44/B

VOL - Volume

CAP - Capacity

LOS - Level of Service

(1) Volume and capacity values in thousands.

(2) Existing roadway cross section.

\* Roadway does not exist.

## RESPONSE TO COMMENTS

### RESPONSE TO COMMENTS: FONTANA GATEWAY SPECIFIC PLAN ENVIRONMENTAL IMPACT REPORT

The following are responses to comments received on the Fontana Gateway Specific Plan Environmental Impact Report. Written comments on the document, which was circulated for public and agency review for a 45-day period ending on Dec. 24, 1986, were received from a total of seven public agencies and one private group. In addition, comments were relayed by telephone by the County of San Bernardino.

The complete text of all comments received is included at the back of this text. Comments are referred to below by responding party and number only.

It is intended that these comments will be utilized as an addendum to the Draft Environmental Impact Report.

#### Comments and Responses

##### City of Rancho Cucamonga, December 4, 1986

1. Comment noted.

##### County of Riverside Planning Department, November 4, 1986

2. Acreage total (755) indicated is correct.
3. Comment noted. Complete document contains page IV-D-2.
4. Revise text to read as follows:

"It should be noted that project-related traffic will comprise approximately 13 to 35 percent of total ADT on perimeter roadways, and will thus contribute a similar portion of ultimate traffic-generated noise."

See also Response #5.

5. As noted in the Draft Environmental Impact Report (see Analysis of Significance on page IV-G-13), ultimate traffic volumes under the proposed project will be generally equivalent to those which would be expected under the "No Project" scenario. Thus, although traffic levels will

## RESPONSE TO COMMENTS

increase, ultimate levels will not exceed those already anticipated and incorporated in planning assumptions for the study area and vicinity. Widening of vicinity roadways, as noted in the Draft EIR, would be required even under the "No Development" scenario, due to increases in traffic generated outside the study area.

As regards noise impacts, the types of land uses proposed within the vicinity of the study area -- which are primarily industrial -- are not noise-sensitive, and are compatible with the noise levels projected to be generated on vicinity roadways.

Although some residential uses are located within the vicinity of the study area, these are either 1) non-conforming uses not considered by the City of Fontana to be maintained in the long term [these are located to the east of the study area], or 2) conforming and provided with noise attenuation measures [these are located to the southeast of the study area]. It should be noted that the majority of truck traffic produced within the study area will use Etiwanda Avenue to access regional transportation routes, and will thus bypass existing conforming and non-conforming residential areas.

6. See Response #5. Industrial uses are not considered noise-sensitive, and are compatible with projected noise levels.
7. All uses within the study area shall be subject to performance standards in the existing City of Fontana zoning ordinance, which prohibit the production of objectionable odors (as well as other nuisance factors such as glare and vibration) detectable on adjacent parcels.

Abatement of objectionable odors is within the jurisdiction of the South Coast Air Quality Management District, which responds to complaints received from residents and other persons. Generally, odors are not abated by the District until complaints are received; however, once sufficient complaints are received, an abatement order is issued.

Thus, although objectionable odors may be generated in the short term, enforcement of abatement procedures by the AQMD will result in reduction of potential long term impacts to a level of insignificance.

8. Insert footnote on page IV-G-2 as follows:

RESPONSE TO COMMENTS

"Note: The Riverside Avenue/Etiwanda Avenue Interchange referred to above is alternately designated the Etiwanda Avenue/Mission Boulevard Interchange."

9. Comment noted. Delete the referenced paragraphs on pages IV-H-2 and IV-H-3, and insert the following footnotes:

Page IV-H-2:

"1. Demand for police services can be reduced through a variety of measures which are detailed in the Mitigation Measures portion of this section."

Page IV-H-3:

"1. Demand for fire protection services can be reduced through a variety of measures which are detailed in the Mitigation Measures portion of this section."

10. Revise Fire Protection mitigation measures as follows:

--- Since Station #4 provides adequate response times for study area development, and since new equipment will be added to this station's inventory, no additional fire facilities are necessary.

-- If significant number of structures in excess of 35 feet in height are constructed within the study area, the Foothill Fire Protection District shall be consulted regarding the potential need for the addition of ladder service to Station #4 or the reallocation of existing ladder service to provide optimum response times."

11. Comment noted. Revise text as indicated.

South Coast Air Quality Management District, December 10, 1986

12. Revise Table IV-B-A to read as shown on the following page. Complete air quality information for all monitoring stations in the South Coast Air Basin is available from the SCAQMD.

RESPONSE TO COMMENTS

TABLE IV-B-A

LOCAL AIR QUALITY, 1985  
 (Source: South Coast Air  
 Quality Management District)

<u>Fontana Monitoring Station</u>			
Pollutant	Standard	Days Exceeding <sup>1,2</sup>	Maximum Concentration
Carbon Monoxide	Federal > 9.3 PPM 8 Hours	0	5
	Federal > 35 PPM 1 Hour	0	
	State > 9.1 PPM 8 Hours	0	
Ozone	State > 20 PPM 1 Hour	0	
	Federal > .12 PPM 1 Hour	126	0.34
	State > .10 PPM 1 Hour	162	
Nitrogen Dioxide	State >= .25 PPM 1 Hour	0	0.14
Sulfur Dioxide	Federal > .14 PPM 24 Hours	0	0.02
	State > .05 PPM 24 Hours	0	

For Suspended Particulate and Sulfate, data indicates number of samples which exceeded standard.

For Lead, data indicates number of occasions on which standard was exceeded.

RESPONSE TO COMMENTS

TABLE IV-B-A  
(Continued)

LOCAL AIR QUALITY, 1984  
(Source: South Coast Air  
Quality Management District)

<u>Fontana Monitoring Station</u>			
Pollutant	Standard	Days Exceeding <sup>1,2</sup>	Maximum Concentration
	Federal > 260 ug/m <sup>3</sup>	2	312
Suspended Particulates (Hi-Vol.)	Federal > 150 ug/m <sup>3</sup>	22	
<hr/>			
Lead (Hi-Vol.)	Federal 1.5 ug/m <sup>3</sup> Quarterly Ave.	0	0.49
	State 1.5 ug/m <sup>3</sup> Monthly Ave.	0	
<hr/>			
Sulfate (Hi-Vol.)	State >= 25 ug/m <sup>3</sup> 24 Hours	0	16.4
<hr/>			

13. A listing and diagram of existing property ownership within the study area is shown in Figure II-A-3, which is included with these comments and should be inserted following Figure II-A-2 in the environmental impact report.

Comment noted.

14. As stated in the Draft EIR on page IV-B-6, expected variations in construction activity over the construction phase of the project make quantification of total construction-related air pollutant emissions impossible. According to information contained in Compilation of Air Pollutant Emission Factors (Third Edition), published by the U.S. Environmental Protection Agency, "The best method for calculating emissions ... is on the basis of 'brake-specific' emission

**RESPONSE TO COMMENTS**

factors." Utilizing this method, the following pollutant emission factors can be utilized:

**AIR POLLUTANT EMISSIONS FOR HEAVY-DUTY  
DIESEL-POWERED CONSTRUCTION EQUIPMENT  
(ALL EMISSIONS IN GRAMS PER HORSEPOWER HOUR)**

SOURCE: U.S. ENVIRONMENTAL PROTECTION AGENCY

Pollutant	Track-	Wheeled	Wheeled	Track-		
	Laying Tractor	Tractor	Bulldozer	Laying Loader		
<hr/>						
Carbon Monoxide	2.39	4.40	1.83	1.80		
Exhaust Hydrocarbons	0.685	1.39	0.576	0.362		
Nitrogen Oxides	9.08	9.35	12.5	6.56		
Aldehydes	0.170	0.282	0.160	0.100		
Sulfur Oxides	0.851	0.851	0.867	0.853		
Particulate	0.692	1.27	0.411	0.655		
<hr/>						
Pollutant	Scraper	Motor Grader	Wheeled Loader	Off- Highway Truck		
				Roller		
<hr/>						
Carbon Monoxide	2.84	2.19	2.62	2.62	3.65	2.82
Exhaust Hydrocarbons	1.22	0.489	0.888	0.853	0.781	1.04
Nitrogen Oxides	12.1	10.5	11.2	14.9	15.7	14.8
Aldehydes	0.280	0.121	0.197	0.220	0.196	0.203
Sulfur Oxides	0.901	0.874	0.857	0.887	1.00	0.932
Particulate	0.789	0.625	0.805	0.502	0.778	0.902

## RESPONSE TO COMMENTS

15. Comment noted. The City of Fontana General Plan contains the following policies, which are intended to reduce utilization of motor vehicle and increase use of public transit systems:

- \*\*\* To continue cooperation with Omnitrans, CalTrans, and other transportation agencies, in order to assure that all City residents have adequate access to public transit as an alternative to the automobile.
- \*\* To coordinate with Omnitrans, CalTrans, and other transportation agencies in the development of route systems and transfer points.
- \*\* To encourage and cooperate in the development of intermodal transfer points for ridesharing programs.
- \*\* To promote efficient and attractive public transit which maintains acceptable personal safety, and minimizes the disruption of neighborhoods attributable to transportation facilities and operations.
- \*\* To encourage privately owned transit systems to interface with the public transit systems."

16. Mitigation measures to reduce impacts related to fugitive dust generation are listed on page IV-A-4 of the Draft EIR.

Revise the mitigation measures on page IV-A-4 to include the following:

- \*\*\* Maintenance of equipment measures in proper tune.
- Scheduling construction activities to reduce peak emission levels.
- Discontinuing construction activities during first- and second-stage smog alerts."

California Department of Transportation, District 8, December 11, 1986

17. Comment noted.

18. According to the Project Engineer, traffic volumes on Interstate 10 have shown significant fluctuations in recent years, primarily as the result of regional freeway system

## RESPONSE TO COMMENTS

improvements which have diverted traffic to and from this route. Thus, projections of future traffic are difficult to accomplish, particularly in light of uncertainties regarding the timing of future system improvements. However, applying a straight-line annual growth project of four percent (an approximation of the average annual traffic increase in the past decade), traffic volumes on I-10 at ultimate project buildout would reach 138,000 vehicles per day. More accurate projections of traffic on this roadway link would require a regional study of proposed system improvements and trip generation, which is beyond the scope of this report.

Peak hour traffic volumes on Etiwanda Avenue at the I-10 freeway, calculated by applying a factor of ten percent to the total traffic volume on this roadway link, would reach 4,300 vehicles at project buildout. Assuming that 70 percent of these vehicles used the freeway ramp system, approximately 3,000 vehicles would access the freeway during this peak hour.

19. Comment noted.
20. Comment noted.
21. Comment noted.

### Public Utilities Commission, December 23, 1986

22. The intent of the referenced mitigation measure is to ensure that adequate internal roadways will be available to serve new development prior to occupancy of new uses. Improvements to major roadways serving the site will be required -- due to increases in study area and regional traffic -- and will be constructed prior to the need for additional roadway capacity. However, construction of ultimate roadway system improvements is not warranted at this time, and is therefore not proposed.
23. As noted on page IV-G-11 of the Draft EIR, although projected "With Project" traffic volumes for Etiwanda Avenue north of Slover Avenue indicate LOS "E", this level of service

".... would likely not be achieved in practice as drivers, experiencing traffic congestion on this route, sought alternate routes."

Thus, the actual level of service which will be achieved on this roadway link is much likely lower than LOS "E", and may be within acceptable levels.

RESPONSE TO COMMENTS

33. Comment noted. Insert information regarding types of potential crimes on page IV-H-2.

34. As regards the level of staffing which will be required to provide adequate service to the study area, authoritative sources agree that no fixed officer-to-acres ratio can be effectively applied to assess manpower needs. According to Municipal Police Administration, (International City Management Association, 1971),

"It may be said categorically that there are 1) no standards for optimum police staffing for cities of various sizes and 2) no precise formulae available for determining personnel needs."

It should be noted that manpower allocation is based on a wide variety of factors individual to a given city, including the level of protection desired, available funding, level of criminal activity, and other variables. Thus, the standard of 340 acres per sworn officer utilized in the City of Irvine, although accurate in light of the availability of and requirements for police protection in that jurisdiction, cannot be expected to yield an accurate projection of staffing requirements for the Gateway project.

In addition, it should be noted that the Irvine standard is based on providing adequate levels of protection to industrial areas which are more "employment intensive" than the proposed project (i.e., containing higher proportions of office, research/development and other uses which require large numbers of employees) and which therefore generate higher numbers of calls for police service than would be expected under the proposed project. Experience in the City of Ontario, which has a number of industrial areas similar in character to the proposed project, indicates that this type of use generates very few calls, particularly when onsite security (as required in the mitigation measures listed on page IV-H-4) is used. For instance, one beat (one sworn officer on duty 24 hours, one and one-half vehicles, and related support staff) is utilized in the City of Ontario to provide police protection to an area which encompasses the "Creekside" residential area and several thousand acres of industrial development on the city's east side. Again, it should be noted that direct comparisons are difficult to make; however, the experience of the City of Ontario would indicate a lower staffing and funding requirement than indicated in this comment.

RESPONSE TO COMMENTS

35. Insert the following mitigation measure on page IV-H-4:

"-- All site designs for individual projects within the study area shall incorporate City of Fontana building security specifications and crime prevention guidelines."

County of San Bernardino

As noted above, the County of San Bernardino commented on the Draft EIR via telephone. Mr. Dennis Brown, of County Solid Waste Management, indicated the County's desire for additional mitigation to reduce the total volume of solid wastes entering County-owned landfills, and suggested that waste separation and recovery programs could be implemented within the study area. In response, the following mitigation measure shall be inserted on page IV-H-5 of the Draft EIR:

"-- To reduce the volume of solid waste produced by the proposed project, the City of Fontana shall encourage the implementation of waste separation

and resource recovery programs within the study area. Such programs could include, but not be limited to:

-- Recycling of waste products, such as paper and metal.

-- Installation of waste separation systems to allow easier recovery of recyclable materials.

-- Dissemination of information to study area uses regarding the availability of recycling services.

ADDENDUM

FONTANA GATEWAY SPECIFIC PLAN  
ENVIRONMENTAL IMPACT REPORT ADDENDUM

In addition to revisions to the text of the Gateway Specific Plan EIR as contained in the Response to Comments, the following changes shall be made in the document:

- 1) On pages III-A-4, III-A-7, III-A-8, III-A-11, IV-C-2, IV-G-13, IV-K-3, the portion of each mitigation measure which reads,

". . . (and prior to the issuance of the certificate of occupancy) . . ."

or

". . . (and prior to issuance of the certificate of occupancy) . . ."

shall be replaced with,

". . . and prior to the issuance of the certificate of occupancy . . ."

- 2) On each of the pages identified above, and on pages III-A-10 and IV-J-5, the following footnote should be inserted:

"Note: It is anticipated that individual parcels of land will have building permits issued prior to completion of the infrastructure improvements. Such issuance may be permitted when the property owner/developer has executed a written agreement acceptable to the city whereby completion of the infrastructure improvements will be assured. The intent of this mitigation measure is to allow the orderly development of infrastructure in conjunction with the development of private improvements. Accordingly, if in the reasonable estimation of the City Engineer and the Director of Building and Safety, the intent of this mitigation measure can be achieved before issuance of the Certificate of Occupancy, building permits will be issued."



CITY OF RANCHO CUCAMONGA

Post Office Box 807, Rancho Cucamonga, California 91730, (714) 989-1851

*Mayer:*  
Dennis L. Stout

*Councilmembers:*  
Charles J. Busquet II  
Deborah N. Brown  
Pamela J. Wright  
Jeffrey King

*City Manager:*  
Lauren Wasserman

December 4, 1986



Laurie Lawhorn, Assistant Director  
City of Fontana Planning Department  
8353 Sierra Avenue  
Fontana, CA 92335

SUBJECT: ENVIRONMENTAL IMPACT REPORT FOR GATEWAY SPECIFIC PLAN

Laurie  
Dear Ms. Lawhorn:

Thank you for the opportunity to comment on the Environmental Impact Report for the Gateway Specific Plan.

We have no comments on this project. However, we would appreciate it very much if you could continue to keep us informed of your future projects in our vicinity. Thank you. 1

Sincerely,

COMMUNITY DEVELOPMENT DEPARTMENT  
PLANNING DIVISION

Otto Kroutil  
Senior Planner

OK:RY:dct

Laurie Lawhorn, Asst. Director  
Advanced Planning  
Projected Police Services  
Fontana Gateway EIR

Page Two

Problems Associated With The Project

Potential problems associated with the project site are as **33** follows:

1. Traffic Control
2. Parking Control
3. Beat Patrol at Night for Crime Deterrence
4. Vandalism
5. Burglaries
6. Labor Disputes
7. Drug Enforcement
8. White Collar Crimes

The aforementioned are not necessarily all inclusive of Police problems that may occur. Specific problems would vary according to the time of day, particular day of the week, and season.

Police Costs To The Project

Currently, the Fontana Police Department is in the process of developing an officer ratio per industrial/commercial acre formula. Therefore, in an attempt to calculate Police service costs to the Gateway project, the department will utilize a formula adopted by the City of Irvine Police Department. **34**

The Irvine Police Department currently utilizes one sworn officer per 340 acre formula. Applying this formula to the 755 acre project, the department is compelled to note that the formula does not necessarily coincide with Fontana's industrial/commercial development land density. However, as previously indicated, the Irvine formula is a basis from which the department can objectively define its service needs.

For simplicity, the department will numerically round off to the nearest whole number to arrive at the total number of officers needed for the project. Based on 755 acres, the department estimates that a total of two (2) sworn personnel will be needed to service the project area. Sworn personnel salary and benefits have been calculated at Officer II, C Step. Utilizing an Officer II, C Step, allows for a salary and benefit adjustment to adequately determine appropriate supervisory levels when necessary.

Cont'd.

Laurie Lawhorn, Asst. Director  
Advanced Planning  
Projected Police Services  
Fontana Gateway EIR

Page Three

Conclusion

The personnel requested in the following budget estimate is the minimum number that should be added to the staff of the Fontana Police Department. Care should be taken to prevent the level of Police services to the current City residents from diminishing whatsoever.

The department would also like to note that the project site should incorporate the building security specifications and crime prevention guidelines for Environmental Design on file in the Police Department and Planning Department. 35

BLA:jm

Attachments

Police Service Costs

Personnel Expense.....	<u>\$117,904</u>
Non-Personnel Expense.....	<u>\$ 21,886</u>
Capital Outlay.....	<u>\$ 23,845</u>

Total Projected Police  
Expense for 12 Months \$163,635

<u>Personnel Expenses</u>	<u>Positions</u>	<u>Salary</u>	<u>Benefits</u>	<u>Total</u>
Officers	2	\$55,200	\$43,188	\$98,388
Overtime Compensation	- 1.59% x 98,388			\$ 1,564
Uniform Allowance	- 2 x 500			\$ 1,000
Special Duty Pay (Court)	- 1.6% x 98,388			\$ 1,574
Administrative Overhd. (Existing)	- 15%			\$15,378
			<b>Total Personnel</b>	<b><u>\$117,904</u></b>

Non-Personnel Expense

Departmental Expense.....	\$ 500
Uniform Expense (2x500).....	\$1,000
Advertising Expense (2x50x2 Newspapers).....	\$ 200
Communications.....	\$ - 0 -
Utilities.....	\$ - 0 -
Rents/Leases.....	\$ - 0 -
Motor Pool.....	\$16,200
(One (1) Blk. & White x 1350 Vehicle Rental x 12 mos.)	
Computer Rentals.....	\$ - 0 -
Equipment Maintenance.....	\$ 150
Professional and Contracted Services.....	\$2,036
Pre-Employment Costs	275 x 2 = 550
Polygraph	75 x 2 = 150
Psychological	125 x 2 = 250
Fingerprints	18 x 2 = 36
Background	500 x 2 = 1,000
Credit Check	25 x 2 = 50
	<hr/>
	2,036

Insurance (Cars) (500 x 1).....	\$ 500
Membership & Dues.....	\$ - 0 -
Training (2 x 500).....	\$1,000
Construction.....	\$ - 0 -
Miscellaneous.....	\$ 100
Employee Education Reimbursement (100 x 2).....	\$ 200
Total Non-Personnel Expense	<u>\$21,886</u>